

COMMUNITY DEVELOPMENT DEPARTMENT

Permit Center
345 N. El Dorado Street
Stockton, CA 95202



August 21, 2018

Brent Collins
8775 Folsom Blvd., Ste. 200
Sacramento, CA 95826

A.H.O Associates, LLC
1350 E. Philadelphia St.
Pomona, CA 91766

RE: Site Plan Review Application #P18-0466 (6440, 6520 Aviation Drive & 6922 South McKinley Avenue, APN 177-450-22, -23, -25).

Dear Mr. Collins, A.H.O Associates, LLC:

This letter provides the City of Stockton's approval of Site Plan Review Application No. P18-0466 for construction of a new warehouse building at 6440, 6520 Aviation Drive & 6922 South McKinley Avenue (herein referred to as "Project"). Per the Development Code Chapter 16.152 (Site Plan Review), the Site Plan Review Committee finds the Project to be, with Very Minor Corrections, in compliance with said chapter, as described below.

Site Plan Review Committee Recommendation

1. On July 23, 2018 and in conformance with Development Code §16.152.050(C), the Site Plan Committee (SRC) reviewed the application and found the Project required Minor Corrections. The SRC recommendation was subsequently transmitted to the applicant.
2. On August 20, 2018, the SRC reviewed the application for conformance with the requested changes and recommended Approval with Very Minor Corrections in conformance with Development Code §16.152.050(C)(3)(b).

Director's Decision

Based on the Site Plan Review Committee's recommendation, the Director issues Site Plan Review approval for the plans included at **Exhibit A**. No work shall be performed until a valid construction permit has been issued. The Very Minor Corrections to be incorporated in documents submitted for purposes of construction shall address and fulfill the requirements enumerated below.

Building & Life Safety

1. This parcel is located in a Special Flood Hazard Areas (SFHA) as shown on the Federal Emergency Management Agency map, Zone A (unnumbered). Because the project is greater

than 5 acres, it must comply with Stockton Municipal Code (SMC) §15.44.170 which states:

- a. All new subdivision proposals and other proposed development, including proposals for manufactured home parks and subdivisions, greater than 50 lots or five (5) acres, whichever is the lesser, shall:
 - b. Identify the Special Flood Hazard Areas (SFHA) and Base Flood Elevation (BFE).
 - c. Identify the elevations of lowest floors of all proposed structures and pads on the final plans.
 - d. If the site is filled above the base flood elevation, the following as-built information for each structure shall be certified by a registered civil engineer or licensed land surveyor and provided as part of an application for a letter of map revision based on fill (LOMR-F) to the Floodplain Administrator:
 - i. Lowest floor elevation,
 - ii. Pad elevation,
 - iii. Lowest adjacent grade.
 - e. All subdivision proposals and other proposed development shall be consistent with the need to minimize flood damage.
 - f. All subdivision proposals and other proposed development shall have public utilities and facilities such as sewer, gas, electrical, and water systems located and constructed to minimize flood damage.
 - g. All subdivisions and other proposed development shall provide adequate drainage to reduce exposure to flood hazards. (Ord. 003-11 C.S. § 1, eff. 2-24-11)
2. In accordance with SMC §15.44.170(A)(1), Base Flood Elevation (BFE) data must be developed for the unnumbered 'A' zone shown on the FEMA map. The BFE data must be developed in accordance with FEMA publication 265: Managing Floodplain Development in Approximate Zone A Areas. If BFE data cannot be obtained per Section IV of the publication then a detailed method must be utilized per Section V. Note; a simplified method approach is not acceptable. The referenced FEMA publication may be found here: https://www.fema.gov/media-library-data/20130726-1453-20490-6341/frm_zna_265.pdf
3. Once the BFE is established; the finished floor must be elevated to or above the BFE.
4. Any accessory buildings such as pump houses, guard sheds, etc. will also have to be elevated to or above the BFE.
5. Written approval for the development will be required from the San Joaquin County Flood Control Agency as it relates to development adjacent to the slough.

Fire Prevention

6. Fire Prevention staff will review this under the building permit submittal.

7. The applicant should attend a pre-building permit submittal meeting with Fire Prevention staff to review site access and utilities. The applicant can contact Fire Prevention Specialist Phil Simon of the Fire Prevention Division at (209) 937-8315 or phil.simon@stocktonca.gov to schedule the meeting.

Municipal Utilities

8. Civil plans for the Building Permit submittal shall show a connection to the public storm drain.

Engineering & Transportation

9. Plans for the Building Permit submittal shall show truck turn templates for simultaneous in-bound and out-bound patterns for both proposed driveways.

Current Planning

10. Prior to issuance of a certificate of occupancy, a reciprocal access agreement shall be required for access to the land-locked parcel to the south west.

Conclusion

This decision is effective immediately. Changes to the Project shall be in compliance with Development Code Chapter 16.104 (Changes to an Approved Project). This approval shall be void unless the required building permit is submitted within 12 months of this decision. However, a time extension may be requested under Development Code Chapter 16.96 (Expirations and Extensions).

If you have any questions, please do not hesitate to contact me at allison.holmstedt@stocktonca.gov or (209) 937-8267.

Regards,

Allison Holmstedt
Assistant Planner, City of Stockton
Community Development Department

Attachments

Exhibit A – Approved Plans

Exhibit A – Approved Plans

- I. Sheet A1.1 – Proposed Site Plan**
- II. Sheet A2.1 – Proposed Building Floor Plan**
- III. Sheet LP – Preliminary Landscape Plan**
- IV. Sheet E1.1 – Site Photometric**

G B D H

DESIGN GROUP, INC.

ARCHITECTURE
ENGINEERING
1815 18th Street, Suite 1
San Francisco, California 94107
Tel: (415) 774-1100 Fax: (415) 774-1108



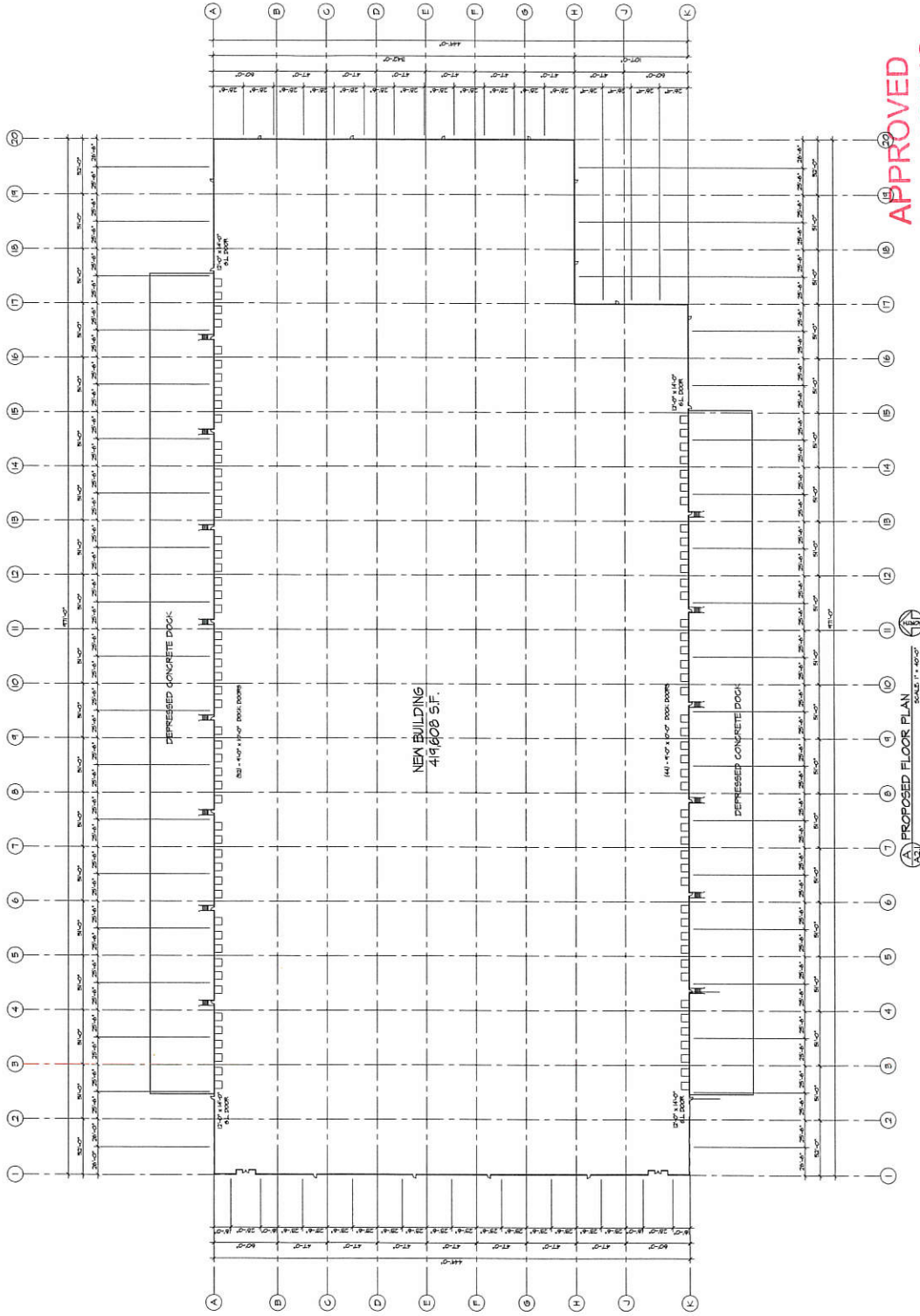
PRELIMINARY
06/14/18

WAREHOUSE /
DISTRIBUTION BUILDING
2400 VAYLON DRIVE
STOCKTON, CALIFORNIA

PANATTONI
PLANNING & ENGINEERING COMPANY
1000 MARKET STREET, SUITE 100
SAN FRANCISCO, CALIFORNIA 94102
(415) 774-1100

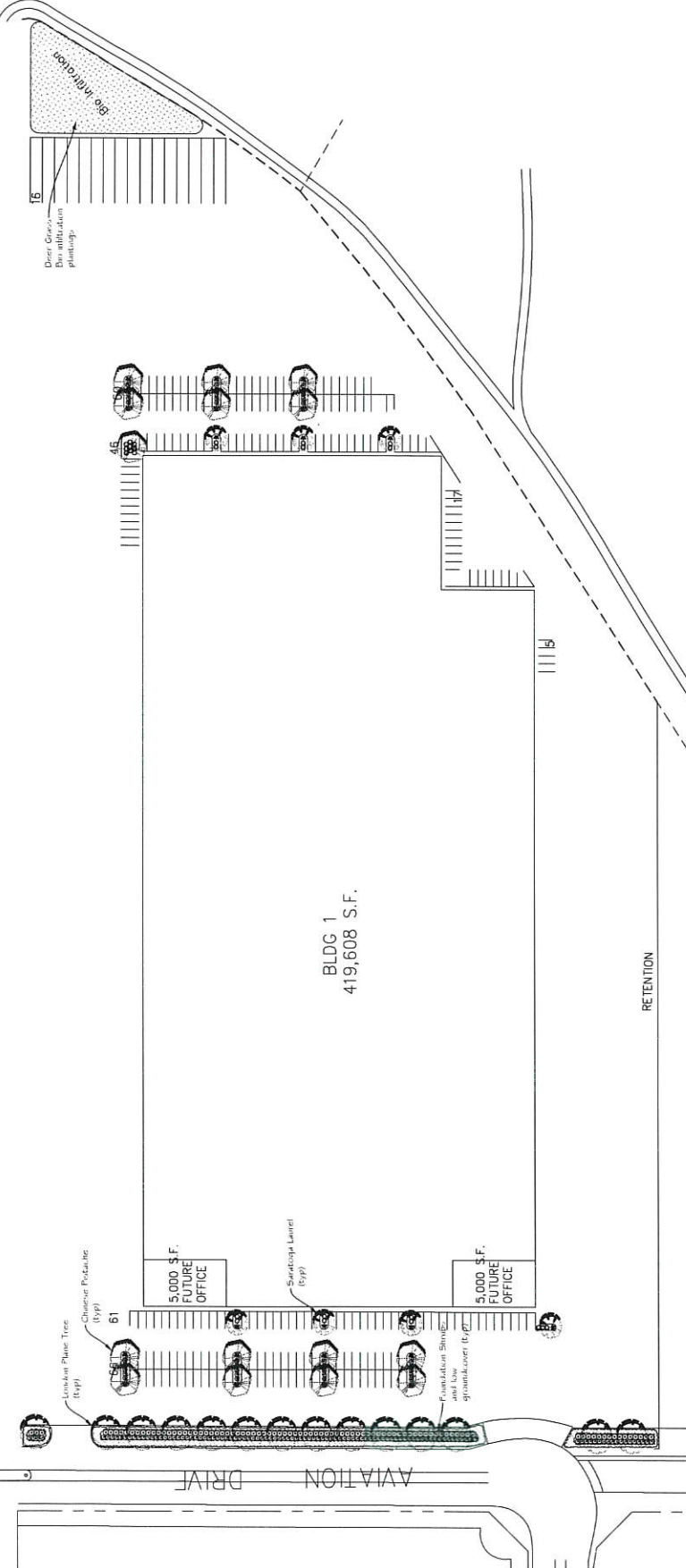
PROPOSED BUILDING
FLOOR PLAN

Project No.	18030118	Project Title	PH/1719
Client No.	DAV	Client Name	
Community	EAH	Scale	A2.1



APPROVED
BY PLANNING
CITY OF STOCKTON

Olivia Hahn
NAME
8-21-18
DATE
A2.1



WELO Worksheet

Analysis Data Worksheet
Reference: Engineering Station (ESt) 51.9

Regulate landscape areas - There are no special landscape areas.

Minimum Factor	Method	Efficiency (E/F)	Area (Ac)	ET&S Area (Ac)	ET&S Water Use (GPD)
1	0.2 Drip	0.81	0.25	2442	316179
2	0.2 Drip	0.81	0.25	2244	28622
3	0.2 Bubble	0.81	0.25	850	10725
4	0.2 Bubble	0.81	0.25	250	3125
5	0.2 Bubble	0.81	0.25	250	3125
6	0.2 Drip	0.81	0.25	1804	22812
				Subtotal	778111

Maximum Allowed Water Allowance (MkWA)
278623
278623
045

Preliminary Notes

- NO INVASIVE PLANT SPECIES SHALL BE USED.
 - ALL PLANTS AND TREES ARE LOW WATER VARIETIES SUITED TO THE ENVIRONMENT OF THE CITY OF STOCKTON.
 - PLANTS ARE GROUPED INTO ZONES WITH SIMILAR WATER REQUIREMENTS
 - ALL PLANTER AREAS SHALL BE TREATED WITH A 3" LAYER OF MULCH.
- PRELIMINARY IRRIGATION NOTES
- A FULLY AUTOMATIC IRRIGATION SYSTEM THAT MEETS CURRENT WATER EFFICIENT LANDSCAPE ORDINANCE REQUIREMENTS SHALL BE INSTALLED INCLUDING THE FOLLOWING:
1. ALL PLANTER AREAS INCLUDING BIO INFILTRATION AREAS SHALL BE IRRIGATED BY DROPPED IRRIGATION CONTROL VALVES.
2. TREES SHALL RECEIVE DEEP WATER/FERTILIZATION TUBES AND DIRT WATER BUBBLERS ON THEIR OWN CONTROL VALVES.
3. CERTIFICATE OF COMPLETION SHALL BE SUBMITTED WITH LANDSCAPE IMPROVEMENT PLANS AND IRRIGATION AUDIT SHALL BE COMPLETED AFTER THE IRRIGATION SYSTEM HAS BEEN INSTALLED FOR WATER USE.

Plant Palette

Botanical Name	Common Name	Water	Size	Use
Laurus 'Saratoga'	Saratoga Bay Tree	Low	15 Gallon	Evergreen Shade Tree
Fistacia c. 'Keith Day'	Chinese Pistache	Low	15 Gallon	Deciduous Shade Tree
Platanus 'Columbia'	London Plane Tree	Low	15 Gallon	Deciduous Street Tree

Botanical Name	Common Name	Water	Size	Use
Foundation Shrubs/Accents	Purple Rock Rose	Low	1 Gallon	Foundation Shrub
Cistus purpureus	Noel Grevillea	Low	5 Gallon	Foundation Shrub
Grevillea noelli	Fringe Flower	Low	5 Gallon	Foundation Shrub
Loropetalum 'Purple Majesty'	Rosemary	Low	5 Gallon	Foundation Shrub
Loxostemum 'Tuscan Blue'	Society Garlic	Low	1 Gallon	Accent Plant
Tulbaghia violacea	Stone Juniper	Low	1 Gallon	48" OC

Botanical Name	Common Name	Water	Size	Use
Ground Cover	Juniper c. 'Blue Pacific'	Low	1 Gallon	48" OC
Bio-Infiltration Plantings	Muhlenbergia rigens	Low	1 Gallon	Grass

APPROVED
BY PLANNING
CITY OF STOCKTON

Olivia Holms
NAME

DATE: 8-21-18

SCALE: 1" = 50'



CITY OF STOCKTON

COMMUNITY DEVELOPMENT DEPARTMENT

345 North El Dorado Street • Stockton, CA 95202-1997 • (209) 937-8266 • Fax (209) 937-8893

June 22, 2021

Patrick Buckley
Alston Construction
8775 Folsom Boulevard, Suite 201
Sacramento, CA 95826

RE: Site Plan Review and Design Review Application #P21-0448 (6440 Aviation Dr; APN 177-450-36)

Mr. Buckley:

This letter provides the City of Stockton's approval of Site Plan Review and Design Review Application No. P21-0448 for the Truck Parking Lot and Guard Shack located in the Industrial, General District (IG) at 6520 Aviation Drive, herein referred to as the "Project." This approval is based on the following findings and conditions of approval.

Site Plan Review Committee

On May 12, 2021 and in conformance with SMC §16.152.010, the Site Plan Review Committee (SPRC) reviewed the Project and made a recommendation to the Director that the project required major corrections. On DATE, revisions for the Project were received.

On DATE and in conformance with SMC §16.152.010, the Site Plan Review Committee (SPRC) reviewed the revised Project and made a recommendation to the Director that the project is in compliance with the requirements of SMC §16.152.010 and approval should be granted. The Director concurs with the SPRC recommendation and hereby issues approval.

Design Review

Pursuant to Section 16.120.050(D)(1) of the Stockton Municipal Code (SMC), the project has been reviewed by Staff for compliance with the Citywide Design Guidelines, and the Development Code standards and found to meet the required findings cited in SMC §16.120.060; and therefore, staff recommends approval. The Director confirms staff's recommendation based on the original findings and project specific conditions of approval.

Findings

Citywide Design Guidelines Consistency

1. Finding: The proposed development is consistent with all applicable provisions of this Development Code and other applicable City ordinances. (§16.120.060(A))

Evidence: The proposed site is zoned Industrial, General (IG) and the Project is consistent with all applicable provisions of the SMC, Title 16, Development Code.

2. Finding: The general design considerations, including the character, quality, and scale of design are consistent with the purpose/intent of this chapter and the Guidelines and other design guidelines that may be adopted by the City. (§16.120.060(B))

Evidence: The Project exhibits a practical and modern design that is consistent with all applicable provisions of the Stockton Citywide Design Guidelines, Section 5.02.070, Industrial and Warehouse Use Design Guidelines for Accessory Structures.

3. Finding: The architectural design of structures and their materials and colors are visually compatible with surrounding development. Design elements (e.g., awnings, exterior lighting, screening of equipment, signs, etc.) have been incorporated into the project to further ensure its compatibility with the character and uses of adjacent development, and/or between the different types of uses in a mixed-use development. (§16.120.060(C))

Evidence: The subject site is located along an Industrial General area. The Project presents an industrial design with pragmatic elements, including a muted paint color and durable features suited to an industrial area. The Project is compatible with the surrounding development and does not interfere with other activities and elements.

4. Finding: The location and configuration of structures are compatible with their sites and with surrounding sites and structures and do not unnecessarily block views from other structures or dominate their surroundings. (§16.120.060(D))

Evidence: The proposed façade maintains compatibility with the surrounding commercial/office uses by employing a consistent color selection with the gray and white Industrial buildings within this area. The location of this structure in the context of this Industrial Area will not block views from other structures or dominate the surroundings.

5. Finding: The general landscape design, including the color, coverage, location, size, texture, and type of plant materials, provisions for irrigation, planned maintenance, and protection of landscape elements have been considered to ensure visual relief, to complement structures, and to provide an attractive environment. (§16.120.060(E))

Evidence: The Project proposes to install landscape elements to ensure visual relief in an industrial area. The trees, shrubs, and ground cover proposed will create a stylized landscaped buffer that will separate this project from the surrounding areas. Additional landscaping in this area will complement the adjacent properties landscaping and create an attractive environment.

6. Finding: The design and layout of the proposed project will not interfere with the use and enjoyment of neighboring existing or future development and will not result in vehicular or pedestrian hazard. (§16.120.060(F))

Evidence: The Project design is a discreet size and is integrated with the style and design of the industrial area. The Project will provide a service to alleviate trailer parking in this neighborhood and will not interfere with the use of neighboring existing development. The Site Plan contains mitigation measures for Vehicular and Pedestrian Safety.

7. Finding: The building design and related site plans, including on-site parking and loading, has been designed and integrated to ensure the intended use will best serve the potential users or patrons of the site. (§16.120.060(G))

Evidence: The proposed design and site plan includes efficient parking and loading for the site patrons as well as parking provisions for employees. The Project has been designed to ensure the intended use will best serve the patrons of the site.

8. Finding: Special requirements or standards have been adequately incorporated, when applicable, into the building and/or site design (e.g., American Disabilities Act regulations, historic preservation, mitigation measures, open space, utilities, etc.). (§16.120.060(H))

Evidence: This site has been reviewed by the Building and Life Safety Division and other relevant City Departments during the required building permit review process to ensure all special requirements and/or standards are incorporated (e.g., American Disabilities Act regulations, historic preservation, mitigation measures, open space, utilities, etc.)

California Environmental Quality Act

The Project is Categorically Exempt from the California Environmental Quality Act (CEQA), pursuant to CEQA Guidelines §15268 (Ministerial Projects). The Design Review of a Comprehensive Sign Program is a ministerial action, because the construction of signs on a commercial property is allowed by right (SMC §16.120.040(A) and §16.120.050(D)(1)(a)).

Conditions of Approval

Project-Specific Conditions

1. The Project approved by this action shall conform to the plans included as Exhibit 1.
2. Plans submitted for the purposes of Building permits shall show all existing property pins and add a note to the plans to protect in place.
3. A Shared Access Agreement with the adjacent property and agreement for asphalt/concrete work proposed on adjacent property shall be submitted for Engineering with a Building Permit.
4. Apply for the Surface Transportation Assistance Act (STAA) Truck Route Designation
5. Plans submitted for the purposes of Building permits shall conform with the applicable requirements of SMC Section 15.44 for flood damage prevention: The finished floor of all structures shall be elevated a minimum of 2-feet above the base flood elevation as determined in accordance with SMC 15.44, all exterior equipment (transformer, generator, etc.) shall be elevated, and flood-zone boundaries shall be shown on the plans.

6. Landscape Plans submitted for the purposes of Building permits shall show Model Water Efficient Landscape Ordinance (MWELO) calculations.
7. A Stormwater Quality Control Plan (SWQCP) and WDID number for the State approved Stormwater Pollution Prevention Plan (SWPPP) shall be provided with the submittal of plans.
8. Plans submitted for the purposes of Building permits shall show an on-site private fire hydrant system and a Fire Department key switch. Building Permits for Off-site improvements shall be reviewed for additional fire hydrant placement.

Standard Conditions of Approval

9. Changes to this approval shall be reviewed under SMC Chapter 16.04 (Changes to an Approved Project).
10. Plans submitted for purposes of building permit(s) shall reflect compliance with the American Disabilities Act and standards at Development Code Table 2-3, including all other aspects of Municipal Code Title 16 (Development Code), as applicable.
11. The use shall be carried out in compliance with all applicable Federal, State, County and City codes, regulations and adopted standards and pay all applicable fees.
12. Compliance with these conditions is mandatory. Failure to comply with these conditions is unlawful, constitutes a public nuisance, and is subject to the remedies and penalties identified in the Stockton Municipal Code, including but not limited to, monetary fines and revocation or modification of said Design Review Approval.
13. This approval shall be come void unless the required building permit is submitted within 12 months of the approvals effective date. An extension may be requested in accordance with SMC §16.120.080(D).
14. All work performed under a building permit for which drawings and plans have been approved under the procedures and requirements of SMC §16.120.010 shall conform to the approved drawings and plans. The signed and/or stamped plans shall be submitted before, or at the same time as, the building permit application.

Conclusion

The Design Review is appealable to the Planning Commission in accordance with SMC §16.120.080(A) and Chapter §16.100 (Appeals). An appeal must have been made in writing to the Community Development Department within 10 days and accompanied by the requisite fee.

If you have any questions, please do not hesitate to contact me at francesca.carr@stocktonca.gov or (209) 937-8331.

Regards,



Francesca Carr
Assistant Planner, City of Stockton
Community Development Department

Attachments:

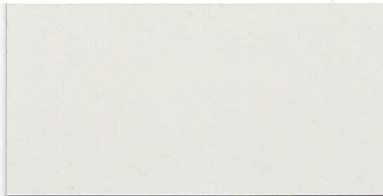
Exhibit 1 – Project Plans



BIG STANDARD COLORS



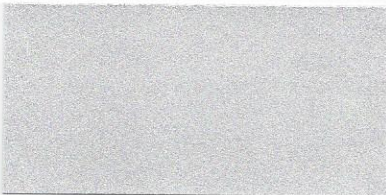
WHITE



LIGHT BEIGE



TERRA COTTA



BRIGHT SILVER - Added Cost. Please inquire for pricing



SUNTAN



BANNER RED



GRAY



DUST BROWN



FOREST GREEN



MARLIN BLUE



COCOA BROWN



MEDIUM BLUE



BLACK



BRONZESTONE



VANTAGE BLUE



ENTERPRISES, INC.

9702 East Rush Street
South El Monte, CA 91733-1730
P:(626) 448-1449 • F:(626) 448-3598
www.bigbooth.com
sales@bigbooth.com

Our High Solids Polyurethane is the most durable coating in the industry. This product features superior resistance to a wide range of chemicals and salt spray corrosion, tested exceeding 3500 hours. It offers outstanding color and gloss stability when exposed to sunlight and is extremely resistant to chipping and abrasion.



product
information

6400 / 340HP SERIES
HIGH SOLIDS POLYURETHANE

Cardinal's 6400 series catalyzed with 340HP is a high-solids aliphatic two-component polyurethane coating. This coating is well suited for exterior applications on both metal and plastic. The 6400 Series was formulated to meet strict air quality regulations, while maintaining the application and performance benefits of a conventional polyurethane coating. Cardinal's 6400 series high solids polyurethane coating is available in a full selection of color and gloss, including metallic, cardtex finish and clear.

TYPICAL USES:

- Top coat for decorative and protective use on metal and plastic
- General metal finishing
- Electronic enclosures
- Trailers and vehicles
- Machinery

BENEFITS:

- Low VOC – 2.8 lbs/gal
- Very high gloss
- Excellent chemical and solvent resistance
- UL approved (phosphatized steel electronics enclosures)
- Available in a complete range of colors, glosses, textures and cardtex finishes

CURED FILM PROPERTIES:

Testing conducted on 6409-10 gloss white catalyzed with 340HP at 1.5 mils DFT (Dry Film Thickness) over 20 gauge Bonderite 1000@ test panels, cured 30 minutes at 180°F and air dried 14 days.

TEST	METHOD	PARAMETERS	RESULT
Adhesion	ASTM D3359	Cross-hatch tape	0% failure
Impact:	ASTM D2794	Direct	130 in. lbs.
		Reverse	60 in. lbs.
Flexibility:	ASTM D1737	1/8" mandrel	No cracking
Hardness	ASTM D3363	Pencil	H - 2H
Abrasion	ASTM D4060	CS-17 wheels, 1 kg, 1000 cycles	Less than 100 mg loss
Humidity	ASTM D2247	168 hrs	No effect
Salt Spray	ASTM B117	1000 hrs 95°, 5% salt solution	Less than 3/16" creep - along scribe, otherwise, no effect
UV Light	ASTM G53	1000 hrs	90.3% gloss retention
Solvent Resistance	ASTM D4752	MEK 100 rubs	No effect
		IPA 200 rubs	No effect
Chemical & Stain Resistance	ASTM D1308 30 min. spot	A – 0.1N HCl, 30 wt. motor oil, ammonia, butyl carbitol, butyl cellosolve, Cascade®, Clorox®, Coca Cola®, coffee, diethyl ether, Drano®, Fantastic®, fiber pen ink, floor stripper, gasoline, IPA, Ivory® Liquid, lanolin lotion, lemon juice, Snap®, Spic & Span®, tap water, vegetable oil, water base ink, WD-40®.	
		B – ball point pen ink, carbon disulfide, correction fluid, Freon TF®, MEK, nail polish.	
		C – chloroform.	
		D – solvent base ink.	
		A: No effect B: Slight dulling C: Moderate effect D: Discolored & softened	

**FOR INDUSTRIAL USE ONLY
NOT FOR RESIDENTIAL USE**

TYPE: Aliphatic polyester polyurethane.

COMPONENTS: Two.

COLORS: Full range including Fed. Std. 595b.

GLOSS: High, semi and flat.

MINIMUM ORDER: 1 gal. of 6400 base; 1 qt. of 340HP.

COVERAGE: At 1.0 mil DFT, 65% transfer efficiency(TE)

Mixed paint, 2.8 lbs/gal : 620 ft²/gal.

Mixed paint, 3.5 lbs/gal : 520 ft²/gal.

Calculation: 1604 ft²/gal x % volume solids x TE + DFT

VOC MIXED: 340 grams/liter = 2.8 lbs/gal minimum.
420 grams/liter = 3.5 lbs/gal minimum.

See mix ratio table below.

VOLUME SOLIDS:

6400 gloss base 53%

340HP 87%

Mixed to 2.8 lbs/gal 60%

Mixed to 3.5 lbs/gal 49%

FLASH POINT: 24°F TCC

SHELF LIFE: 1 year from date of manufacture in factory sealed container.

APPLICATION: After preparing the surface, thoroughly mix component 1 before adding catalyst. Mix only the amount of material needed. The base to catalyst proportion must be measured accurately, by volume only, to obtain optimum film properties. Do not use reducers that contain water or alcohol; these react with the catalyst and can cause a variety of problems. Be aware of spray-able pot life. Brushing, rolling and dipping are not recommended.

MIX RATIOS: Two components must be mixed properly to obtain coating performance. Thinning depends on applicator's regulatory VOC limits.

Parts are by volume	COLORS GLOSS	COLORS SEMI GLOSS	CLEAR ALL GLOSS
6400 base	4	5	4
340HP catalyst	1	1	1
1600-0# reducer			
for 340 gms/l	0	1/2	N/A
for 420 gms/l	1	1-1/2	1

VISCOSITY: Will vary depending on color and gloss at a given VOC. At 2.8 lbs/gal, most semi gloss colors will be in the 25"-30" #3 Zahn range. At 3.5 lbs/gal, 28"-32" #2 Zahn can be expected for most colors.

SPRAY-able Pot Life: 2-3 hrs. at 2.8 lbs. VOC/gal
4-5 hrs. at 3.5 lbs. VOC/gal

RECOMMENDED DFT: 1.5 – 2.5 mils (depending on color)

CURE:	Air Dry	Force Dry *
Tack free	2 hrs.	1 hr at 120° F
Dry to handle	24 hrs.	30 min at 140° F
Dry hard	72 hrs.	15 min at 180° F
	(At 1.5 mils dry film thickness, 78° F, 50% RH)	

* Some Air quality regulations require a maximum temp. of 194° F to qualify as an "air dry" system which generally have higher VOC limits than baking systems.

Continued on page 2

So. El Monte, CA 1329 Polero Ave., 91733 • (323) 283-9335 • (626) 444-9274 • Fax: (626) 444-0382
 Phoenix, AZ 3816 E. Superior Ave., 85040 • (602) 437-2401 • Fax: (602) 437-9251
 San Jose, CA 890 Commercial St., 95112 • (408) 452-8522 • Fax: (408) 452-0318
 Denver, CO 1195 E. 64th Ave., 80229 • (303) 286-1876 • Fax: (303) 286-1878
 Lakeville, MN 21326 Heywood Avenue 55044 • (952) 469-6021 • Fax: (952) 985-5052
 Woodinville, WA 19230 144th Avenue N. E., 98072 • (425) 483-5665 • Fax: (425) 483-5401
 Maryland Heights, MO 44 Worthington Access Drive 63043 • (314) 878-3010 • Fax: (314) 878-0903
 Charlotte, NC 7403 North Tryon Street 28213 • (704) 596-0926 • Fax: (704) 596-8182
 Warren, PA 4 Harmer Street, 16365 • (814) 723-0721 • Fax: (814) 723-7556
 Dallas, TX 4606 Brass Way 75236 • (214) 333-9801 • Fax: (214) 333-9831

NO WARRANTY EXPRESSED OR IMPLIED, ACCEPTABILITY TO BE DETERMINED BY USER, SPECIFICATIONS ARE SUBJECT TO CHANGE WITHOUT NOTICE

SURFACE PREPARATION AND PRIMING: The most important steps in a successful coating process are cleaning, pretreatment and priming. The following is a brief outline of some basics for unpainted substrates. It is not intended to be all-inclusive. For more information on your particular application contact Cardinal.

Cleaning the substrate: All surfaces to be coated, must be free of dirt, grease, oil, oxidation, mill scale, and all other contaminants. The surface must be thoroughly dry before painting. Air quality regulations have limited the allowable emissions from cleaning operations.

Steel — A phosphate chemical conversion coating is highly recommended. When this is not possible, a vinyl acid wash pretreatment primer is recommended such as Cardinal's 4860 series primers. UL approval on our product requires the minimum of a three stage iron phosphate pre-treatment.

Aluminum — A chemical conversion coating is highly recommended. When this is not possible, a vinyl acid wash pretreatment primer is recommended such as Cardinal's 4860 series primers.

Galvanized — Cardinal's W-303-A surface preparation solution helps improve adhesion followed by a vinyl acid wash pretreatment primer such as Cardinal's 4860 series primers.

Stainless Steel — Brush-off or blast clean per SSPC-SP 7 to a uniform profile of 1.5 mils. Cardinal's W-303-A surface preparation solution can help improve adhesion followed by a vinyl acid wash pretreatment primer such as Cardinal's 4860 series primers.

Plastic — All mold release should be completely removed. 6400 series polyurethane is compatible with a variety of plastics, however, since there are numerous different formulations of plastic, a trial sample should be painted and checked before running production. If 6400 attacks or weakens the plastic, a barrier coat of 3777-1 clear waterborne acrylic enamel may help.

PRIMER SELECTION:

PRODUCT NO.	DESCRIPTION	FUNCTION
6460-4702	Polyurethane Gray	Corrosion resistance, some surfacing
7063-20	Epoxy Mastic Gray	Very high build, corrosion resistance, abrasion resistance, chemical resistance
7160-4702	Epoxy Gray	Corrosion resistance, chemical resistance
3777-1	Waterborne Acrylic Clear	Barrier coat for some plastics

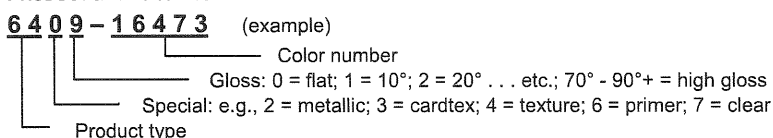
RELATED PRODUCTS:

PRODUCT NO.	DESCRIPTION
1600 Series Reducers	Thinners. Urethane grade. 1600-01, fast; 1600-02, medium; 1600-03, slow; 1600-06, very slow.
EL-005	Accelerator. Speeds up dry time (and shortens pot life).
J-3081	Surfactant. Helps eliminate blisters, bubbles, pin holes, solvent-pop.
P-5033	Surfactant. Helps eliminate craters and fish-eyes.

TROUBLE SHOOTING:

PROBLEM	CAUSE	REMEDY
Blisters, pin holes or solvent pop	Water contamination. Entrapped air. Entrapped solvent	Eliminate water – Check air lines. Use fresh catalyst. Use urethane grade thinners. Increase atomization, decrease film build.
Craters	Contaminated ambient air, e.g., silicone mist, dust.	Locate and eliminate source of contamination.
Fish-eyes	Substrate contamination.	Clean and prepare substrate.
Not drying	Alcohol in reducer. Wrong catalyst ratio.	Use Cardinal's 1600 series or urethane grade reducers only. Double check mix ratio.
Poor adhesion	Improper surface preparation.	See surface preparation section.
Gloss variation	Variation in application, cure schedule, catalyst ratio, humidity.	Consistent gloss depends upon consistent process.

PRODUCT IDENTIFICATION



APPLICATION EQUIPMENT: Most air quality regulations require the paint application transfer efficiency to be 65% or better. This generally means using electrostatic or high volume low pressure (HVLP) spray guns. Otherwise, conventional pressure feed, airless or air assisted airless spray equipment can be used. Air supply lines need water and oil traps.

EQUIPMENT CLEAN-UP: Clean up should be done as soon as possible keeping in mind the pot life of the mixed paint. Avoid leaving catalyzed paint in the lines. Air quality regulations have limited the allowable emissions from cleaning operations.

PRODUCT LIMITATIONS:

- Catalyst reacts with water. Air supply should be dry. Containers should be kept tightly closed. Use urethane grade thinners only.
- Alcohols and glycols interfere with curing chemistry and should be avoided. They can be found in some lacquer thinners and certain synthetic reducers.
- Optimum film properties are dependent upon proper mixing of paint and catalyst.

SAFETY: Refer to the product's Material Safety Data Sheet (MSDS) for complete safety information. Contains organic solvents. Use with adequate ventilation. Do not breathe vapors or spray mists. If component TLVs are exceeded, a NIOSH approved air supplied respirator is advised. See MSDS for TLV information.

Contents are FLAMMABLE. Keep from heat, sparks or open flame.

Allergic reactions are possible. Avoid use by persons with respiratory problems.

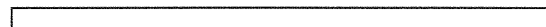
Avoid contact with eyes, skin, and clothing. Wash thoroughly after handling.

FIRST AID:

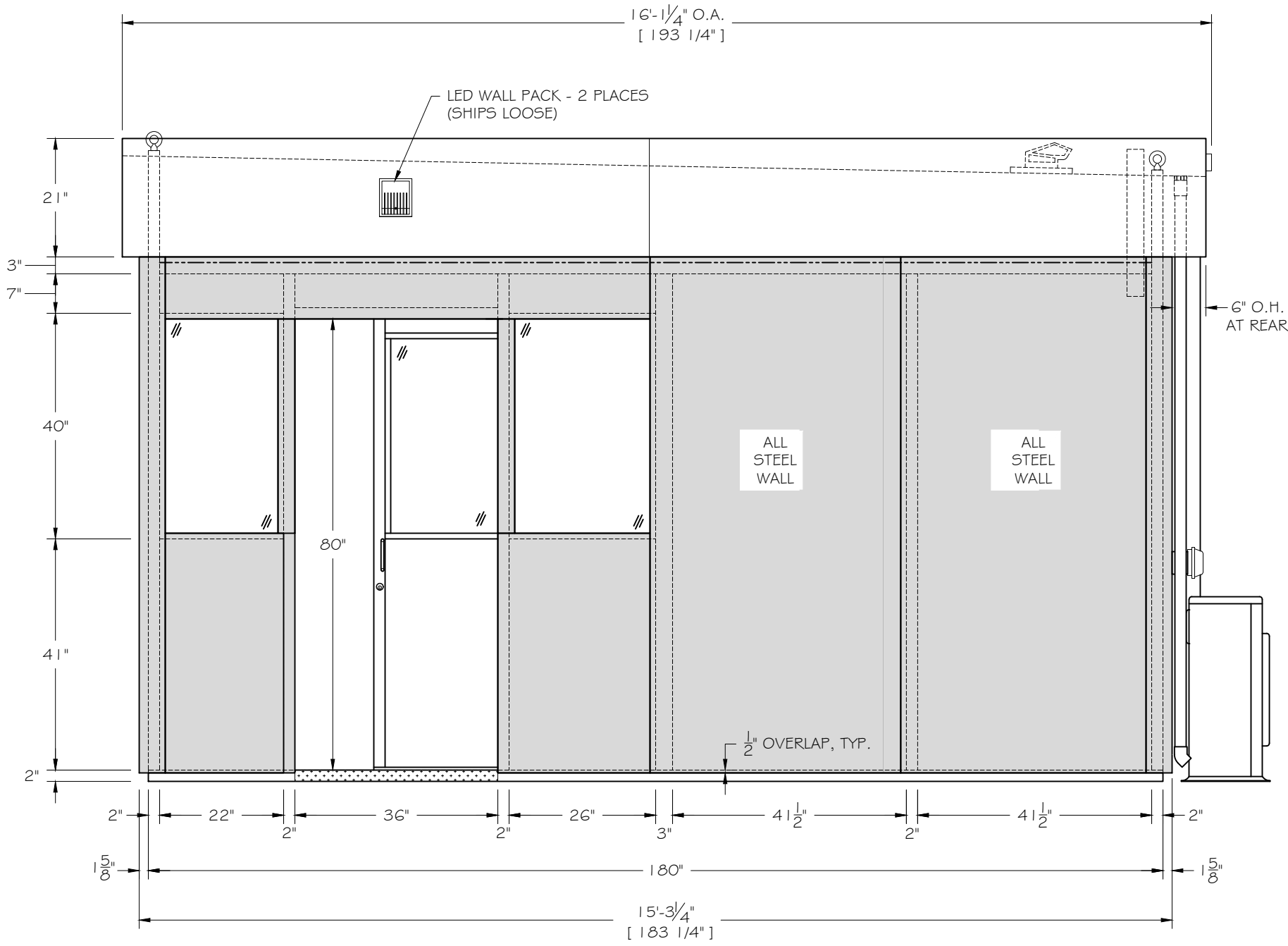
Eye contact: flush immediately with plenty of water for at least 15 min. and get medical attention.

Skin contact: wash thoroughly with soap and water for 5 minutes.

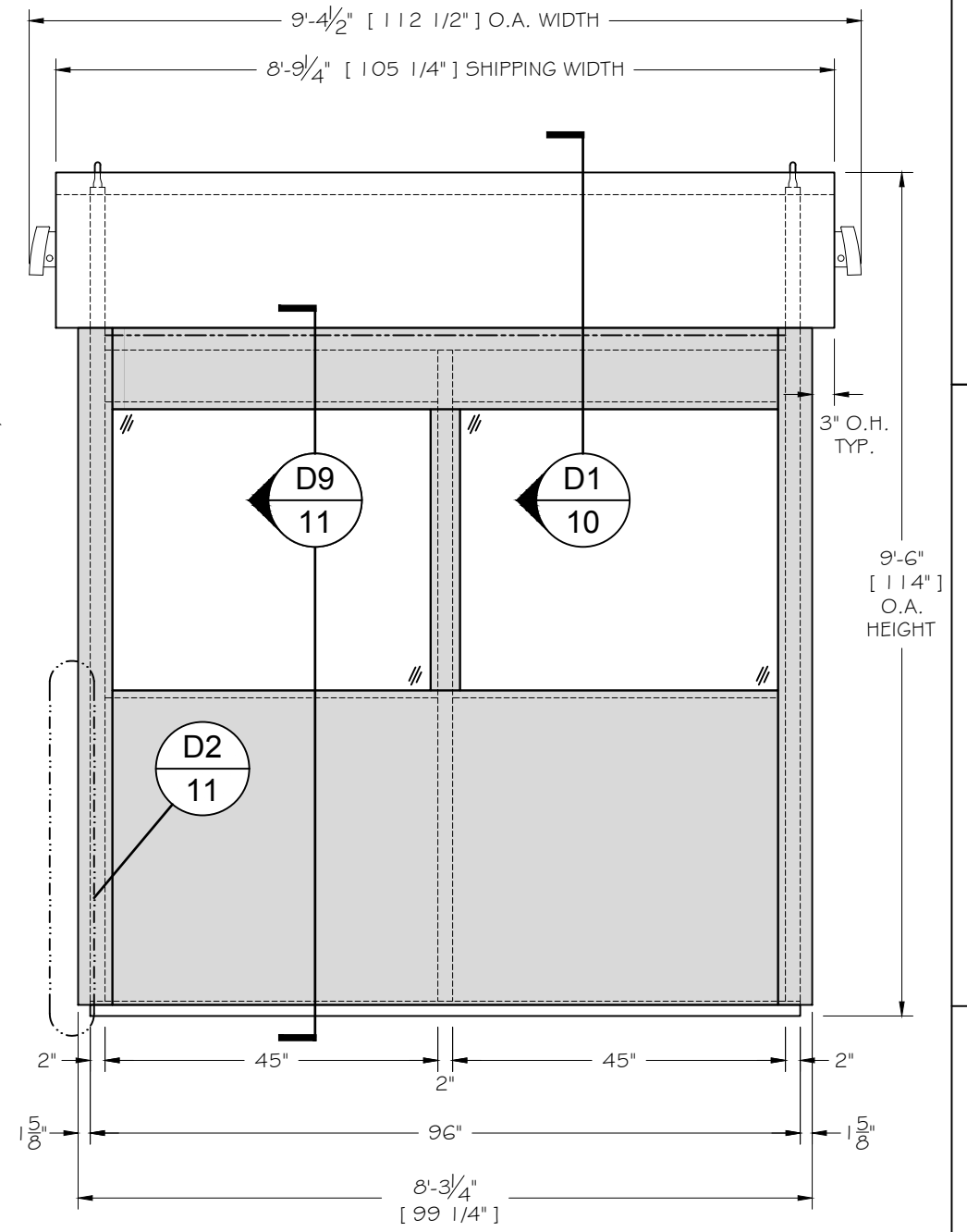
If swallowed, do not induce vomiting and get medical attention immediately.



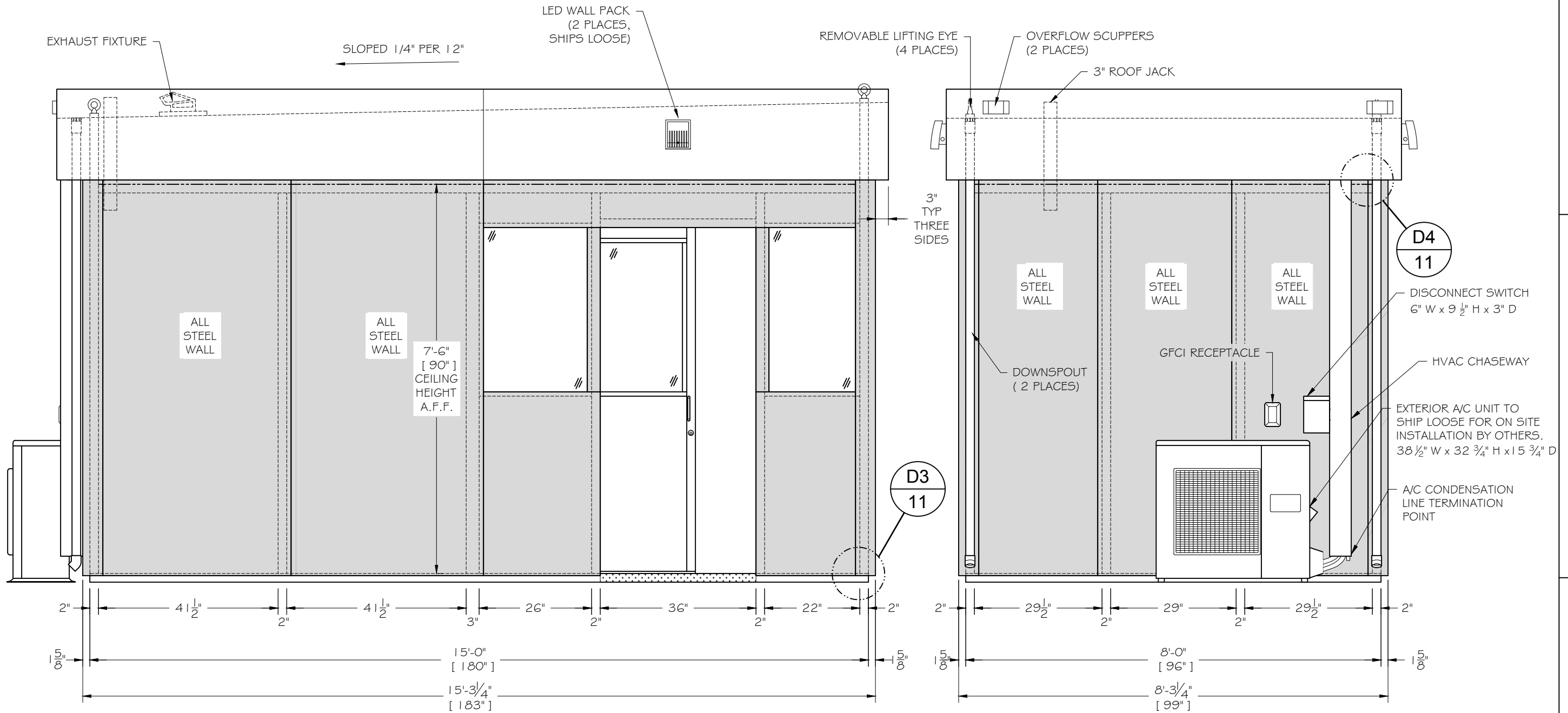
IMPORTANT: Warranty and Disclaimer — The performance characteristics of these products vary according to product application, operating conditions, materials applied to or with and use. Since these factors can affect results, we strongly recommend that you make your own test to determine to your satisfaction whether the product is of acceptable quality, has not been affected by storage or transport and is suitable for your particular purpose under your own operation conditions prior to using any product in full scale production. Seller warrants the products to be free from defects in materials and workmanship. SUCH WARRANTY IS EXCLUSIVE AND IS IN LIEU OF ANY OTHER WARRANTY, EXPRESSED OR IMPLIED, INCLUDING, BUT NOT LIMITED TO, ANY IMPLIED WARRANTY OF MERCHANTABILITY OF FITNESS FOR A PARTICULAR PURPOSE. No representative of ours has authority to waive or change this provision, which applies to all sales of these products.



E1 ELEVATION
SCALE: 1/2" = 1' - 0"



E2 ELEVATION
SCALE: 1/2" = 1' - 0"



E3 ELEVATION
SCALE: 1/2" = 1' - 0"

E4 ELEVATION
SCALE: 1/2" = 1' - 0"

Exhibit 1

- 1 36" x 80" SLIDING DOORS - GENERAL NOTE D 1
- 2 22" SHELF - GENERAL NOTE A 1
- 3 3" CORD ACCESS HOLE (3 TOTAL)
- 4 RECEPTACLE (3 TOTAL) - GENERAL NOTE E 6
- 5 JUNCTION BOX (2 TOTAL) - GENERAL NOTE E 7
- 6 1' x 4' LED LIGHT FIXTURE - GENERAL NOTE E 2
- 7 OCCUPANCY SENSOR
- 8 LOAD CENTER - GENERAL NOTE E 1

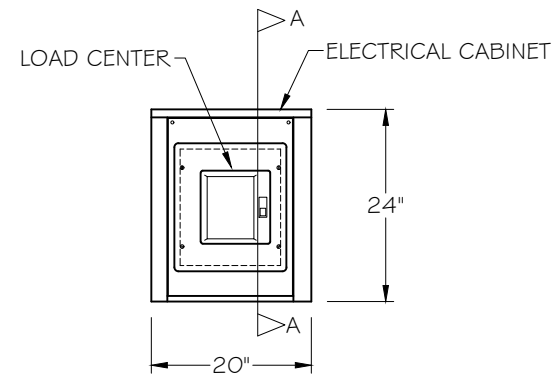
- 9 ELECTRICAL CABINET
- 10 LED WALL PACK (2 TOTAL) - GENERAL NOTE E 6
- 11 EXTERIOR LIGHT SWITCH - GENERAL NOTE E 5
- 12 INTERIOR HVAC UNIT - GENERAL NOTES E 10, E 1 1
- 13 THERMOSTAT
- 14 EXTERIOR HVAC UNIT - GENERAL NOTES E 10, E 12, E 13
- 15 1 6 GA. HVAC CHASE
- 16 1' x 4' LED LIGHT FIXTURE (RESTROOM) - GENERAL NOTE E 3

- 17 OCCUPANCY SENSOR
- 18 WATER HEATER J-BOX - GENERAL NOTE E 9
- 19 GFCI RECEPTACLE (RESTROOM) - GENERAL NOTE E 8
- 20 3" ROOF JACK - GENERAL NOTE RR 1
- 21 GRAB BAR (2 PLACES) - GENERAL NOTE RR
- 22 DOWNSPOUT (2 TOTAL)
- 23 ANCHOR CLIPS (4 TOTAL) - GENERAL NOTE 95
- 24 EXHAUST FAN - GENERAL NOTE RR 1

- 25 OVERHANG
- 26 WINDOWS - GENERAL NOTES W 1
- 27 INTERIOR STEEL DOOR W/COAT HOOK
- 28 12" X 12" SUPPLY GRILLE - GENERAL NOTE E 1 1

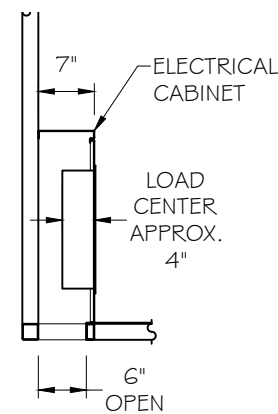
NOTE:

ALL RESTROOM EQUIPMENT TO BE SHIPPED LOOSE FOR INSTALLATION ON SITE BY OTHERS. B.I. G. TO PROVIDE BRACING HARD POINTS FOR SINK AND ANY REQUESTED GRAB BARS. ANY OTHER BRACING POINTS OR CUTOUTS REQUIRED MUST BE SPECIFIED PRIOR TO B.I. G. PRODUCTION RELEASE.



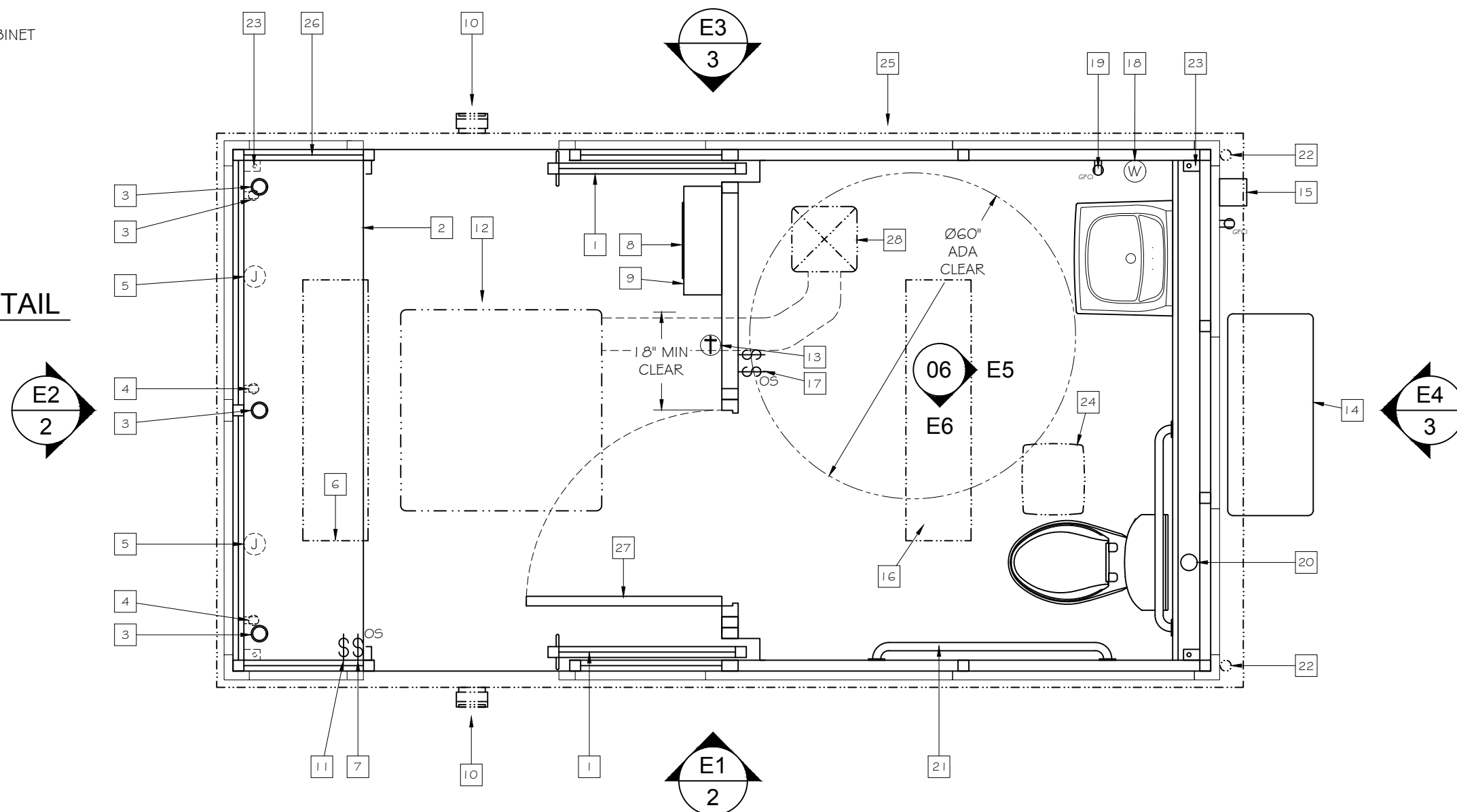
ELECTRICAL CABINET DETAIL

SCALE: 1/2" = 1' - 0"



SECTION A-A

SCALE: 1/2" = 1' - 0"



FLOOR PLAN

SCALE: 1/2" = 1' - 0"

SHEET
4
OF
12

JOB No:10983

BY: JRM

DATE: 21-DEC-20

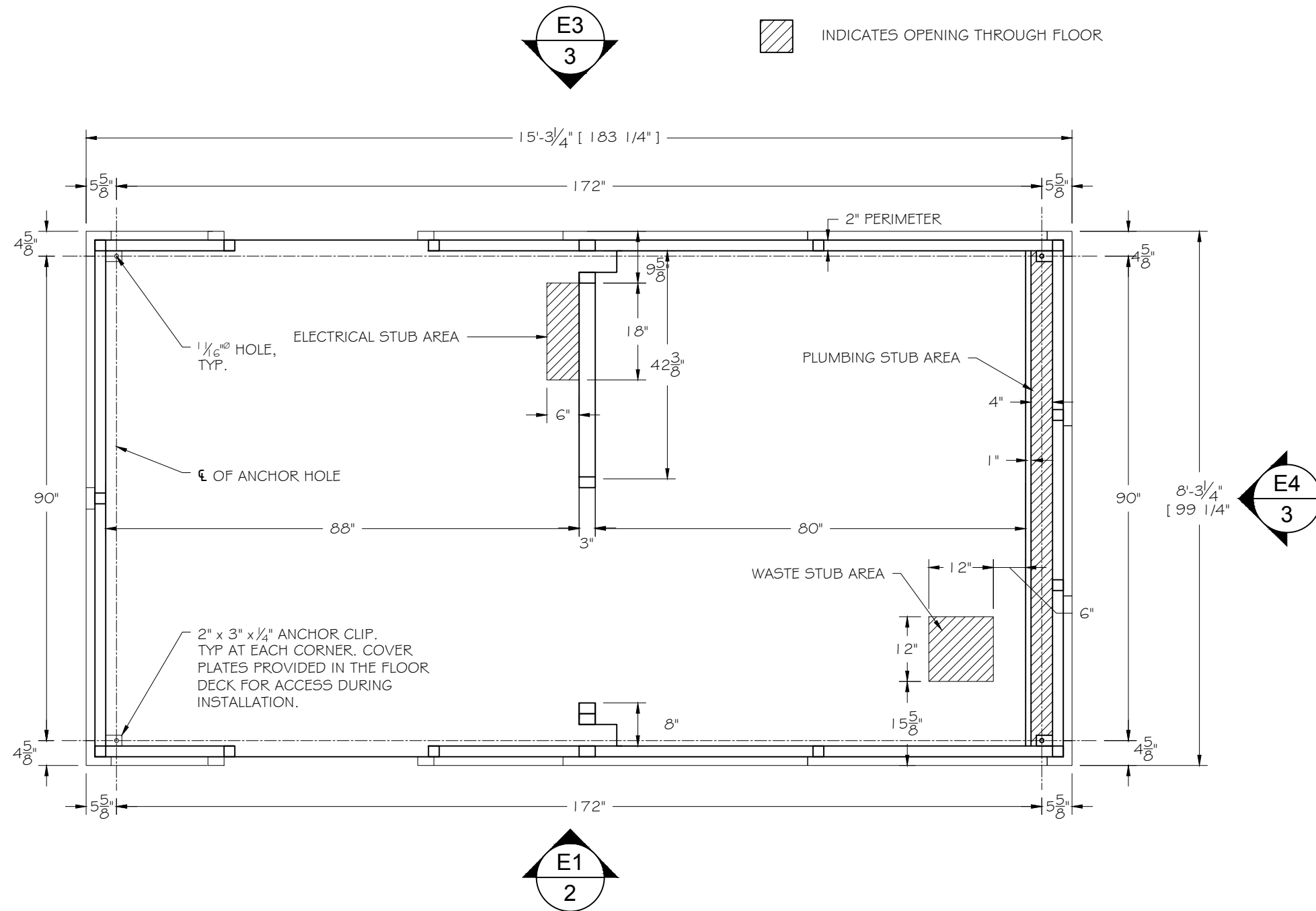
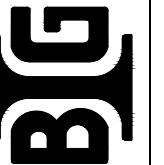
SCALE: AS NOTED

FULLMER CONSTRUCTION

CLARION TRAILER YARD GUARD
BOOTH

B.I.G. ENTERPRISES, INC.
9702 E. RUSH STREET
SOUTH EL MONTE, CA 91733-1730
(626) 448-1449





STUB UP AREA / ANCHOR PLAN

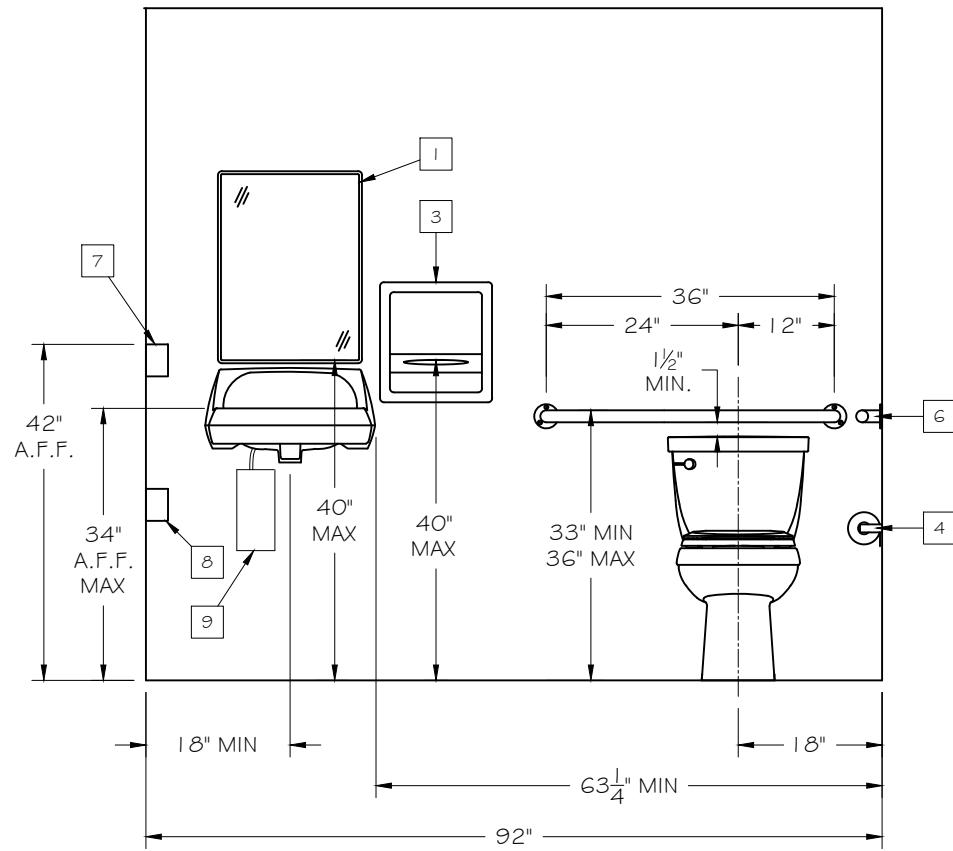
SCALE: 1/2" = 1' - 0"

Exhibit 1

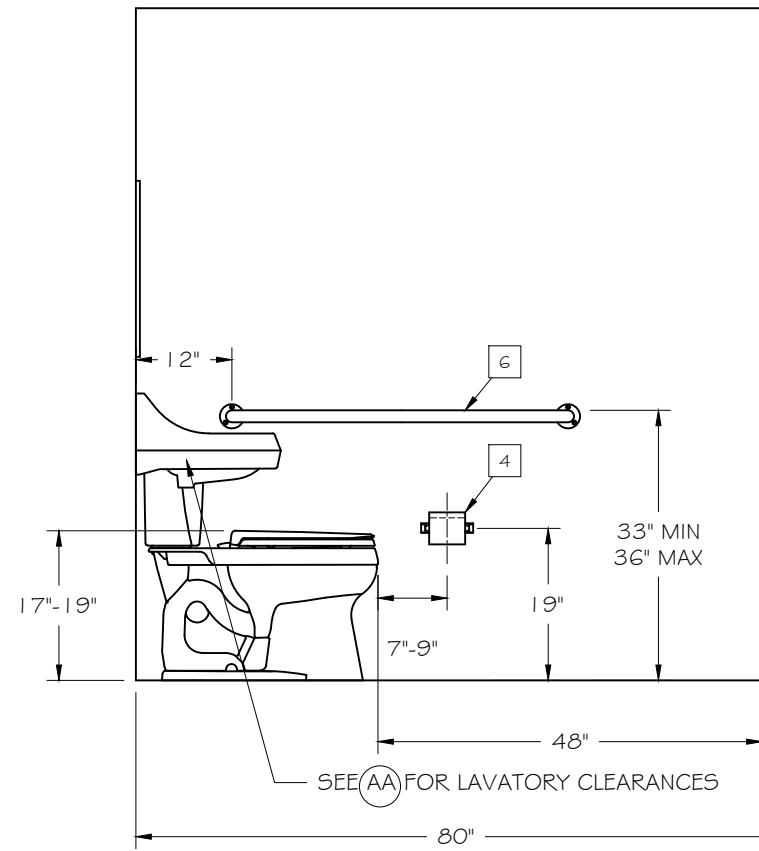
- | | | |
|-------------------------|-----------------------|-----------------------------|
| 1 16" x 22" MIRROR | 4 TOILET PAPER HOLDER | 7 GFCI RECEPTACLE |
| 2 36" GRAB BAR | 5 WATER HEATER | 8 WATER HEATER JUNCTION BOX |
| 3 PAPER TOWEL DISPENSER | 6 42" GRAB BAR | 9 SOAP DISPENSER |

NOTE:

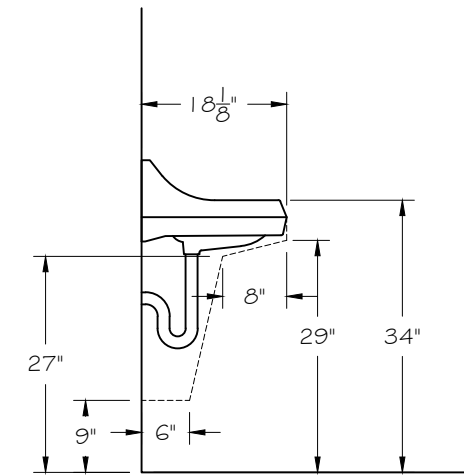
ALL RESTROOM EQUIPMENT TO BE SHIPPED LOOSE FOR INSTALLATION ON SITE BY OTHERS. B.I. G. TO PROVIDE BRACING HARD POINTS FOR SINK AND ANY REQUESTED GRAB BARS. ANY OTHER BRACING POINTS OR CUTOUTS REQUIRED MUST BE SPECIFIED PRIOR TO B.I. G. PRODUCTION RELEASE.



E5 ELEVATION
SCALE: 1/2" = 1' - 0"



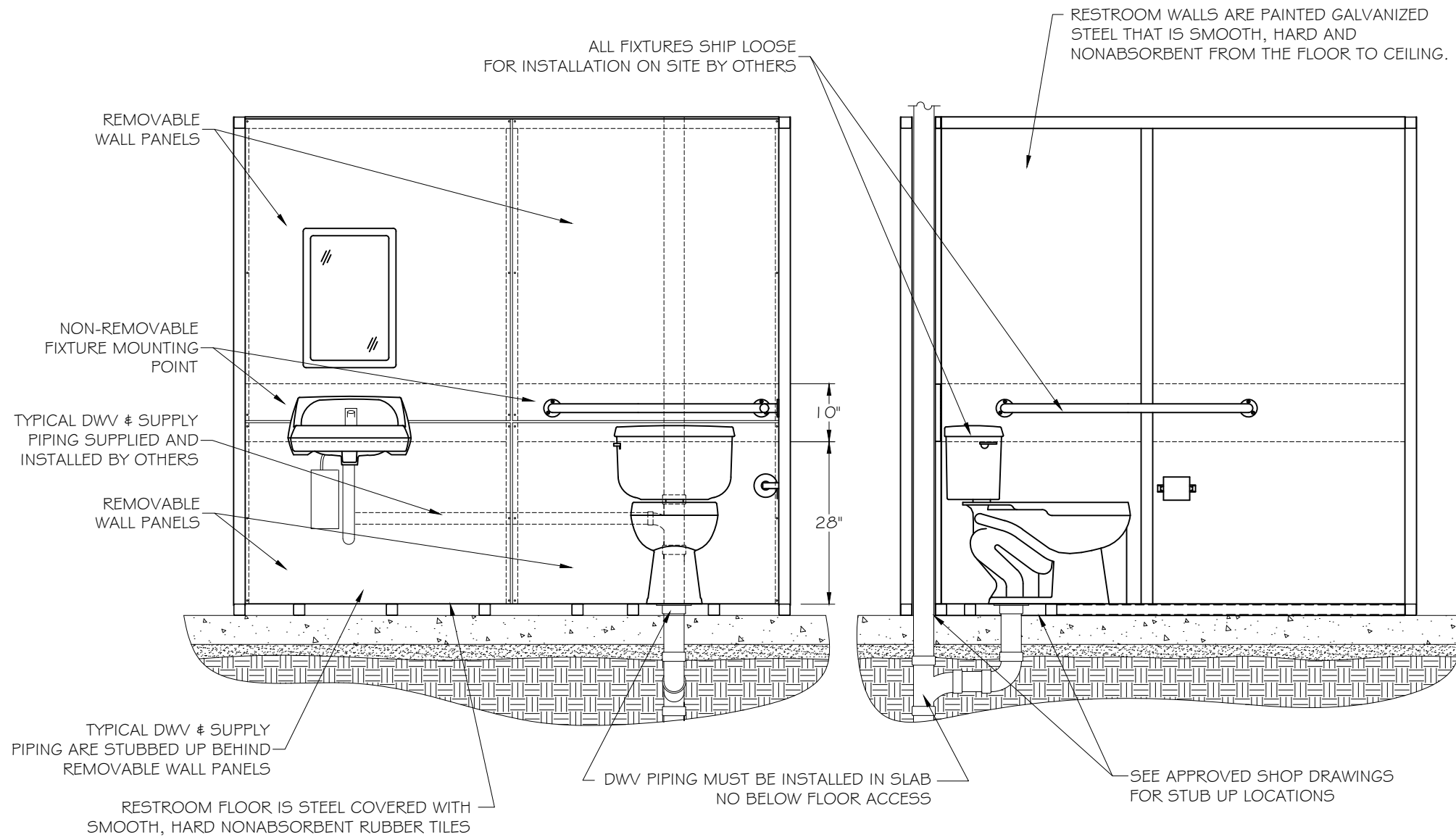
E6 ELEVATION
SCALE: 1/2" = 1' - 0"



AA LAVATORY CLEARANCES
SCALE: 1/2" = 1' - 0"



Exhibit 1



PLUMBING FIXTURE COUNT			
FIXTURE	QTY	DRAIN UNITS	EXTENDED
WATER CLOSET	1	4	4
LAVATORY	1	1	1
TOTAL DRAIN UNITS			5

INLET SIZES			
FIXTURE	QTY	FLOW RATE	EXTENDED
WATER CLOSET	1	1.28 GPM	1.28 GPM
LAVATORY	1	2.0 GPM	2.0 GPM
TOTAL FLOW RATE			3.28 GPM

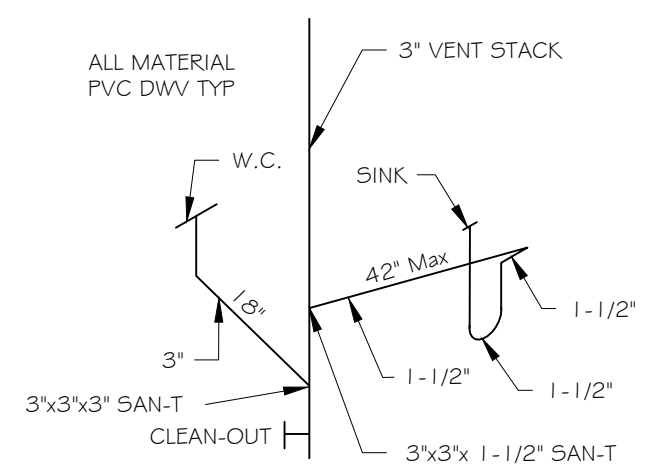
WATER SUPPLY FIXTURE UNITS			
FIXTURE	QTY	WSFU	MIN. BRANCH PIPE SIZE
WATER CLOSET	1	2.5	1/2"
LAVATORY	1	1.0	1/2"
TOTAL		3.5	3/4"

System will require a minimum pressure range of 30 to 45 psi with a maximum development length of 600 ft with a 3/4" dia. supply.

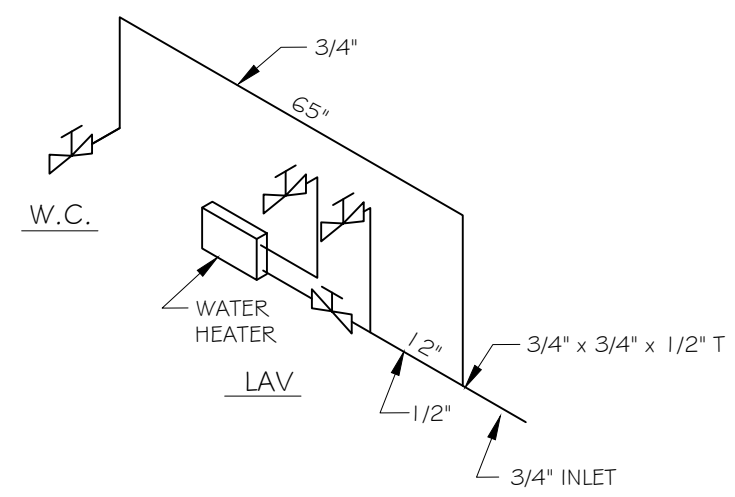
NOTE:
THIS DRAWING IS MEANT TO SHOW THE TYPE OF PLUMBING WORK REQUIRED TO CONNECT THE GUARD HOUSE. ACTUAL PLANS, PERMITS AND INSTALLATION ARE BY OTHERS.

TYPICAL PLUMBING INSTALLATION

SCALE: 1/2" = 1' - 0"



SANITARY STACK DIAGRAM
(BY OTHERS ON SITE)



WATER RISER DIAGRAM
(BY OTHERS ON SITE)

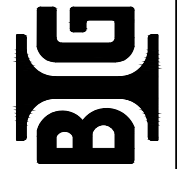
SHEET 7 OF 12

JOB No: 10983

BY: JRM
DATE: 21-DEC-20
SCALE: AS NOTED

FULLMER CONSTRUCTION
CLARION TRAILER YARD GUARD BOOTH

B.I.G. ENTERPRISES, INC.
9702 E. RUSH STREET
SOUTH EL MONTE, CA 91733-1730
(626) 448-1449



December 21, 2020

Exhibit 1

LEGEND	
	125v DULPEX OUTLET
	125v GFCI DULPEX OUTLET
	125V WEATHER-RESISTANT GFCI DUPLEX RCPT. w/ WEATHERPROOF COVER
	WATER HEATER J-BOX
	THERMOSTAT
	DISCONNECT SWITCH, UN-FUSED
	OCCUPANCY SENSOR
	EXTERIOR LIGHT SWITCH
	SWITCH LEG

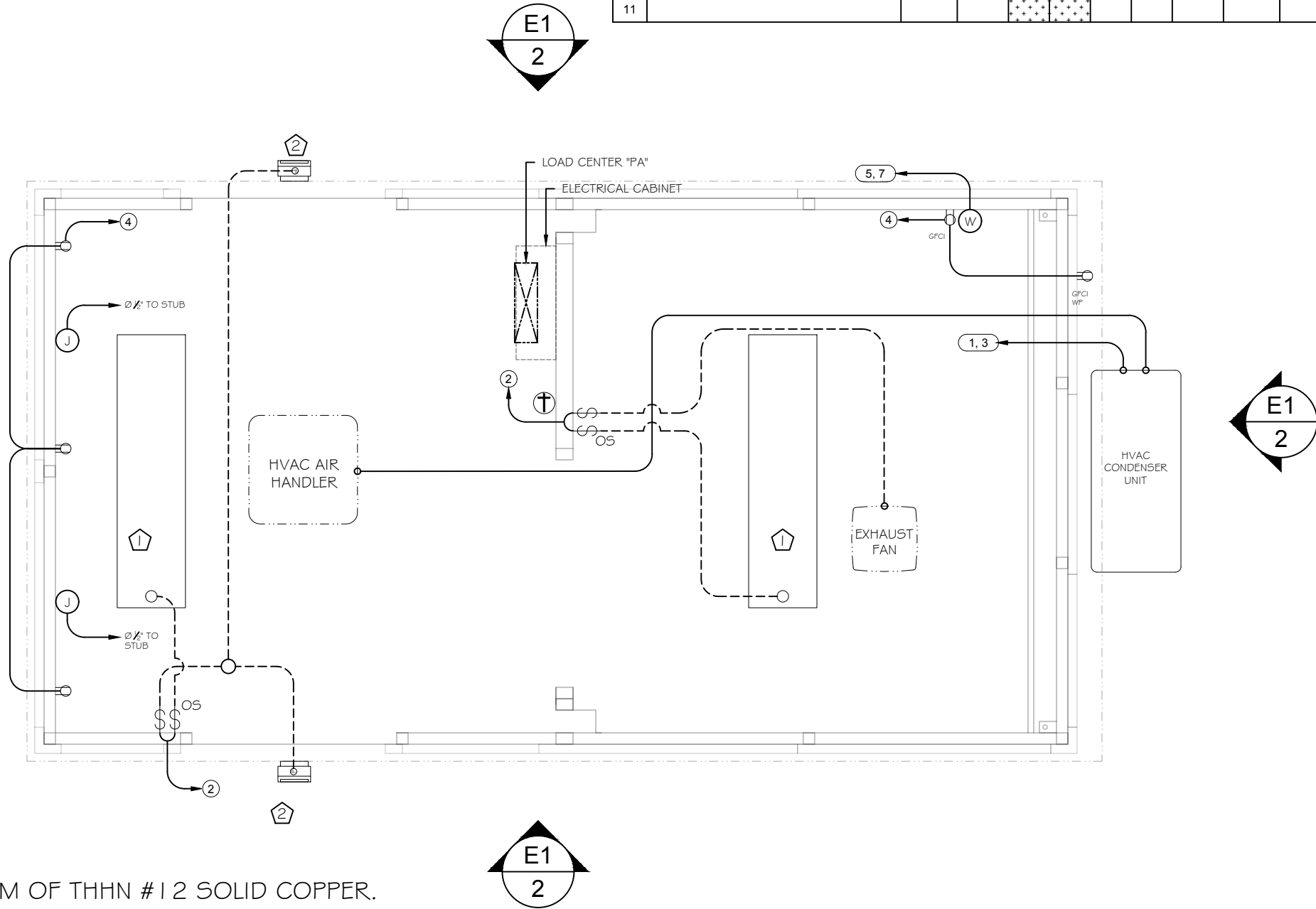
LIGHTING SCHEDULE		
①	1' x 4' LED FIXTURE	MAXLITE MLFP14EP40 40W LED
②	EXTERIOR LED WALL PACK	Lumark XTROR1B-PC1 12W LED

PANEL NAME: GB1		VOLTS: 120/240		MAIN TYPE: MAIN LUG					
MOUNTING: FLUSH		PHASE: SINGLE		BUS RATING: 125A					
ENCLOSURE: NEMA 1		WIRES: THREE		MCB RATING: NONE					
CKT	CIRCUIT DESCRIPTION	C.B. AMPS	C.B. POLES	A WATTS	B WATTS	C.B. POLES	C.B. AMPS	CIRCUIT DESCRIPTION	CKT
1	HVAC	25A	2	1035	145	1	20A	LIGHT/EXTERIOR LIGHTS/EX. FAN	2
3	HVAC			1035	540			(3) RECEPTACLES, FRONT	4
5	WATER HEATER	20A	2	1750	360	1	20A	(2) RECEPTACLES, GFCI	6
7	WATER HEATER			1750					8
9									10
11									12

JOB No:10983
 SHEET 8 OF 12
 BY: JRM
 DATE: 21-DEC-20
 SCALE: AS NOTED

FULLMER CONSTRUCTION
 CLARION TRAILER YARD GUARD BOOTH

B.I.G. ENTERPRISES, INC.
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 SOUTH EL MONTE, CA 91733-1730
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- NOTE:
1. ALL WIRING TO BE A MINIMUM OF THHN #12 SOLID COPPER.
 2. ALL CIRCUITS TO BE GROUNDED.
 3. ALL STEEL EQUIPMENT AND EXPOSED STRUCTURAL STEEL TO BE BONDED TO ELECTRICAL SYSTEM.

ELECTRICAL PLAN
 SCALE: 1/2" = 1' - 0"

GENERAL NOTES:

STRUCTURAL:

- S1. Frame members to consist of 2" x 3" x .083" and 3" x 3" x .083" steel tube.
- S2. Unit to have half glazed 16 ga galvanized steel walls with 16 ga galvanized interior liners.
- S3. Walls to have a R-9.6 insulation value with a layer of continuous R-9.6 insulation encapsulated in an exterior 16 ga galvanized steel wall. Overall insulation value is R-19.2.
- S4. Unit to have a 12 ga A653 galvanized steel floor covered with 1/4" thick x 20" square, black commercial polyvinyl chloride interlocking tiles with a 4" high black perimeter cove molding. Floor to be mounted on a 2" tall galvanized steel tube frame.
- S5. Unit to have four interior anchor clips. Four 5/8" Hilti KB-TZ bolts (LARR 25701, ICC-ESR-1917) with 3 1/8" minimum embedment in 2500 psi concrete (or equal) are required for placement of booth (Anchor bolts are not included). Concrete to be level. If sloped, slope towards drain opening(s) on booth. Concrete and booth installation shall be by others. For a 3-1/8" embedment the foundation/slab design must be a minimum of 5" thick and be sized to allow for a minimum distance of 6-1/2" from the edge of the slab to the center line of the anchors for maximum strength.
- S6. Floor will require a ramp the full width of one door opening with a slope of 1" to 1'-0" to meet ADA handicap access requirements (ramp by others onsite) or alternatively the booth can be recessed 2" into the slab.

CAUTION: Please contact your sales representative for additional information when planning on recessing the booth into the slab.

ROOF:

- R1. Unit to have a 21" tall fascia, 3" overhang on front & sides, 6" on rear with a 16ga galvanized steel roof sloped to drain through two rear mount 1-1/2" PVC downspouts and overflow scuppers.
- R2. Roof to have a R-19 insulation value.
- R3. Unit to have four removable lifting eyes (3/4" dia. shank, eye is 1-1/2" I.D.) mounted on the roof. Lifting eyes are designed for a STRAIGHT VERTICAL LIFT ONLY. Spreader bars must be used when lifting booth to ensure this vertical lift. All warranties will be void if not lifted in this manner.

DOORS & WINDOWS:

- D1. Unit to have two 36" x 80" commercial grade steel framed slide doors constructed of 18 ga steel panels with steel tube frames. Doors to be top hung, in a steel track, on a minimum of eight 2 1/4" steel ball bearing rollers with stainless steel lower guide, mortise hook type lock, stainless steel pull handles welded on to the frame, and complete weather stripping. Doors to be keyed alike.
- W1. Unit to have 3/4" dual pane insulated tempered, safety glass with a make up of 1/4" tinted, 1/4" air space, 1/4" clear with Low-E coating.
- W2. Customer to specify the color of tint prior to B.I.G. production release. Standard tint colors are Bronze or Gray.
- W3. If customer does not provide a tint selection, B.I.G. will choose a standard tint color that best complements the paint color selected.

ELECTRICAL:

- E1. Unit to have one 3 wire single phase, 12 pole, 125 amp 120 / 240 volt, load center flush mounted in a SK60 cabinet. All electrical equipment to be U.L. listed and all wiring to be per current published NEC standards. All conductors to be copper, with a minimum size of #12. Exposed wiring to be in surface mounted EMT conduit. Concealed wiring to be in flexible aluminum conduit. Final power connection and grounding to be done on site by others. All work to be done by a qualified electrician in accordance with all applicable local codes.
- E2. Unit to have one 1' x 4' LED light fixtures, recessed in the acoustic tile drop ceiling in the guard area, controlled by a occupancy sensor (manual on / auto off).
- E3. Unit to have one 1' x 4' LED light fixtures, recessed in the acoustic tile drop ceiling in the restroom area, controlled by a occupancy sensor (manual on / auto off).

ELECTRICAL:

- E4. The ceiling is to be an acoustic tile ceiling. The "T" grid will be painted the same color as the booth interior and the tiles will be white. Unit to have a ceiling height of 90" A.F.F.
- E5. Unit to have a 12w LED wall pack light mounted above each door on the fascia controlled by a single pole switch and integral photocell. Light to ship loose for installation on site by others. Fixture to be Lumark model XTRORIB-PC1 or equal.
- E6. Unit to have three 125 volt duplex outlets mounted under the shelf.
- E7. Unit to have two single gang junction boxes mounted under the shelf. Each junction box is to have an empty 1/2" conduit, with pull string, run to the electrical stub up area for data/communication lines by others.
- E8. Unit to have a GFCI duplex outlet mounted in the restroom at 42" A.F.F.
- E9. Unit to have a junction box mounted in the restroom. Junction box is to be wired for 240V, and is provided for the electrical connection of the water heater. The water heater is shipped loose, and is to be installed as part of the overall plumbing installation, by others.
- E10. Unit HVAC system to be a ductless split system providing a minimum of 24,000 BTU heating and 22,000 BTU cooling. A/C to have a SEER of 15.0 and R-410A refrigerant. A/C to be Fujitsu model no. AOU24RGLX (exterior unit) and AUU24RGLX (interior unit) or equal.
- E11. The HVAC interior air handler unit, refrigerant lines, condensate drain line, and electrical to ship fully installed in a chase to the exterior. Chase to be 16ga galvanized steel painted to match the booth. Interior air handler unit to have duct supply air to restroom through a 12" x 12" grille, painted to match booth interior.
- E12. The HVAC exterior unit will ship loose. Final installation of the HVAC system including: connection of refrigerant lines to the interior and exterior units, evacuating & charging, programming the thermostat, and diagnostics test must be done on site. Installation and maintenance of the entire system must be performed by a certified HVAC contractor in order to maintain warranty coverage.
- E13. The HVAC exterior unit will include a disconnect switch & a GFCI receptacle for servicing.

ACCESSORIES:

- A1. Unit to have one 22" deep front shelf with 3" cord access holes with grommets for drop down cords. Shelf to be 16ga steel, painted the same color as the booth interior and mounted at 34" above finished floor.
- A2. Shelf to be rated for a 250 lb. load.

RESTROOM:

- RR1. Unit to have restroom area with partition wall, all steel swing door with privacy lock, exhaust fixture (min 80 cfm, Broan #A80 or equal) and one installed 3" roof jack with 3" PVC tube vent stubbed 6" below ceiling. Verification of vent sizing, and vent installation to be by others in coordination with the local applicable codes.
- RR2. All piping is to be supplied and installed on site by others. The water closet shall not exceed 1.28 GPF. The lavatory flow rate shall not exceed 0.5 gpm at 60 psi. Total flow shall not exceed 0.25 gallons per activation.
- RR3. The following equipment (or equals) will be shipped loose for on site installation by others:
 - Standard lavatory: Kohler #K2005-0
 - Coat hook, double: Bobnck S.S., B-6727
 - Faucet set: Delta 86T 1153
 - Soap dispenser: Bobnck S.S., B-2111
 - Toilet: Kohler #K3609
 - Mirror: 18" x 24" Bobnck #B-1651 1824
 - Toilet seat: PROFLO-PFTSCOF2000WH
 - Water Heater: 240v 3.5KW EEMAX SP35
 - 42" Grab Bar: Bobnck S.S. 6806-42
 - Paper towel holder: Bobnck S.S. B-262
 - 36" Grab Bar: Bobnck S.S. 6806-36
 - TP holder: Bobnck S.S. B-6857

WEATHER PROOFING & FINISH:

- F1. All exposed steel surfaces except roof deck to be coated with two component high solids polyurethane rust inhibitive primer, and two component high solids polyurethane finish coat.
- F2. Unit to be painted one color inside and out, per Customer selection.
- F3. Roof to be fully weather sealed with a three step elastomeric membrane as follows:
 - A. All seams are sealed with a high bond seam sealing tape.
 - B. The entire roof deck is then coated with a liquid applied one-part polyurethane coating forming a tough waterproof, weather-resistant elastomeric coating.
 - C. The entire deck surface finished with a white heat reflective polyurethane coating. The coating meets Energy Star reflectance and emissivity performance requirements, has an SRI of 95, and is approved by the Cool Roof Rating Council (CRRC).

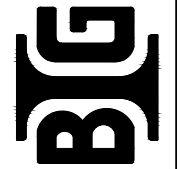
Booth Width in Inches	96
Booth Length in Inches	180
A. Lighting Loads	
Continuous Lighting Load	
120.0 ft ² x 3.5 VA / ft ² x 125% =	525.00
Actual Interior Lighting Load	
Total VA of all interior light fixtures = 80.00	
Exterior Lighting Load	
12 VA x 2 Fixtures x 125%	30.00
Larger of Continuous or Actual load + Exterior Load = 555.00	
B. Receptacle Loads	
Non-continuous Duty	
5 (No. of recep) x 180 VA =	900.00
Continuous Duty	
0 (No. of recep) x 180 VA x 125%	0.00
C. Heating, A/C & Motor Loads	
Larger of heating or A/C Load	
3542 VA x 125% =	4427.50
Motor Loads (Use VA rating on motor if provided)	
Continuous Motor Loads	
0 A x 0 V x 125% =	0.00
Non-Continuous Motor Loads	
Exhaust Fan 0.5 A x 120 V =	60.00
D. Largest Motor Load	
Largest motor load from C	
0 VA x 25% =	0.00
E. Additional Loads	
Nameplate Loads for additional continuous loads provided with building x 125%	
0 VA x 125% =	0.00
Nameplate Loads for additional non-continuous loads provided with building	
Water Heater 3500 VA =	3500.00
F. Total Loads	
Total VA	9442.50
Total Va 9443 / 240 V =	39.34 Total Amps

SHEET
12
OF
12

JOB No: 10983
BY: JRM
DATE: 21-DEC-20
SCALE: AS NOTED

FULLMER CONSTRUCTION
CLARION TRAILER YARD GUARD
BOOTH

B.I.G. ENTERPRISES, INC.
9702 E. RUSH STREET
SOUTH EL MONTE, CA 91733-1730
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December 21, 2020

TRAILER STORAGE AT AVIATION DRIVE

6440 AVIATION DRIVE
STOCKTON, CA 95206

Exhibit 1



3428 Brookside Road
Stockton, California 95219
209-943-2021
Fax: 209-942-0214
www.siegfriedeng.com

- CIVIL
- STRUCTURAL
- LANDSCAPE ARCHITECTURE
- SURVEYING
- PLANNING
- ATHLETIC FACILITY DESIGN

REVISIONS
No. Date Description

PROJECT

**TRAILER STORAGE
AVIATION DRIVE**

6440 AVIATION DRIVE
STOCKTON, CA



SHEET TITLE

COVER SHEET

Proj Mgr	AKM
Drawn by	JR
Date	04/20/2021
Job No.	21081

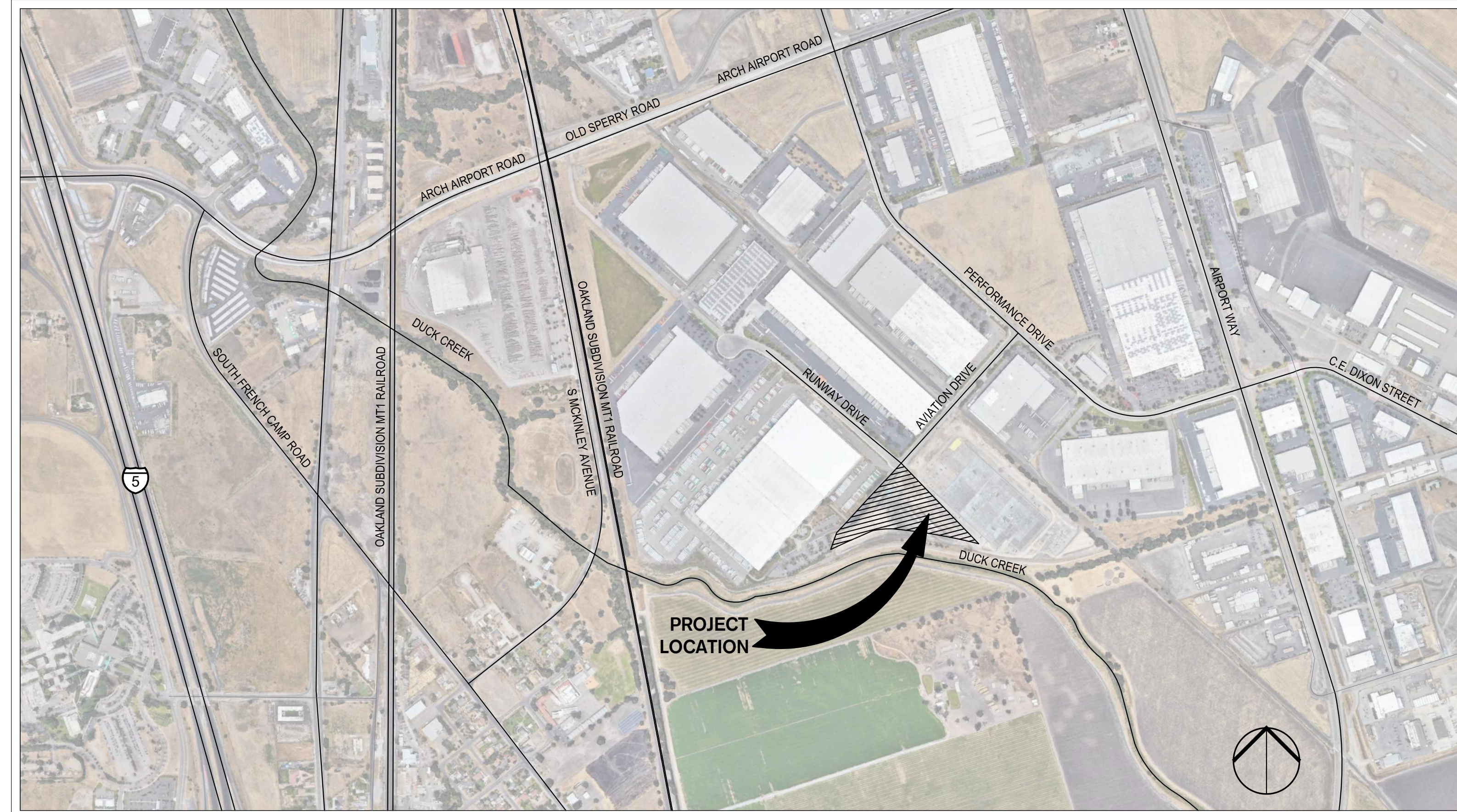
SHEET:

C1.0

OF: 18

ABBREVIATIONS

ABBREVIATION	DESCRIPTION
@	AT
AB	AGGREGATE BASE
ABS	ACRYLONITRILE-BUTADIENE-STYRENE
AC	ASPHALT CONCRETE
BO	BLOWOFF
BOC	BACK OF CURB
BOW	BACK OF WALK
CALWATER	CALIFORNIA WATER SERVICE COMPANY
C.G. & SW	CURB, GUTTER, AND SIDEWALK
CB	CATCH BASIN
CL	CENTERLINE
CO	CLEANOUT
DI	DROP INLET
DIA	DIAMETER
DIP	DUCTILE IRON PIPE
DWG	DRAWING
EBOW	EXISTING BACK OF WALK
EG	EXISTING GROUND
EL	ELEVATION
EP	EDGE OF PAVEMENT, EXISTING PAVEMENT
ESDMH	EXISTING STORM DRAIN MAINTENANCE HOLE
ESMT	EASEMENT
ESSMH	EXISTING SANITARY SEWER MAINTENANCE HOLE
EX	EXISTING
FH	FIRE HYDRANT
FL	FLOWLINE
FOC	FACE OF CURB
FT.	FEET
G	GROUND
GB	GRADE BREAK
HP	HIGH POINT
ID	INSIDE DIAMETER
IN.	INCH
INV	INVERT
LF	LINEAL FEET
LH	LAMP HOLE
LP	LOW POINT
LT	LEFT
MAX	MAXIMUM
MH	MAINTENANCE HOLE
MIN	MINIMUM
MUD	MUNICIPAL UTILITIES DEPARTMENT
NAVD	NORTH AMERICAN VERTICAL DATUM
NO.	NUMBER
NRCP	NON-REINFORCED CONCRETE PIPE
NTS	NOT TO SCALE
OD	OUTSIDE DIAMETER
P	PAVEMENT
PG&E	PACIFIC GAS AND ELECTRIC COMPANY
PL	PROPERTY LINE
PP	POWER POLE
PT	POINT
PUE	PUBLIC UTILITY EASEMENT
PVC	POLYVINYL CHLORIDE
R	RADIAL OR RADIUS
RW	RIGHT-OF-WAY
RC	ROLL-CURB
RT	RIGHT
SD	STORM DRAIN
SDMH	STORM DRAIN MAINTENANCE HOLE
SHT	SHEET
SJC	SAN JOAQUIN COUNTY
SPECS	SPECIFICATIONS
SS	SANITARY SEWER
SSCO	SANITARY SEWER CLEANOUT
SDDI	STORM DRAIN DROP INLET
SSMH	SANITARY SEWER MAINTENANCE HOLE
STA	STATION
STD	STANDARD
SW	SIDEWALK
TC	TOP OF CURB
TFMR	TRANSFORMER
TI	TRAFFIC INDEX
TOW	TOP OF WALL
TYP	TYPICAL
U.S.A.	UNDERGROUND SERVICE ALERT
VCP	VITRIFIED CLAY PIPE
W	WATER
WW	WATER VALVE



VICINITY MAP
NOT TO SCALE

PROJECT CONTACTS

DEVELOPER:
ALSTON CONSTRUCTION
8775 FOLSOM BLVD
SACRAMENTO, CA 95826
CONTACT: PATRICK BUCKLEY
PH: 916-231-3882

CIVIL ENGINEER:
SIEGFRIED ENGINEERING, INC.
3428 BROOKSIDE ROAD
STOCKTON, CA 95219
CONTACT: ADAM MERRILL, P.E.
PH: 209-943-2021

WORKING HOURS

ALL WORKING HOURS SHALL BE FROM 8:00 AM - 5:00 PM, MONDAY THROUGH FRIDAY. ANY EXTENDED WORKING HOURS SHALL REQUIRE THE APPROVAL OF THE ENGINEER.

LEGEND

EXISTING		PROPOSED
	ELECTRICAL CONDUIT	
	GROUND CONTOUR	
	TOP OF CURB ELEVATION	
	PAVEMENT ELEVATION	
	CONCRETE ELEVATION	
	EASEMENT LINE	
	PROPERTY & R/W LINE	
	SANITARY SEWER LINE	
	STORM DRAIN LINE	
	WATER LINE	
	FIRE HYDRANT LINE	
	STREET LIGHT	
	SITE LIGHT	
	CLEANOUT OR LAMPHOLE	
	AREA DRAIN	
	DRAINAGE INLET	
	CATCH BASIN	
	GATE VALVE	
	FIRE HYDRANT	
	SIGN	
	WATER METER	
	BACKFLOW PREVENTER	
	MAINTENANCE HOLE	
	EXISTING TREE	

SHEET INDEX

SHEET NO.	SHEET TITLE
C1.0	COVER SHEET
C1.1	GENERAL NOTES
C2.0	EXISTING TOPOGRAPHY AND DEMOLITION PLAN
C3.0	OVERALL PAVING AND DIMENSIONING PLAN
C3.1	PAVING AND DIMENSIONING PLAN I
C3.2	PAVING AND DIMENSIONING PLAN II
C3.3	PAVING AND DIMENSIONING PLAN III
C4.0	OVERALL GRADING PLAN
C4.1	GRADING PLAN I
C4.2	GRADING PLAN II
C4.3	GRADING PLAN III
C5.0	OVERALL UTILITY PLAN
C5.1	UTILITY PLAN I
C5.2	UTILITY PLAN II
C5.3	UTILITY PLAN III
C6.0	EROSION CONTROL PLAN
C7.0	DETAILS I
C7.1	DETAILS II
E-1	ELECTRICAL SITE PLAN
E-2	ELECTRICAL SITE LIGHTING
E-3	ELECTRICAL ONE-LINE/ PANEL SCHEDULES
E-4	ELECTRICAL TITLE 24 OUTDOOR LIGHTING
L1	IRRIGATION PLAN
L2	PLANTING PLAN
L3	LANDSCAPE DETAILS



Know what's below.
Call before you dig.

GENERAL NOTES

- ALL IMPROVEMENTS SHALL BE CONSTRUCTED IN STRICT ACCORDANCE WITH THE CITY OF STOCKTON STANDARD SPECIFICATIONS AND PLANS, LATEST EDITION, AND ALL AMENDMENTS THERE TO-DATE.
- FOR ELEVATIONS REFER TO BENCHMARK REFERENCED ON SHEET C2.0.
- PRIOR TO AND DURING CONSTRUCTION, THE CONTRACTOR SHALL BE RESPONSIBLE FOR BEING FAMILIAR WITH THE CURRENT CITY OF STOCKTON STANDARDS AND ALL UPDATES AND REVISIONS MADE TO ANY OF THE CITY OF STOCKTON STANDARD DETAILS SHOWN ON THESE PLANS. THE CONTRACTOR IS ALSO RESPONSIBLE FOR BEING FAMILIAR WITH THE WRITTEN SPECIFICATIONS AND/OR OTHER STANDARD DETAILS NOT SHOWN BUT WHICH ARE INCLUDED IN THE "CITY OF STOCKTON STANDARD SPECIFICATIONS AND PLANS".
- DRAWING NUMBERS SHOWN ON THE PLANS REFER TO CITY OF STOCKTON STANDARD PLANS, SHOWN THUS: COS STD. DWG. NO.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING FROM DAMAGE ALL EXISTING IMPROVEMENTS THAT ARE TO REMAIN. SUCH IMPROVEMENTS THAT ARE DAMAGED BY THE CONTRACTOR SHALL BE REPLACED AT HIS/HER EXPENSE.
- ALL TRENCH EXCAVATION SHALL BE IN ACCORDANCE WITH CITY OF STOCKTON STANDARD SPECIFICATIONS.
- EXCAVATION OF 5 FEET OR MORE IN DEPTH WILL REQUIRE AN EXCAVATION PERMIT FROM THE STATE OF CALIFORNIA DEPARTMENT OF INDUSTRIAL SAFETY.
- THE CONTRACTOR SHALL DEMOLISH, EXCAVATE, REMOVE AND DISPOSE OF ALL EXISTING CONCRETE CURB, GUTTER OR SIDEWALK, ASPHALT CONCRETE PAVING, AND DELETERIOUS MATERIAL AS REQUIRED TO CONSTRUCT THE CONTRACT WORK. ALL SUCH EXCESS MATERIAL GENERATED SHALL BE DISPOSED OF FROM THE SITE BY THE CONTRACTOR.
- THE CONTRACTOR SHALL OBTAIN AN ENCROACHMENT PERMIT FOR ANY WORK DONE WITHIN THE CITY RIGHT-OF-WAY FROM THE CITY OF STOCKTON PUBLIC WORKS DEPARTMENT, AND NOTIFY THE CITY 48 HOURS IN ADVANCE OF STARTING ANY WORK TO BE ACCEPTED FOR OWNERSHIP AND MAINTENANCE BY THE CITY OF STOCKTON.
- EXISTING UTILITIES ARE SHOWN AS THEY ARE BELIEVED TO EXIST. THE OWNER AND THE ENGINEER DO NOT ACCEPT RESPONSIBILITY FOR THEIR ACCURACY. PRIOR TO COMMENCING ANY WORK, THE CONTRACTOR SHALL HAVE EACH UTILITY COMPANY ACCURATELY LOCATE IN THE FIELD THEIR MAINS AND SERVICE LINES. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO PROTECT ALL EXISTING UTILITIES.
- ATTENTION IS CALLED TO: SECTION 1540 (A) (1) OF THE CONSTRUCTION SAFETY ORDERS (TITLE 8 CALIFORNIA ADMINISTRATION CODE SECTION 1540), ISSUED BY THE OCCUPATIONAL SAFETY AND HEALTH STANDARDS BOARD PURSUANT TO THE CALIFORNIA OCCUPATIONAL SAFETY AND HEALTH ACT OF 1973, AS AMENDED, WHICH STATES:

"PRIOR TO OPENING AN EXCAVATION, EFFORT SHALL BE MADE TO DETERMINE WHETHER UNDERGROUND INSTALLATION I.E., SEWER, WATER, FUEL, ELECTRIC LINES, ETC., WILL BE ENCOUNTERED AND, IF SO, WHERE SUCH UNDERGROUND INSTALLATIONS ARE LOCATED. WHEN THE EXCAVATION APPROACHES THE APPROXIMATE LOCATION OF SUCH AN INSTALLATION, THE EXACT LOCATION SHALL BE DETERMINED BY CAREFUL PROBING OR HAND DIGGING AND WHEN IT IS UNCOVERED, ADEQUATE PROTECTION SHALL BE PROVIDED FOR THE EXISTING INSTALLATION. ALL KNOWN OWNERS OF UNDERGROUND FACILITIES IN THE AREA CONCERNED SHALL BE ADVISED OF PROPOSED WORK AT LEAST 48 HOURS PRIOR TO THE START OF ACTUAL EXCAVATION."
- THE CONTRACTOR SHALL CONTACT THE CITY OF STOCKTON AT LEAST 72 HOURS IN ADVANCE OF THE CONTRACTOR'S INTENT TO CONNECT TO PUBLIC UTILITIES TO COORDINATE THE CONNECTION TO PUBLIC WATER, SEWER AND STORM DRAINAGE SYSTEMS.
- PRIOR TO COMMENCING ANY WORK, IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO HAVE EACH UTILITY COMPANY LOCATE IN THE FIELD THEIR MAIN SERVICE LINES. THE CONTRACTOR SHALL NOTIFY MEMBERS OF THE UNDERGROUND SERVICE ALERT (U.S.A.) 48 HOURS IN ADVANCE OF PERFORMING EXCAVATION WORK BY CALLING THE TOLL-FREE NUMBER (800-227-2600).
- THE CONTRACTOR SHALL CHECK WITH THE UTILITY COMPANIES AND VERIFY ALL UTILITY LOCATIONS. IT SHALL BE CONTRACTOR'S SOLE RESPONSIBILITY TO PROTECT ALL EXISTING UTILITIES SO THAT NO DAMAGE RESULTS TO THEM DURING THE PERFORMANCE OF HIS CONTRACT. THE CONTRACTOR SHALL BE REQUIRED TO COOPERATE WITH OTHER CONTRACTORS AND UTILITY COMPANIES INSTALLING NEW STRUCTURES, UTILITIES AND SERVICES TO THE DEVELOPMENT.
- WHENEVER EXISTING PAVEMENT IS BROKEN OR CUT DURING THE INSTALLATION OF THE WORK COVERED BY THESE PLANS AND SPECIFICATIONS, THE PAVEMENT SHALL BE REPLACED WITH PAVEMENT MATERIALS EQUAL TO OR BETTER THAN THE MATERIALS USED IN THE ORIGINAL PAVING. THE FINISHED PAVEMENT SHALL BE SUBJECT TO THE APPROVAL OF THE CITY ENGINEER IF LOCATED WITHIN CITY RIGHT-OF-WAY.
- PAYMENT FOR PAVEMENT WILL BE MADE FOR THE AREAS SHOWN ON THE PLANS. REPLACEMENT OF PAVEMENT WHICH IS BROKEN OR CUT IN THE INSTALLATION OF THE IMPROVEMENTS COVERED BY THESE PLANS AND SPECIFICATIONS, AND WHICH LIES OUTSIDE OF SAID AREAS, SHALL BE INCLUDED IN THE STREET CONTRACTOR'S UNIT PRICE FOR PAVEMENT, AND NO ADDITIONAL PAYMENT SHALL BE MADE FOR SUCH WORK.
- THE CONTRACTOR SHALL EXPOSE EXISTING STORM DRAINS, WATER MAINS, AND SANITARY SEWERS WHERE CONNECTIONS AND CROSSINGS ARE TO BE MADE SO EXISTING FLOWLINES AND LOCATIONS CAN BE VERIFIED BEFORE THE START OF CONSTRUCTION.
- THE CONTRACTOR AGREES THAT HE SHALL ASSUME SOLE AND COMPLETE RESPONSIBILITY FOR JOB SITE CONDITIONS DURING THE COURSE OF CONSTRUCTION OF THIS PROJECT INCLUDING SAFETY OF ALL PERSONS AND PROPERTY, THAT THIS REQUIREMENT SHALL APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS, AND THAT THE CONTRACTOR SHALL DEFEND, INDEMNIFY AND HOLD THE OWNER AND THE ENGINEER HARMLESS FROM ANY AND ALL LIABILITY, REAL OR ALLEGED, IN CONNECTION WITH THE PERFORMANCE OF WORK ON THIS PROJECT, EXCEPTING FOR LIABILITY ARISING FROM THE SOLE NEGLIGENCE OF THE OWNER OR THE ENGINEER.
- CONSTRUCTION STAKING: CONSTRUCTION STAKING IS NORMALLY PERFORMED BY THE ENGINEER WHO PREPARED THE PLAN. THIS PERMITS APPROPRIATE ON-SITE INTERPRETATION AND ADJUSTMENT OF THE PLANS. IF NECESSARY, CONTRACTOR SHALL NOTIFY THE ENGINEER FORTY-EIGHT (48) HOURS PRIOR TO THE ACTUAL NEED FOR STAKING. ANY STAKING REQUESTED BY THE CONTRACTOR OR HIS SUBCONTRACTORS WHICH EXTENDS BEYOND THE ORIGINAL SCOPE OF WORK DEFINED IN THESE PLANS SHALL BE SUBJECT TO AN EXTRA WORK CHARGE TO THE CONTRACTOR. THIS NOTE GIVES FORMAL NOTICE THAT THE FIRM OF SIEGFRIED ENGINEERING, INC. CANNOT, AND WILL NOT, TAKE RESPONSIBILITY FOR ERRORS OR OMISSIONS, IF ANY, WHICH MIGHT OCCUR AND WHICH COULD HAVE BEEN AVOIDED OR DETECTED AND/OR CORRECTED OR MITIGATED HAD SIEGFRIED ENGINEERING, INC. PERFORMED THE CONTRACT STAKING WORK.
- THE CONTRACTOR SHALL OBTAIN A PERMIT FROM THE CITY OF STOCKTON FOR USE OF WATER FROM FIRE HYDRANTS FOR CONSTRUCTION PURPOSES. THE PERMIT SHALL BE APPROVED BY THE CITY OF STOCKTON FIRE DEPARTMENT.
- THE PROPERTY OWNERS, DEVELOPERS, AND/OR SUCCESSORS IN INTEREST SHALL COMPLY WITH THE PROVISIONS OF THE CALIFORNIA GENERAL CONSTRUCTION ACTIVITY STORM WATER PERMIT AND STATE WATER RESOURCES CONTROL BOARD ORDER NUMBER 99-08-DWQ COMPLIANCE IS MANDATORY PER THE CITY OF STOCKTON'S GRADING AND EROSION CONTROL STANDARDS.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL PERMITS AND LICENSES REQUIRED FOR THE CONSTRUCTION AND COMPLETION OF THE PROJECT. THE CONTRACTOR SHALL OBTAIN AN ENCROACHMENT PERMIT AND A GRADING AND EROSION CONTROL PERMIT PRIOR TO STARTING ANY WORK, UNLESS OTHERWISE APPROVED BY THE CITY.
- DUST CONTROL SHALL BE PERFORMED AT ALL TIMES, AT THE CONTRACTORS' EXPENSE, TO MINIMIZE ANY DUST NUISANCE AND SHALL BE IN ACCORDANCE WITH SECTION 10 OF CALTRANS STANDARD SPECIFICATIONS AND THE REQUIREMENTS OF THE CITY OF STOCKTON.
- THE CONTRACTOR SHALL FURNISH, INSTALL OPERATE AND MAINTAIN ALL MACHINERY, APPLIANCES AND EQUIPMENT TO MAINTAIN ALL EXCAVATIONS FREE FROM WATER DURING CONSTRUCTION, AND SHALL DEWATER AND DISPOSE OF THE WATER SO AS TO NOT CAUSE INJURY TO PUBLIC OR PRIVATE PROPERTY, OR TO CAUSE A NUISANCE OR MENACE TO THE PUBLIC. THE DEWATERING SYSTEM SHALL BE INSTALLED AND OPERATED SO THE GROUNDWATER LEVEL OUTSIDE THE EXCAVATION IS NOT REDUCED TO THE EXTENT WHICH WOULD CAUSE DAMAGE OR ENDANGER ADJACENT STRUCTURES OR PROPERTY. ALL COSTS FOR DEWATERING SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ALL PIPE CONSTRUCTION. THE STATIC WATER LEVEL SHALL BE DRAWN DOWN A MINIMUM OF 1 FOOT BELOW THE BOTTOM OF EXCAVATION OF ANY FILL TO THE SPECIFIED DENSITY. DISPOSAL OF WATER SHALL BE IN ACCORDANCE WITH THE APPROVED SWPPP AND SHALL NOT DAMAGE PROPERTY, CREATE A PUBLIC NUISANCE OR VIOLATE THE LAW. THE CONTRACTOR SHALL HAVE ON HAND, PUMPING EQUIPMENT AND MACHINERY IN GOOD WORKING CONDITION FOR EMERGENCIES AND SHALL HAVE WORKMEN AVAILABLE FOR ITS OPERATION. THE DEWATERING SYSTEM SHALL OPERATE CONTINUOUSLY UNTIL BACK-FILL HAS BEEN COMPLETED TO 1 FOOT ABOVE THE NORMAL STATIC GROUNDWATER LEVEL.
- SITE PREPARATION SHALL INCLUDE STRIPPING AND REMOVAL OF ALL VEGETATION AND ANY DEBRIS FROM THE CONSTRUCTION AREAS. THE DEPTH OF STRIPPING ONSITE SHALL BE EVALUATED BY THE OWNER'S SOILS ENGINEER.
- ANY YOIDS LEFT BY THE REMOVAL OF UNDERGROUND UTILITIES OR OTHER BURIED OBJECTS SHALL BE CLEANED OF ALL LOOSE SOILS AND SHALL BE PROPERLY BACKFILLED WITH ENGINEERED FILLED THAT THE OWNER'S SOILS ENGINEER APPROVES, MONITORS, TESTS, AND APPROVES.
- THE SUBGRADE SOILS BENEATH ALL PAVING AREAS SHOULD BE COMPACTED TO A MINIMUM OF 95% OF THE DRY DENSITY WHICH THE OWNER'S SOILS ENGINEER SPECIFIES.

- IF ANY SOFT OR LOOSE SOIL POCKETS ARE FOUND ONSITE DURING THE STRIPPING OR RECOMPACTION PROCESS, THEY SHALL BE OVER EXCAVATED AND RECOMPACTED.
- ENGINEERED FILL SHALL BE PLACED IN HORIZONTAL LAYERS A MAXIMUM OF 8 INCHES IN LOOSE THICKNESS AND BE COMPACTED TO A MINIMUM OF 90 PERCENT OF MAXIMUM DRY DENSITY AS DETERMINED BY THE OWNER'S SOILS ENGINEER.
- SUBGRADE SOILS UNDER EXTERIOR CONCRETE SHALL BE MOISTURE CONDITIONED AS APPROVED, TESTED AND INSPECTED BY THE OWNER'S SOILS ENGINEER.
- CONTRACTOR SHALL MAINTAIN MOISTURE CONDITION RIGHT UP TO POURING OF CONCRETE.
- ALL CITY OF STOCKTON STANDARD DETAILS REFER TO CITY OF STOCKTON PUBLIC WORKS DEPARTMENT STANDARD SPECIFICATIONS AND DETAILS 2006 UNLESS OTHERWISE NOTED.
- WHERE COMBINATIONS OF SIDEWALK OR CURB AND GUTTER ARE POURED CONTIGUOUS TO EXISTING, ALL ADJOINING EXISTING CONCRETE VERTICAL FACES SHALL BE DOWELED. ALL ABUTTING SIDEWALK ENDS SHALL BE DOWELED MID-SECTION VERTICALLY WITH TWO DOWELS FOR FOUR THROUGH SIX-FOOT WIDE SIDEWALK AND THREE DOWELS FOR WIDER SIDEWALK. ABUTTING CURB AND GUTTER ENDS SHALL BE DOWELED TWICE, 18 INCHES APART AT GUTTER PAN MID-SECTION. DOWEL CONNECTIONS OF LONGITUDINAL RUNS OF SIDEWALK TO BACK OF CURB SHALL BE THREE FEET ON CENTER. ALL DOWELS SHALL BE 16 INCHES LONG, GRADE 60; #4 REBAR PENETRATING FOUR INCHES. THE DOWEL HOLE SHALL BE 5/8-INCH DIAMETER AT A SLIGHT HORIZONTAL ANGLE FROM PERPENDICULAR. THE PENETRATING PORTION OF THE DOWEL AND THE ENTIRE (CLEANED) VERTICAL SURFACE OF THE ADJOINING, EXISTING CONCRETE SHALL BE THOROUGHLY COATED WITH STATE STANDARD TWO-PART EPOXY.
- SURVEY MONUMENT PRESERVATION: PRIOR TO CONSTRUCTION, CONTRACTOR SHALL ENGAGE A LICENSED SURVEYOR TO PERFORM A PRE-CONSTRUCTION MONUMENT PRESERVATION SURVEY IN ACCORDANCE WITH SECTIONS 8771(b) OF THE BUSINESS AND PROFESSIONS CODE OF THE STATE OF CALIFORNIA. LOCATIONS OF EXISTING MONUMENTATION KNOWN TO THE ENGINEER THAT ARE WITHIN THE AREA OF PROJECT HAVE BEEN INDICATED ON THE PLANS FOR REFERENCE. AT THE COMPLETION OF CONSTRUCTION, BUT PRIOR TO FINAL APPROVAL OF THE IMPROVEMENTS, CONTRACTOR'S LICENSED SURVEYOR SHALL PERFORM A POST-CONSTRUCTION MONUMENT PRESERVATION SURVEY IN ACCORDANCE WITH SECTIONS 8771(c) AND 8771(d), AND, IF NECESSARY, FILE A RECORD OF SURVEY OR CORNER RECORD(S) IN ACCORDANCE WITH SECTION 8771(f).

CITY OF STOCKTON GENERAL NOTES

- ALL MATERIAL AND WORK SHALL CONFORM TO CITY OF STOCKTON SPECIFICATIONS AND PLANS. THE IMPROVEMENTS ARE SUBJECT TO THE INSPECTION AND APPROVAL OF THE PUBLIC WORKS DEPARTMENT. CONTACT THE ADMINISTRATION OFFICE AT (209) 977-8411 TWO (2) WORKING DAYS (48 HOURS) PRIOR TO THE START OF ANY WORK TO ARRANGE FOR INSPECTION.
- PRIOR TO EXCAVATING NEAR ANY UNDERGROUND UTILITIES, CALL U.S.A. AT LEAST 48 HOURS IN ADVANCE AT PHONE 811.
- THESE PLANS HAVE BEEN CHECKED BY THE CITY OF STOCKTON AND/OR ITS AUTHORIZED REPRESENTATIVE, BUT SUCH CHECKING AND/OR APPROVAL DOES NOT RELIEVE THE DEVELOPER AND CONTRACTOR FROM HIS/HER RESPONSIBILITY TO CORRECT ERRORS, OMISSIONS OR MAKE CHANGES REQUIRED BY CONDITIONS DISCOVERED IN THE FIELD DURING THE COURSE OF CONSTRUCTION.
- RELOCATION OF DESIGNED UTILITY SYSTEMS MORE THAN 25' MUST BE REVIEWED BY THE PUBLIC WORKS DEPARTMENT PRIOR TO CONSTRUCTION AND SHALL BE ACCURATELY SHOWN ON REVISED PLANS STAMPED AND SHALL BE APPROVED BY CITY STAFF AND THE DESIGN ENGINEER PRIOR TO THE INSTALLATION OF THE IMPROVEMENTS.
- ALL CONSTRUCTION STAKING FOR CURB, GUTTER, AND SIDEWALK, SANITARY SEWERS, STORM DRAINS, WATER LINES, FIRE HYDRANTS, AND ELECTROLIERS, ETC. SHALL BE DONE UNDER THE DIRECTION OF A CIVIL ENGINEER OR AN INDIVIDUAL LICENSED TO PERFORM LAND SURVEYING UNDER THE CALIFORNIA BUSINESS & PROFESSIONS CODE.
- HOUSE SERVICES, FIRE HYDRANT LATERALS, GAS AND TELEPHONE LINES, AND ALL OTHER UNDERGROUND UTILITIES SHALL BE INSTALLED PRIOR TO CURB, GUTTER, AND SIDEWALK CONSTRUCTION AND STREET PAVING.
- ALL LINES ABANDONED DURING CONSTRUCTION SHALL BE REMOVED.
- FOR ALL PROJECTS, REGARDLESS OF SIZE, THE CONTRACTOR SHALL IMPLEMENT BEST MANAGEMENT PRACTICES TO ELIMINATE OR MINIMIZE POLLUTION DISCHARGE CAUSED BY CONSTRUCTION (SEE CHAPTER 16).
- MATERIAL TESTING:
 - ALL INDEPENDENT MATERIAL TESTING AND INSPECTION CALLED FOR BY THE CITY ENGINEER SHALL BE FURNISHED AND PAID FOR BY THE DEVELOPER OR CONTRACTOR.
 - SUBGRADE TESTING FOR R-VALUES IS REQUIRED PRIOR TO THE INSTALLATION OF BASE ROCK.
 - CONTRACTOR SHALL FURNISH MATERIAL CERTIFICATIONS OF PRE-MANUFACTURED MATERIAL WHEN CALLED FOR BY THE CITY ENGINEER.
- STREET STRIPING SHALL INCLUDE STOP BARS, CENTERLINE STRIPING OR MARKERS, CROSSWALKS AND ALL OTHER MARKINGS REQUIRED BY THE CITY ENGINEER. STRIPING SHALL BE DONE WITH THERMOPLASTIC AND REFLECTIVE MARKERS.
- WHEN WIDENING THE PAVEMENT ON AN EXISTING ROAD, THE EXISTING PAVEMENT SHALL BE CUT TO A NEAT LINE AND REMOVED BACK TO AN EXISTING ADEQUATE STRUCTURAL SECTION, OR TO THE ORIGINAL ROAD SECTION. AN EXPLORATORY TRENCH, OR POT-HOLING, MAY BE REQUIRED TO DETERMINE THE LIMITS OF PAVEMENT REMOVAL.
- EXISTING CURB AND SIDEWALK WITHIN THE PROJECT LIMITS THAT ARE DAMAGED OR DISPLACED, EVEN THOUGH THEY WERE NOT TO BE REMOVED, SHALL BE REPAIRED OR REPLACED PER CITY STANDARD DETAILS EVEN IF DAMAGE OR DISPLACEMENT OCCURRED PRIOR TO ANY WORK PERFORMED BY THE CONTRACTOR.
- ASBESTOS CEMENT PIPE (ACP) OR FITTINGS SHALL NOT BE USED WITHIN THE CITY OF STOCKTON.
- PRIOR TO TRENCHING FOR ANY SEWER, WATER, OR STORM DRAIN PIPE, THE CONTRACTOR SHALL VERIFY, IN THE FIELD, THE SIZE AND LOCATION OF THE EXISTING PIPE AT THE POINT OF CONNECTION. ANY DEVIATION FROM THE PLANS SHALL BE RESOLVED BY THE DESIGN ENGINEER PRIOR TO TRENCHING.
- MANHOLES, VALVES, CLEANOUTS, ETC. SHALL BE BROUGHT TO FINISH GRADE BY THE UNDERGROUND CONTRACTOR AFTER THE FINAL PAVING COURSE IS PLACED.
- FOR PIPES GREATER THAN 30" ON SEWER AND 36" ON STORM DRAIN, 60" INSIDE DIAMETER, MANHOLES SHALL BE USED.
- STREET CLOSURE OR LANE CLOSURE WILL REQUIRE A TRAFFIC CONTROL PLAN AND THE DESIGNATION OF A QUALIFIED INDIVIDUAL FOR ITS IMPLEMENTATION AND SAFE MAINTENANCE.

GRADING NOTES

- GRADING AND LAND STABILIZATION SHALL INCLUDE EXCAVATION AND FILL OF STREETS IN ACCORDANCE WITH THE SPECIFICATIONS OF THE SOILS ENGINEER AND UNDER THE DIRECTION, SUPERVISION, MONITORING, TESTING AND APPROVAL OF THE OWNER AND OWNER'S SOILS ENGINEER.
- GRADING AND LAND STABILIZATION SHALL INCLUDE COST OF DEWATERING, REMOVING FROM THE SITE ALL STRIPPED VEGETATION, DEBRIS, STRUCTURES, POWER POLES, EXISTING PAVEMENT, BUILDINGS, TREES, AND OTHER DELETERIOUS MATERIALS.
- STOCKPILES OF EXISTING DELETERIOUS MATERIAL SHALL BE DISPOSED OF UNDER THE DIRECTION AND SUPERVISION OF THE OWNER AND OWNER'S SOILS ENGINEER.
- ALL IMPORTED FILL SHALL BE APPROVED BY THE SOILS ENGINEER.
- TOPOGRAPHICAL INFORMATION SHOWN REFLECTS A TOPOGRAPHY SURVEY PERFORMED BY SIEGFRIED ENGINEERING.
- ANY AND ALL SEDIMENT AND/OR EROSION CONTROL DETAILS CONTAINED WITHIN THESE PLANS ARE TO BE CONSIDERED AS "REFERENCE DETAILS" ONLY AND THE CITY'S APPROVAL OF THESE PLANS AND "REFERENCE DETAILS" DOES NOT RELIEVE THE OWNER/DEVELOPER FROM COMPLIANCE WITH THE STORM WATER POLLUTION PREVENTION PLAN AS APPROVED BY THE CITY'S STORM WATER DIVISION.

UTILITY NOTES

WATER

- ALL WATER LINES SHALL BE PRESSURE-TESTED, DISINFECTED, FLUSHED, AND TESTED FOR BACTERIA IN CONFORMANCE WITH THE CITY OF STOCKTON SPECIFICATIONS PRIOR TO FINAL ACCEPTANCE BY THE CITY.
- ALL WATER SERVICES SHALL BE 1.5" MINIMUM. WATER SERVICE SHALL BE CONNECTED TO WATER MAINS WITH TWO-STRAP BRONZE SADDLES. CITY SHALL MAKE ALL TAPS ON EXISTING WATER MAINS ONLY.
- ALL VALVES, TEES AND CROSSES TO BE FLANGED TO THEIR RESPECTIVE FITTINGS. WATER VALVES TO BE RESILIENT SEAT ONLY.
- WATER MAINS AT THE END OF FUTURE STREETS SHALL HAVE TWO (2) HALF-LENGTHS OF PIPE BETWEEN GATE VALVE AND BLOW-OFF. DEFLECTION OF WATER LINES SHALL NOT EXCEED 80% OF MANUFACTURER'S SPECIFICATIONS.
- WATER MAINS SHALL BE AS PER CITY STANDARDS AND FIRE HYDRANTS SHALL BE JONES J-4040, LONG BEACH 425, CLOW 850, AVK 2470 OR APPROVED EQUAL AND SHALL CONFORM TO THE LATEST AWWA SPECIFICATIONS, C-503, FOR WET-BARREL FIRE HYDRANTS. ALL HYDRANTS SHALL BE PAINTED WITH CATERPILLAR YELLOW POLYURETHANE HIGH DUTY INDUSTRIAL ENAMEL. NO LEAD BASED PAINTS MAY BE USED. 2-16
- ALL VALVE STEMS MUST BE BROUGHT TO A MINIMUM OF 4' BELOW FINISH GRADE WITH STEM EXTENSION UNITS.
- THRUST BLOCKS SHALL BE PROVIDED AT ALL REQUIRED LOCATIONS ON WATER LINE IN ACCORDANCE WITH THE CITY OF STOCKTON SPECIFICATIONS AND DETAILS.
- ALL BACKFLOW DEVICES SHALL BE INSTALLED WITH A WEATHER BLANKET FOR PROTECTION.

STORM DRAIN

- STORM DRAIN PIPE SIZES SHALL NOT BE CHANGED WITHOUT THE APPROVAL OF THE DESIGN ENGINEER.
- STORM DRAIN PIPE:
 - REINFORCED CONCRETE PIPE, PER CITY OF STOCKTON STANDARD
 - CAST-IN-PLACE CONCRETE PIPE, PER CITY OF STOCKTON STANDARD
 - PVC PIPE, PER CITY OF STOCKTON STANDARD
 - HDPE PIPE, PER CITY OF STOCKTON STANDARD
- CATCH BASINS TO BE CONSTRUCTED PER CITY STANDARD DETAILS.
- ALL CONSTRUCTION SITE ACTIVITIES, REGARDLESS OF PROJECT SIZE, SHALL CONFORM TO THESE STANDARDS. PROJECTS GREATER THAN ONE (1) ACRE SHALL ALSO CONFORM TO THE STATE WATER RESOURCES CONTROL BOARD (SWRCB) GENERAL CONSTRUCTION ACTIVITY STORM WATER PERMIT.

SANITARY SEWER

- SANITARY SEWER PIPE SHALL BE:
 - V.C.P. EXTRA STRENGTH (ASTM C-700)
 - PVC PIPE, PER CITY OF STOCKTON STANDARD
 - HDPE PIPE, PER CITY OF STOCKTON STANDARD
- ALL SANITARY SEWER MAINS SHALL BE TELEVISION INSPECTED, FLUSHED WITH AN APPROVED SEWER BALL AND PASS A LEAKAGE TEST IN CONFORMANCE WITH CITY OF STOCKTON STANDARD SPECIFICATIONS PRIOR TO ACCEPTANCE BY THE CITY. ALL TESTING SHALL BE PERFORMED AFTER THE COMPACTION FOR STREET BASE ROCK AND PRIOR TO PAVING.
- A CLEANOUT SHALL BE PLACED AT RIGHT-OF-WAY LINE PER COS STD. DWG. NO. S-17. COVER ON LATERAL AT PROPERTY LINE TO BE 3' MINIMUM TO 5' MAXIMUM EXCEPT AS NOTED ON PLANS.
- ALL MANHOLES CONSTRUCTED ON A TRUNK MAIN SHALL BE PVC LINED PER INDUSTRY STANDARD/MANUFACTURER'S SPECIFICATIONS AND TESTED IN CONFORMANCE WITH CITY OF STOCKTON STANDARD SPECIFICATIONS.

GEOTECHNICAL NOTES

- CONTRACTOR TO REFERENCE GEOTECHNICAL REPORT FOR ALL SUBGRADE PREPARATION, PAVEMENT RECOMMENDATIONS, SLAB ON GRADE THICKNESS, ETC. AND COMPARE WITH ANY RECOMMENDATIONS ON THE PLANS. IF ANY DISCREPANCIES EXISTING NOTIFY THE ENGINEERS IMMEDIATELY.
- GEOTECHNICAL ENGINEERING SERVICES REPORT PREPARED BY RANEY GEOTECHNICAL, INC. TITLED GEOTECHNICAL INVESTIGATION TWO TILT-UP CONCRETE BUILDINGS DATED APRIL 6, 2018.

FEMA MAP NUMBER: 06077C0470F
ZONE: SPECIAL FLOOD HAZARD AREA, ZONE A
BASE FLOOD ELEVATION: 22.2 NAVD88

ABBREVIATIONS

ABBREVIATION

@
AB
ABS
AC
BCR
BO
BOC
BOW
C & G
C.G. & SW
CL
CE
C.B.C.
CJ
CO
C.O.S.
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DESCRIPTION

AT
AGGREGATE BASE
ACRYLONITRILE-BUTADIENE-STYRENE
ASPHALT CONCRETE
BEGINNING OF CURB RETURN
BLOWOFF
BACK OF CURB
BACK OF WALK
CURB AND GUTTER
CURB, GUTTER, AND SIDEWALK
CENTERLINE
CATCH BASIN
CALIFORNIA BUILDING CODE 2016
CONSTRUCTION JOINT
CLEANOUT
CITY OF STOCKTON
DIAMETER
DUCTILE IRON PIPE
DRAWING
EXISTING BACK OF WALK
END OF CURB RETURN
ELEVATION
EDGE OF EXISTING PAVEMENT
EDGE OF PAVEMENT, EXISTING PAVEMENT
EASEMENT
EXISTING
FLOWLINE
FIRE HYDRANT
FACE OF CURB
FEET
GROUND
GRADE BREAK
HORIZONTAL
HIGH POINT
INSIDE DIAMETER
INCH
LINEAL FEET
LAMP HOLE
LTS
LOW POINT
LEFT
LIME TREATED SUB-BASE
MAXIMUM
MAINTENANCE HOLE
MINIMUM
NUMBER
NON-REINFORCED CONCRETE PIPE
NOT TO SCALE
OUTSIDE DIAMETER
PAVEMENT
POINT OF CURVATURE
POINT OF COMPOUND CURVATURE
POINT OF TANGENCY
POWER POLE
POINT OF REVERSE CURVATURE
POINT
PUBLIC UTILITY EASEMENT
POLYVINYL CHLORIDE
PROPERTY LINE
RADIAL OR RADIUS
RIGHT-OF-WAY
ROLL-CURB
REINFORCED CONCRETE PIPE
RETURN
RADIUS POINT
RIGHT
STORM DRAIN
SANITARY SEWER
STREET LIGHT
SIDEWALK
STORM DRAIN MAINTENANCE HOLE
SHEET
SANITARY SEWER MAINTENANCE HOLE
STATION
STANDARD
TOP OF CURB
TOP OF WALL
TOP OF WALL
THROUGH
TRAFFIC INDEX
TYPICAL
UNLESS NOTED OTHERWISE
UNDERGROUND VAULT
VITRIFIED CLAY PIPE
VERTICAL
WATER
WEAKENED PLANE
WEST
EAST
SOUTH
NORTH
PLUS OR MINUS

Exhibit 1



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- CIVIL
- STRUCTURAL
- LANDSCAPE ARCHITECTURE
- SURVEYING
- PLANNING
- ATHLETIC FACILITY DESIGN

REVISIONS

No. Date Description

PROJECT

TRAILER STORAGE AVIATION DRIVE

6440 AVIATION DRIVE
STOCKTON, CA



SHEET TITLE

GENERAL NOTES

Proj Mgr **AKM**
Drawn by **JR**
Date **04/20/2021**
Job No. **21081**

SHEET:

C1.1

Exhibit 1



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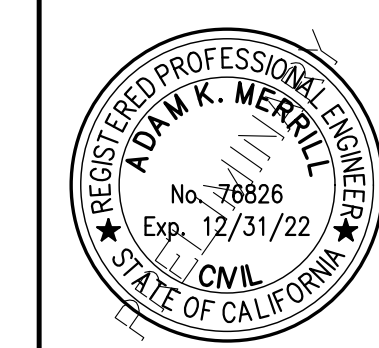
- CIVIL
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REVISIONS
No. Date Description

PROJECT

TRAILER STORAGE AVIATION DRIVE

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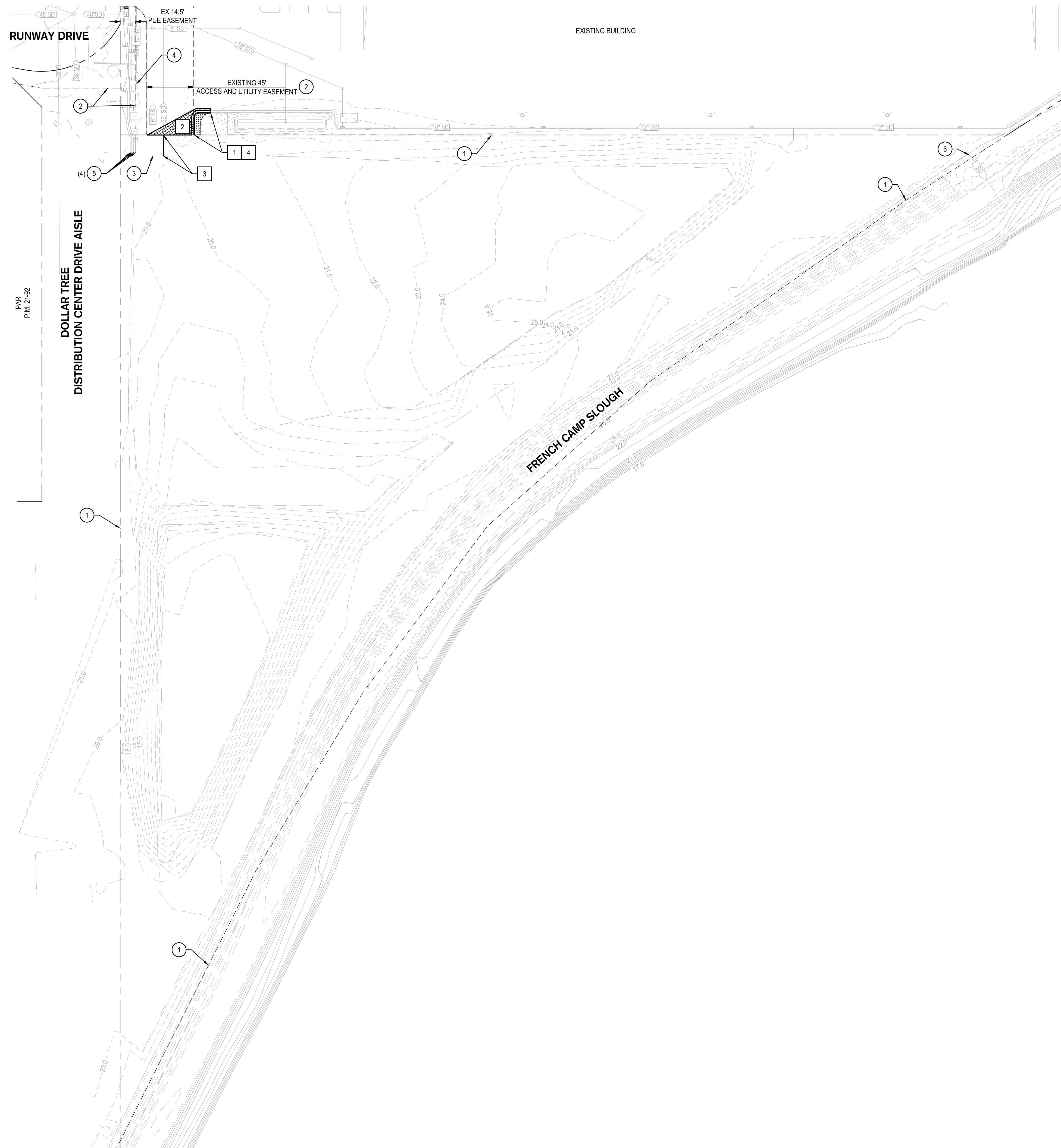


SHEET TITLE

EXISTING TOPOGRAPHY AND DEMOLITION PLAN

Proj Mgr AKM
Drawn by JR
Date 04/20/2021
Job No. 21081

SHEET:
C2.0
OF: 18



LEGEND

- CLEAR AND GRUB, REMOVAL SHALL ACCOMMODATE THE PROPOSED IMPROVEMENTS
- REMOVE AND DISPOSE OF EXISTING ASPHALT PAVEMENT AND SUBGRADE TO ACCOMMODATE THE PROPOSED IMPROVEMENTS
- EXISTING STORM DRAIN LINE
- EXISTING SANITARY SEWER LINE
- EXISTING WATER LINE
- EXISTING ELECTRICAL LINE
- REMOVE EXISTING STORM DRAIN PIPE

NOTES:

1. CONTRACTOR IS RESPONSIBLE FOR REMOVING AND PROPERLY DISPOSING OF ALL MATERIALS DEMOLISHED FROM THE SITE INCLUDING: PAVEMENT, CONCRETE, CURB AND GUTTER, STORM DRAINAGE MATERIALS AND ELECTRICAL MATERIALS.
2. IF ANY QUESTIONS ARISE AS TO WHETHER SOMETHING SHOULD BE REMOVED, CONTRACTOR SHALL CONTACT SIEGFRIED ENGINEERING, INC. IMMEDIATELY AT 209-943-2021.
3. ANYTHING NOT CALLED OUT TO BE REMOVED SHALL BE PROTECTED IN PLACE, AND IF DAMAGED, SHALL BE REPLACED / REPAIRED AT THE CONTRACTOR'S EXPENSE.
4. ALL EXISTING UTILITIES WERE PLOTTED FROM RECORD INFORMATION AND FIELD TOPOGRAPHY. ACTUAL LOCATIONS MAY VARY AND ADDITIONAL CROSSINGS MAY EXIST IN THE FIELD. IT IS IMPERATIVE THAT "U.S.A. LOCATING SERVICES" LOCATE AND MARK EXISTING UTILITIES PRIOR TO THE START OF EXCAVATION.
5. THE CONTRACTOR SHALL USE EXTREME CAUTION WHEN EXPOSING EXISTING UTILITY CROSSINGS AND SERVICES.
6. LANDSCAPE PROTECTION AND DEMOLITION TO BE PERFORMED BY LANDSCAPE CONTRACTOR(S). CONTRACTOR TO COORDINATE EARTH MOVING AND DEMOLITION OPERATIONS WITH LANDSCAPE CONTRACTOR(S).
7. BOLD ITEMS TO BE REMOVED UNLESS OTHERWISE NOTED.

PROTECTION KEY NOTES

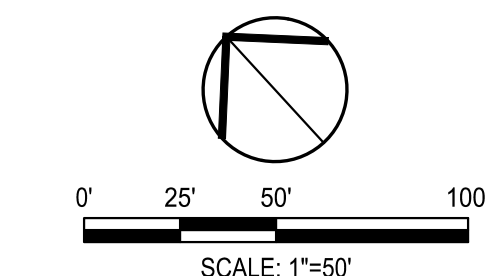
- ① RIGHT-OF-WAY / PROPERTY LINE
- ② PUBLIC UTILITIES EASEMENT
- ③ EXISTING SANITARY SEWER STUB
- ④ EXISTING POTABLE WATER STUB
- ⑤ EXISTING UNDERGROUND UTILITY STUB
- ⑥ EXISTING STORM DRAIN CULVERT

REMOVAL KEY NOTES

- 1 REMOVE AND DISPOSE EXISTING CURB AND GUTTER
- 2 REMOVE AND DISPOSE EXISTING ASPHALT CONCRETE
- 3 REMOVE AND DISPOSE EXISTING STORM DRAIN PIPE
- 4 CLEAR AND GRUB, REMOVAL SHALL ACCOMMODATE THE PROPOSED IMPROVEMENTS

BENCHMARK:

BRASS DISK MARKING CITY OF STOCKTON MONUMENT
STAMPED "SS-14" IN MONUMENT WELL NEAR THE INTERSECTION
OF CENTERLINE OF AIRPORT WAY AND C. E. DIXON DRIVE.
BENCH MARK NO. 204 ELEV. = 24.27 FEET (2008 CITYWIDE
BENCHMARK VERTICAL DATUM CONVERSION TO NAVD88)



F:\projects\21081_Aviation Drive Site Development\Plans and Graphics\Improvement Plans\21081_051021_042021.dwg

Exhibit 1



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- SURVEYING
- PLANNING
- ATHLETIC FACILITY DESIGN

REVISIONS
No. Date Description

PROJECT

TRAILER STORAGE AVIATION DRIVE

6440 AVIATION DRIVE
STOCKTON, CA



SHEET TITLE

OVERALL PAVING AND DIMENSIONING PLAN

Proj Mgr: AKM
Drawn by: JR
Date: 04/20/2021
Job No.: 21081

SHEET:

C3.0

OF: 18

LEGEND

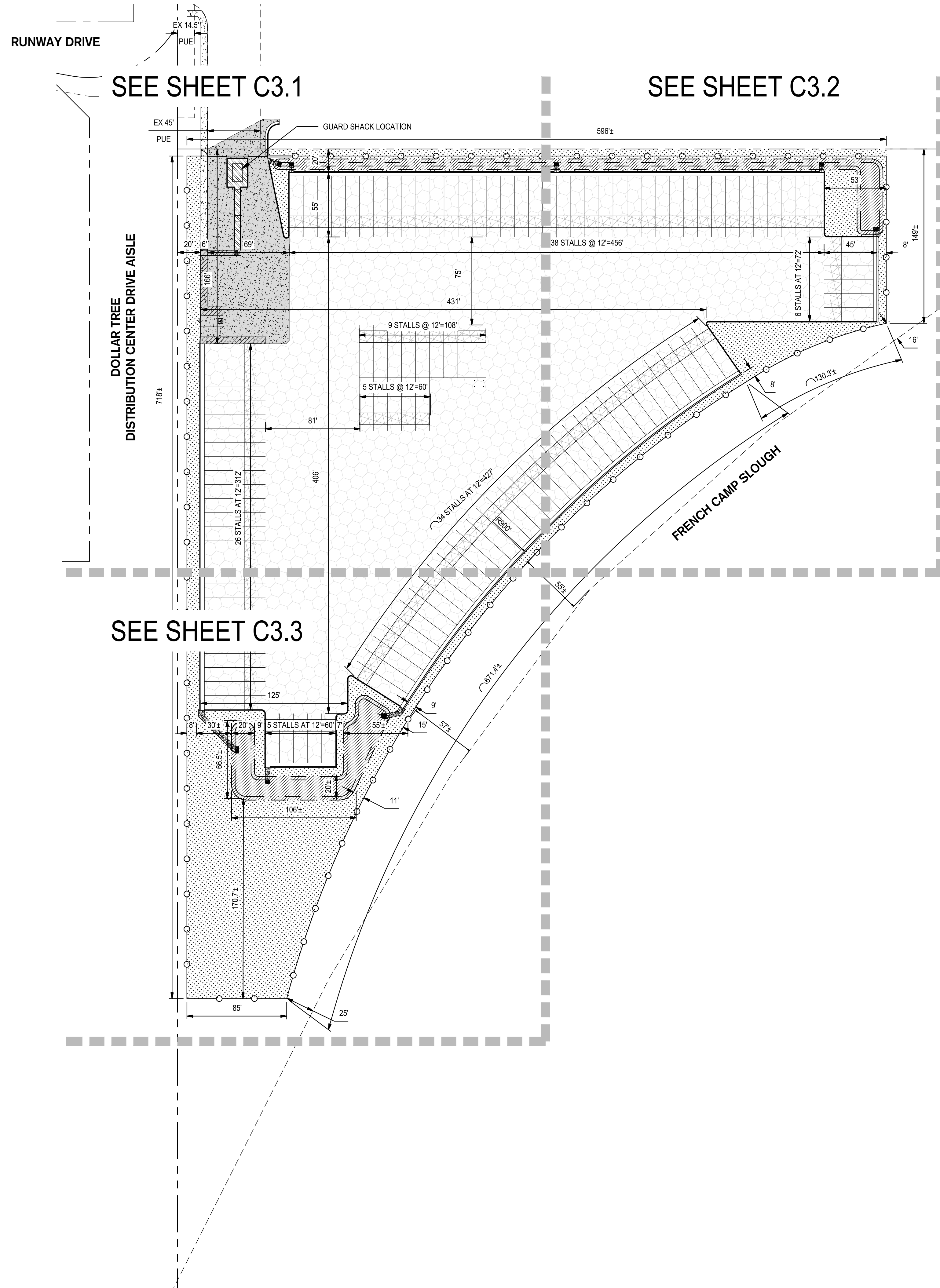
- HEAVY DUTY CONCRETE PAVEMENT (TI = 8.5)
7.0" (4000 PSI) CONCRETE WITH #4 BARS AT 24" O.C. EACH WAY OVER 3.0" CLASS II AB OVER 12.0" LIME TREATED SOIL OVER SUBGRADE PREPARED PER PROJECT GEOTECHNICAL REPORT.
- HEAVY DUTY ASPHALT PAVEMENT (TI = 8.5)
4.0" AC OVER 4.5" CLASS II AB OVER 15.0" LIME TREATED SOIL OVER SUBGRADE PREPARED PER PROJECT'S GEOTECHNICAL REPORT.
- DRIVEWAY ENTRY CONCRETE (TI = 9.5)
7.5" (4000 PSI) CONCRETE WITH #4 BARS AT 24" O.C. EACH WAY OVER 3.0" CLASS II AB OVER 12.0" LIME TREATED SOIL OVER SUBGRADE PREPARED PER PROJECT'S GEOTECHNICAL REPORT.
- PEDESTRIAN CONCRETE
5.0" (3000 PSI) CONCRETE WITH #3 BARS AT 24" O.C. OVER 4.0" CLASS II AB OVER 6.0" SUBGRADE PREPARED PER PROJECT'S GEOTECHNICAL REPORT. INSTALL CONCRETE JOINTS PER DETAIL 4 ON SHEET C7.0.
- LANDSCAPE AREA
SEE LANDSCAPE PLANS FOR PLANTING/VEGETATION AND IRRIGATION DETAILS.
- GUARD SHACK WITH ACCESS RAMP
- STORM WATER TREATMENT AREA
AREA TO BE USED FOR BIORETENTION FOR STORMWATER TREATMENT. SEE LANDSCAPE PLANS FOR PLANTING AND IRRIGATION DETAILS.

GENERAL PAVING AND DIMENSIONING NOTES:

ALL MATERIAL AND CONSTRUCTION PROCEDURES WITHIN THE SCOPE OF THIS PROJECT SHALL CONFORM TO ALL APPLICABLE CITY OF STOCKTON STANDARD SPECIFICATIONS (LATEST EDITION).

1. CONTRACTOR SHALL IMMEDIATELY NOTIFY ENGINEER OF ANY QUESTION THAT MAY ARISE CONCERNING THE INTENT, PLACEMENT, OR LIMITS OF DIMENSIONS NECESSARY FOR CONSTRUCTION OF THIS PROJECT.
2. ALL DIMENSIONS ARE TO FACE OF CURB, FACE OF BUILDING, CENTER OF PAINT STRIPING OR PERPENDICULAR TO THE PROPERTY LINE. CONTRACTOR SHALL VERIFY DIMENSIONS MATCH STRUCTURAL AND ARCHITECTURAL PLANS PRIOR TO CONSTRUCTION.
3. ALL COORDINATE POINTS ARE AT FACE OF CURB OR RADIUS POINT. BUILDING CONTROL POINTS ARE AT THE OUTER MOST EDGE OF THE BUILDING ENVELOPE.
4. THE CONTRACTOR SHALL SAWCUT EXISTING PAVEMENT, CURBS, AND SIDEWALKS AT NEW PAVEMENT, CURB AND SIDEWALK JUNCTIONS. NO JAGGED OR IRREGULAR CUTS WILL BE ALLOWED OR ACCEPTED.
5. ALL PAINT SHALL BE 4" WIDE REFLECTIVE PAINT: WHITE ON TOP OF 6" WIDE BLACK STRIPE ON CONCRETE UNLESS OTHERWISE NOTED ON THE DRAWINGS.
6. ALL PAVEMENT MARKINGS SHALL RECEIVE TWO COATS OF PAINT.
7. ALL SIGNS SHALL CONFORM TO MUTCD, LATEST EDITION.
8. ALL CURBS ARE 6" HIGH UNLESS OTHERWISE NOTED.
9. CONTRACTOR TO REFER TO GEOTECHNICAL REPORT FOR CONSTRUCTION JOINT SPACING REQUIREMENTS IN CONCRETE PAVEMENT.
10. ALL CRACKED, DAMAGED, OR DISPLACED CONCRETE CURB, GUTTER, AND SIDEWALK TO BE REMOVED AND REPLACED TO CURRENT CITY STANDARDS.

NOTE: GUARD SHACK CORNER POINTS ARE APPROXIMATE AND PROVIDED FOR GUARD SHACK PAD PREPARATION ONLY. CONTRACTOR SHALL REFERENCE STRUCTURAL ENGINEER'S FOUNDATION PLAN FOR ACTUAL GUARD SHACK DIMENSIONS.



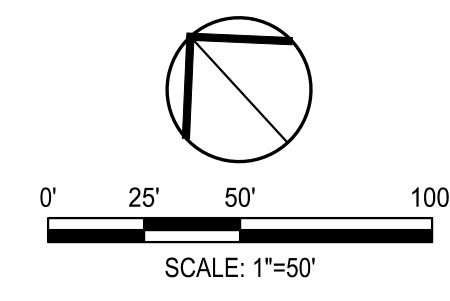
SEE SHEET C3.1

SEE SHEET C3.2

SEE SHEET C3.3



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- CIVIL
- STRUCTURAL
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- PLANNING
- ATHLETIC FACILITY DESIGN

REVISIONS
No. Date Description

PROJECT

TRAILER STORAGE AVIATION DRIVE

6440 AVIATION DRIVE
STOCKTON, CA



SHEET TITLE

PAVING AND DIMENSIONING PLAN I

Proj Mgr AKM
Drawn by JR
Date 04/20/2021
Job No. 21081
SHEET:

C3.1
OF 18

Exhibit 1

LEGEND

- HEAVY DUTY CONCRETE PAVEMENT (TI = 8.5)
7.0" (4000 PSI) CONCRETE WITH #4 BARS AT 24" O.C. EACH WAY OVER 3.0" CLASS II AB OVER 12.0" LIME TREATED SOIL OVER SUBGRADE PREPARED PER PROJECT GEOTECHNICAL REPORT.
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4.0" AC OVER 4.5" CLASS II AB OVER 15.0" LIME TREATED SOIL OVER SUBGRADE PREPARED PER PROJECT'S GEOTECHNICAL REPORT.
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5.0" (3000 PSI) CONCRETE WITH #3 BARS AT 24" O.C. OVER 4.0" CLASS II AB OVER 6.0" SUBGRADE PREPARED PER PROJECT'S GEOTECHNICAL REPORT. INSTALL CONCRETE JOINTS PER DETAIL 4 ON SHEET C7.0.
- LANDSCAPE AREA
SEE LANDSCAPE PLANS FOR PLANTING/VEGETATION AND IRRIGATION DETAILS.
- GUARD SHACK WITH ACCESS RAMP
- STORM WATER TREATMENT AREA
AREA TO BE USED FOR BIORETENTION FOR STORMWATER TREATMENT. SEE LANDSCAPE PLANS FOR PLANTING AND IRRIGATION DETAILS.
- ACCESSIBILITY PATH

GENERAL PAVING AND DIMENSIONING NOTES:

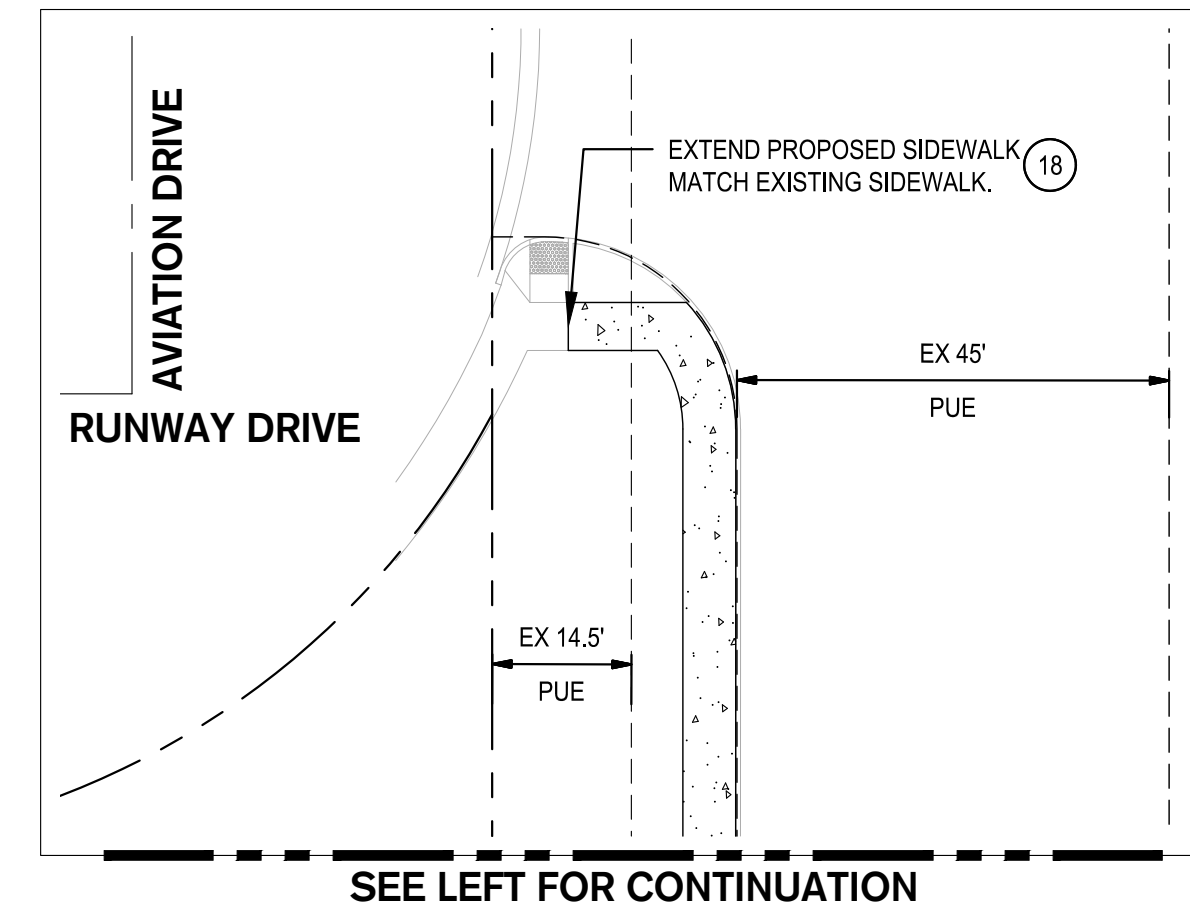
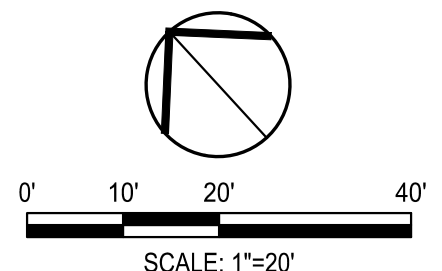
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2. ALL DIMENSIONS ARE TO FACE OF CURB, FACE OF BUILDING, CENTER OF PAINT STRIPING OR PERPENDICULAR TO THE PROPERTY LINE. CONTRACTOR SHALL VERIFY DIMENSIONS MATCH STRUCTURAL AND ARCHITECTURAL PLANS PRIOR TO CONSTRUCTION.
3. ALL COORDINATE POINTS ARE AT FACE OF CURB OR RADIUS POINT. BUILDING CONTROL POINTS ARE AT THE OUTER MOST EDGE OF THE BUILDING ENVELOPE.
4. THE CONTRACTOR SHALL SAWCUT EXISTING PAVEMENT, CURBS, AND SIDEWALKS AT NEW PAVEMENT, CURB AND SIDEWALK JUNCTURES. NO JAGGED OR IRREGULAR CUTS WILL BE ALLOWED OR ACCEPTED.
5. ALL PAINT SHALL BE 4" WIDE REFLECTIVE PAINT; WHITE ON TOP OF 6" WIDE BLACK STRIPE ON CONCRETE UNLESS OTHERWISE NOTED ON THE DRAWINGS.
6. ALL PAVEMENT MARKINGS SHALL RECEIVE TWO COATS OF PAINT.
7. ALL SIGNS SHALL CONFORM TO MUTCD, LATEST EDITION.
8. ALL CURBS ARE 6" HIGH UNLESS OTHERWISE NOTED.
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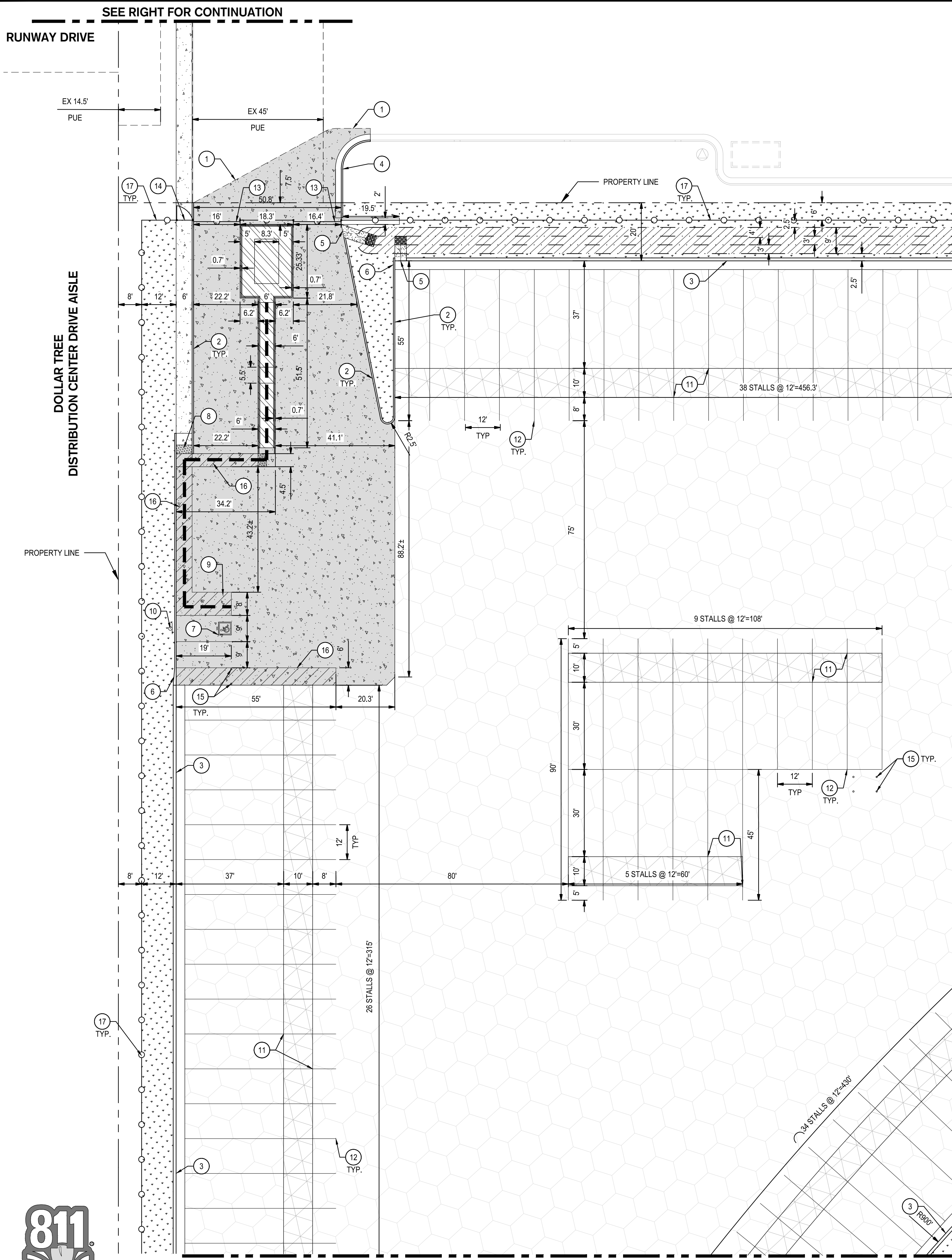
NOTE: GUARD SHACK CORNER POINTS ARE APPROXIMATE AND PROVIDED FOR GUARD SHACK PAD PREPARATION ONLY. CONTRACTOR SHALL REFERENCE STRUCTURAL ENGINEER'S FOUNDATION PLAN FOR ACTUAL GUARD SHACK DIMENSIONS.

KEY NOTES

- ① SAWCUT A NEAT, CLEAN LINE
- ② INSTALL 6" VERTICAL CURB PER DETAIL 1, SHEET C7.0
- ③ INSTALL 12" VERTICAL CURB AND GUTTER PER DETAIL 2, SHEET C7.0
- ④ INSTALL 6" VERTICAL CURB AND GUTTER PER DETAIL 3, SHEET C7.0
- ⑤ INSTALL CURB CUT PER DETAIL 5, SHEET C7.0
- ⑥ 3' TRANSITION FROM A 6" VERTICAL CURB TO 12" VERTICAL CURB
- ⑦ INSTALL ACCESSIBLE PARKING SYMBOL PER DETAIL 9, SHEET C7.0
- ⑧ INSTALL TRUNCATED DOMES PER DETAIL 10, SHEET C7.0
- ⑨ INSTALL ADA PARKING PER DETAIL 7, SHEET C7.0
- ⑩ INSTALL ACCESSIBLE PARKING SIGNAGE PER DETAIL 8, SHEET C7.0
- ⑪ INSTALL 10' WIDE DOLLY PAD
- ⑫ INSTALL 4" WHITE STRIPING PER C.O.S STANDARDS
- ⑬ INSTALL AMERISTAR DOUBLE ROLLING ACCESS GATE, OR SIMILAR PER DETAIL 5 SHEET C7.1
- ⑭ INSTALL AMERISTAR PEDESTRIAN GATE, OR SIMILAR PER DETAIL 6 SHEET C7.1
- ⑮ INSTALL STEEL BOLLARD PER DETAIL 2, SHEET C7.1
- ⑯ INSTALL CROSSWALK STRIPING 4" THICK WHITE THERMOPLASTIC PAINT AT 36" O.C.
- ⑰ INSTALL AMERISTAR 8" BLACK ORNAMENTAL IRON FENCE AT PROPERTY LINE, OR SIMILAR. SEE DETAIL 4 ON SHEET C7.1
- ⑱ INSTALL DOWELS WHEN INSTALLING NEW CONCRETE TO EXISTING CONNECTION. SEE CONCRETE JOINT DETAIL 4, SHEET C7.0



SEE LEFT FOR CONTINUATION



FOR CONTINUATION SEE SHEET C3.3

FOR CONTINUATION SEE SHEET C3.2

SEE RIGHT FOR CONTINUATION



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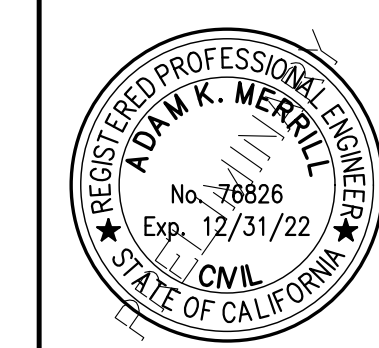
- CIVIL
- STRUCTURAL
- LANDSCAPE ARCHITECTURE
- SURVEYING
- PLANNING
- ATHLETIC FACILITY DESIGN

REVISIONS
No. Date Description

PROJECT

TRAILER STORAGE AVIATION DRIVE

6440 AVIATION DRIVE
STOCKTON, CA



SHEET TITLE

PAVING AND DIMENSIONING PLAN III

Proj Mgr AKM
Drawn by JR
Date 04/20/2021
Job No. 21081

SHEET:
C3.3
OF: 18

LEGEND

- HEAVY DUTY CONCRETE PAVEMENT (TI = 8.5)
7.0" (4000 PSI) CONCRETE WITH #4 BARS AT 24" O.C. EACH WAY OVER 3.0" CLASS II AB OVER 12.0" LIME TREATED SOIL OVER SUBGRADE PREPARED PER PROJECT GEOTECHNICAL REPORT.
- HEAVY DUTY ASPHALT PAVEMENT (TI = 8.5)
4.0" AC OVER 4.5" CLASS II AB OVER 15.0" LIME TREATED SOIL OVER SUBGRADE PREPARED PER PROJECT GEOTECHNICAL REPORT.
- LANDSCAPE AREA
SEE LANDSCAPE PLANS FOR PLANTING/VEGETATION AND IRRIGATION DETAILS.
- STORM WATER TREATMENT AREA
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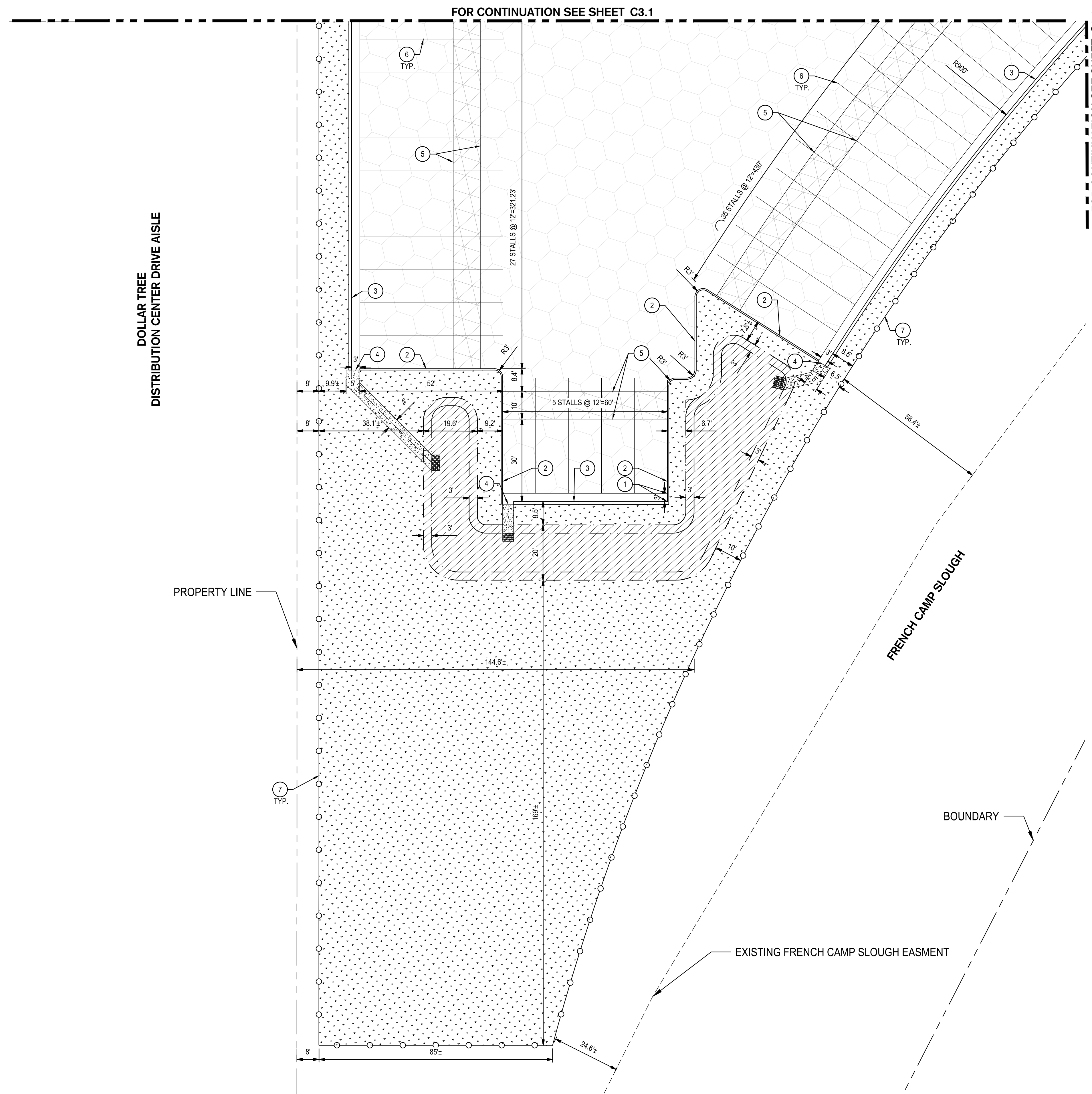
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KEY NOTES

- ① 3' TRANSITION FROM A 6" VERTICAL CURB TO 12" VERTICAL CURB
- ② INSTALL 6" VERTICAL CURB PER DETAIL 1, SHEET C7.0
- ③ INSTALL 12" VERTICAL CURB AND GUTTER PER DETAIL 2, SHEET C7.0
- ④ INSTALL CURB CUT PER DETAIL 5, SHEET C7.0
- ⑤ INSTALL 10' WIDE DOLLY PAD
- ⑥ INSTALL 4" WHITE STRIPING PER C.O.S STANDARDS
- ⑦ INSTALL AMERISTAR 8" BLACK ORNAMENTAL IRON FENCE AT PROPERTY LINE, OR SIMILAR. SEE DETAIL 3 ON SHEET C7.1



DOLLAR TREE
DISTRIBUTION CENTER DRIVE AISLE

PROPERTY LINE

FRENCH CAMP SLOUGH

BOUNDARY

EXISTING FRENCH CAMP SLOUGH EASMENT

FOR CONTINUATION SEE SHEET C3.1

FOR CONTINUATION SEE SHEET C3.2



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- ATHLETIC FACILITY DESIGN

REVISIONS
No. Date Description

PROJECT

TRAILER STORAGE AVIATION DRIVE

6440 AVIATION DRIVE
STOCKTON, CA



SHEET TITLE

OVERALL GRADING PLAN

Proj Mgr AKM

Drawn by JR

Date 04/20/2021

Job No. 21081

SHEET:

C4.0

OF: 18

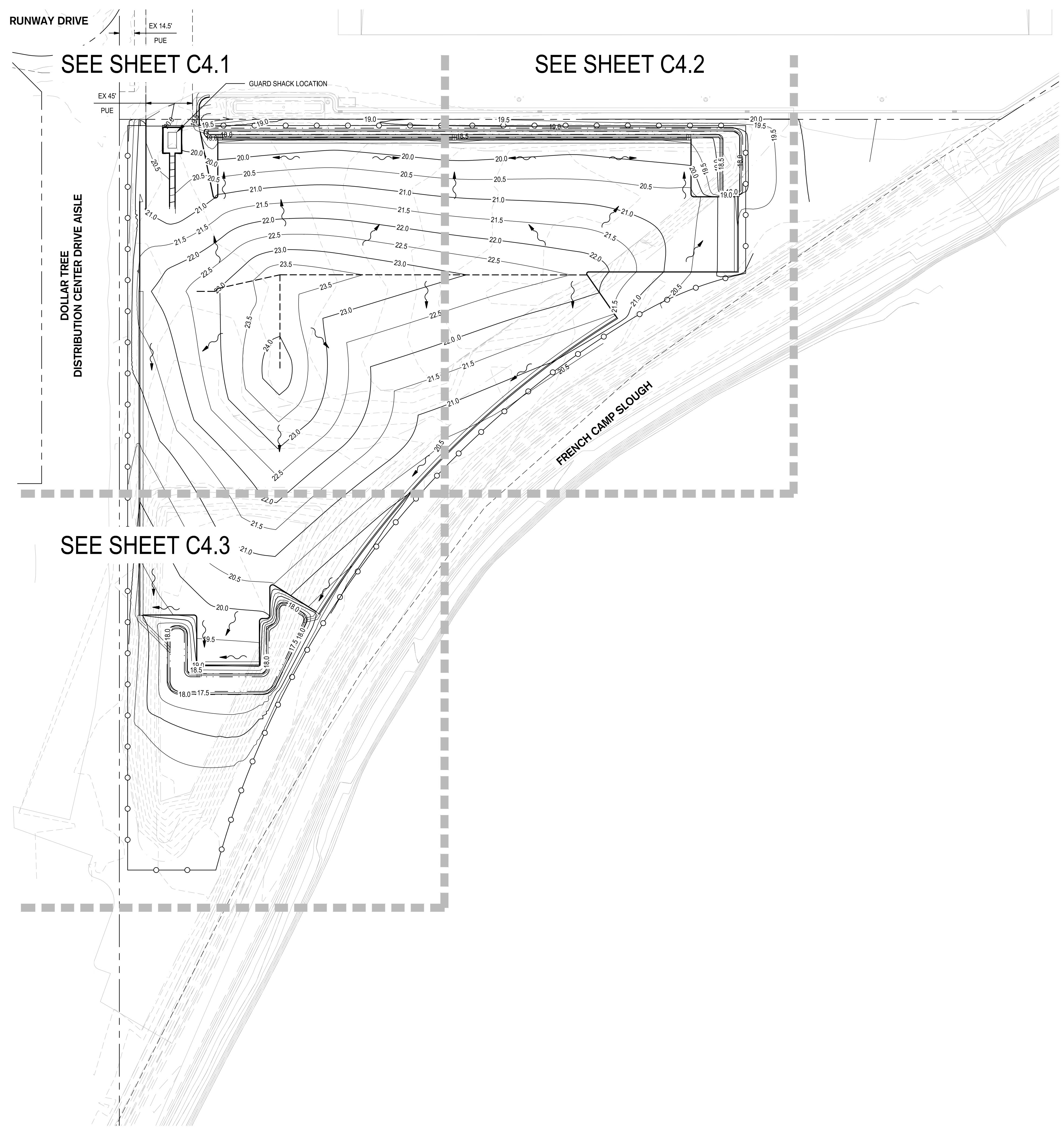
LEGEND

- $\%$ GRADE SLOPE PERCENTAGE
- 3.0 EXISTING GROUND CONTOUR
- 3.0 PROPOSED GROUND CONTOUR
- GRADE BREAK/RIDGELINE

GRADING LEGEND

ABBREVIATION	DESCRIPTION
BW	BACK OF WALK
BOW	BOTTOM OF WALL
C	CONCRETE
DG	DECOMPOSED GRANITE
EC	EXISTING CONCRETE
EFL	EXISTING FLOWLINE
EG	EXISTING GROUND
EL	ELEVATION
EP	EXISTING PAVEMENT
EX	EXISTING
FF	FINISHED FLOOR
FG	FINISH GROUND
FL	FLOWLINE
G	GROUND
GB	GRADE BREAK
MAX	MAXIMUM
MIN	MINIMUM
P	PAVEMENT
SDCB	STORM DRAIN CATCH BASIN
SDDI	STORM DRAIN INLET
TC	TOP OF CURB
TW	TOP OF WALL
TYP	TYPICAL

NOTE: GUARD SHACK CORNER POINTS ARE APPROXIMATE AND PROVIDED FOR GUARD SHACK PAD PREPARATION ONLY. CONTRACTOR SHALL REFERENCE STRUCTURAL ENGINEER'S FOUNDATION PLAN FOR ACTUAL GUARD SHACK DIMENSIONS.



F:\projects\21081_Aviation Drive Site Development\Plans and Graphics\Improvement Plans\C4.0 OVERALL GRADING PLAN.dwg -- 04/20/21



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- LANDSCAPE ARCHITECTURE
- SURVEYING
- PLANNING
- ATHLETIC FACILITY DESIGN

REVISIONS
No. Date Description

PROJECT

TRAILER STORAGE AVIATION DRIVE

6440 AVIATION DRIVE
STOCKTON, CA



SHEET TITLE

GRADING PLAN I

Proj Mgr AKM
 Drawn by JR
 Date 04/20/2021
 Job No. 21081

SHEET:

C4.1

OF: 18

Exhibit 1

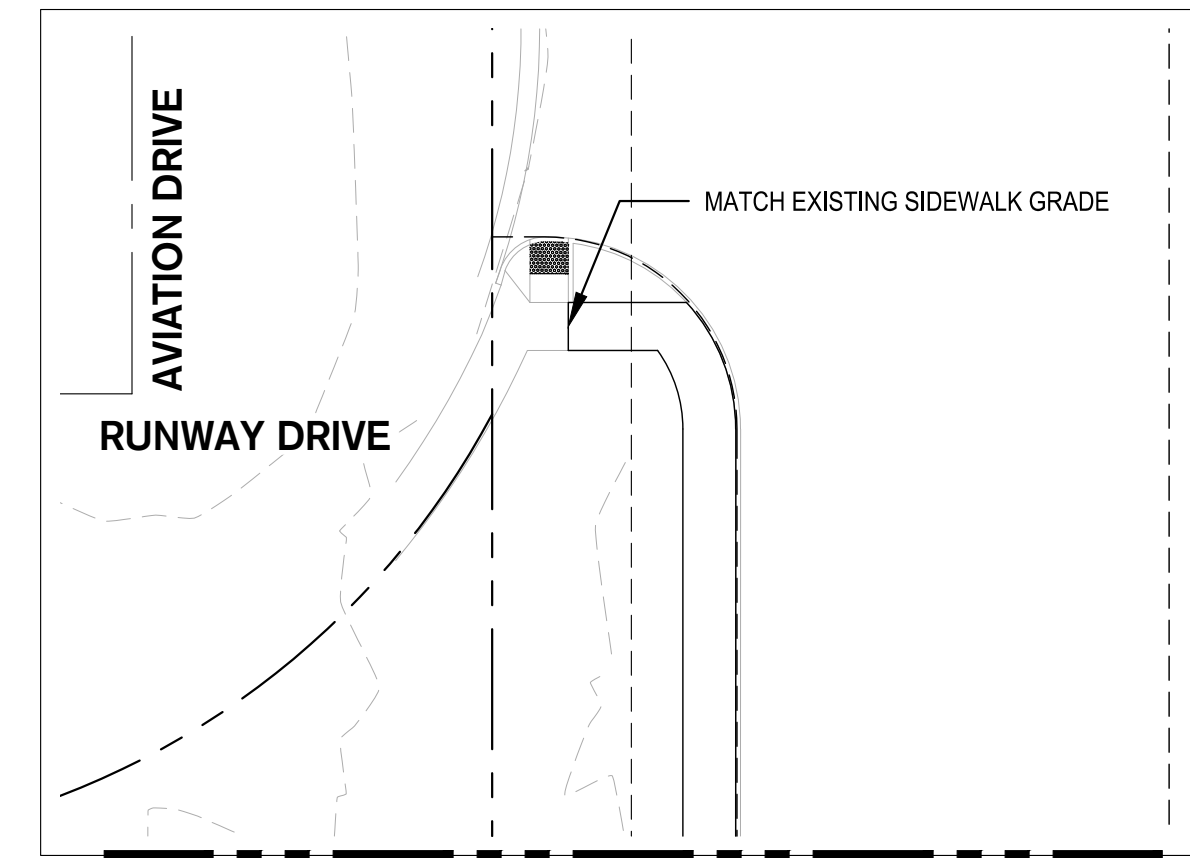
LEGEND

- % GRADE SLOPE PERCENTAGE
- - - 3.0 EXISTING GROUND CONTOUR
- 3.0 PROPOSED GROUND CONTOUR
- - - GRADE BREAK/RIDGELINE

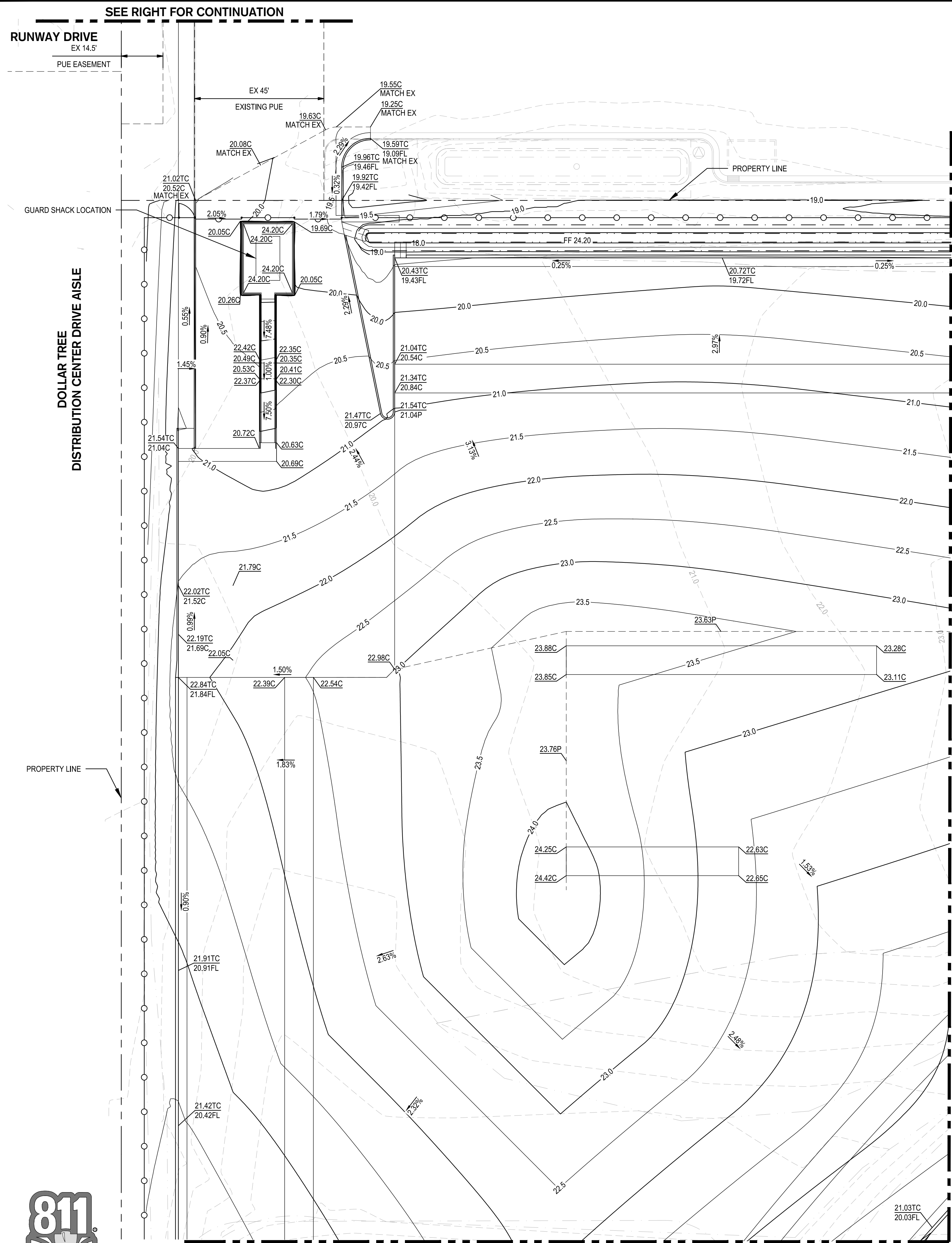
GRADING LEGEND

ABBREVIATION	DESCRIPTION
BW	BACK OF WALK
BOW	BOTTOM OF WALL
C	CONCRETE
DG	DECOMPOSED GRANITE
EC	EXISTING CONCRETE
EFL	EXISTING FLOWLINE
EG	EXISTING GROUND
EL	ELEVATION
EP	EXISTING PAVEMENT
EX	EXISTING
FF	FINISHED FLOOR
FG	FINISH GROUND
FL	FLOWLINE
G	GROUND
GB	GRADE BREAK
MAX	MAXIMUM
MIN	MINIMUM
P	PAVEMENT
SDCB	STORM DRAIN CATCH BASIN
SDDI	STORM DRAIN INLET
TC	TOP OF CURB
TW	TOP OF WALL
TYP	TYPICAL

NOTE: GUARD SHACK CORNER POINTS ARE APPROXIMATE AND PROVIDED FOR GUARD SHACK PAD PREPARATION ONLY. CONTRACTOR SHALL REFERENCE STRUCTURAL ENGINEER'S FOUNDATION PLAN FOR ACTUAL GUARD SHACK DIMENSIONS.



SEE LEFT FOR CONTINUATION



FOR CONTINUATION SEE SHEET C4.3

FOR CONTINUATION SEE SHEET C4.2

SEE RIGHT FOR CONTINUATION

RUNWAY DRIVE
EX 14.5'

PUE EASEMENT

EX 45'

EXISTING PUE

19.63C
MATCH EX

20.08C
MATCH EX

21.02TC
20.52C
MATCH EX

20.05C

24.20C

24.20C

20.26C

22.42C

20.49C

20.53C

22.37C

21.54TC
21.04C

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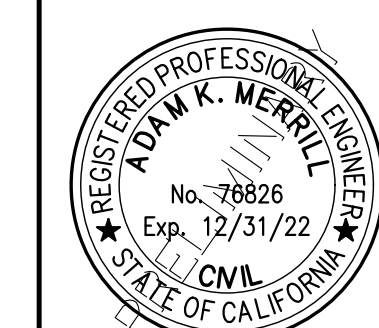
- CIVIL
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REVISIONS
No. Date Description

PROJECT

TRAILER STORAGE AVIATION DRIVE

6440 AVIATION DRIVE
STOCKTON, CA



SHEET TITLE

GRADING PLAN III

Proj Mgr AKM

Drawn by JR

Date 04/20/2021

Job No. 21081

SHEET:

C4.3

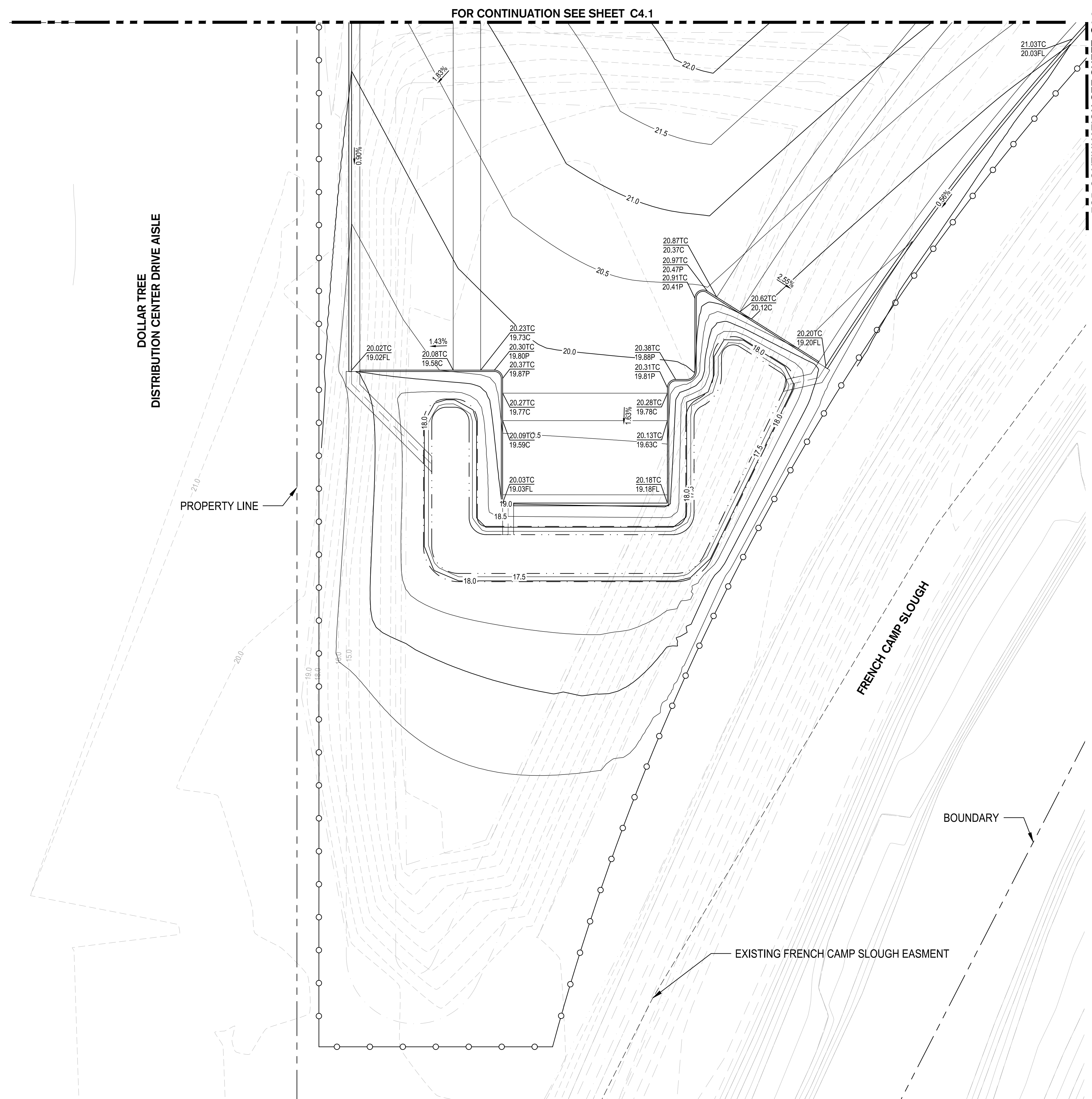
OF: 18

LEGEND

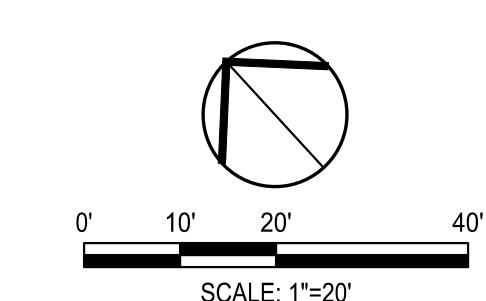
- ↗ % GRADE SLOPE PERCENTAGE
- 3.0 EXISTING GROUND CONTOUR
- 3.0 PROPOSED GROUND CONTOUR
- - - - GRADE BREAK/ RIDGELINE

GRADING LEGEND

ABBREVIATION	DESCRIPTION
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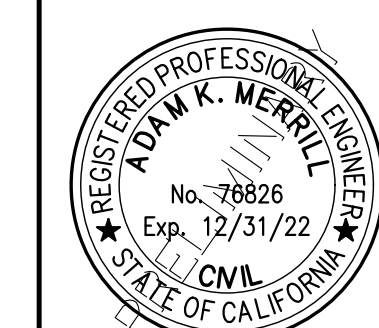
- CIVIL
- STRUCTURAL
- LANDSCAPE ARCHITECTURE
- SURVEYING
- PLANNING
- ATHLETIC FACILITY DESIGN

REVISIONS
No. Date Description

PROJECT

TRAILER STORAGE AVIATION DRIVE

6440 AVIATION DRIVE
STOCKTON, CA



SHEET TITLE

OVERALL UTILITY PLAN

Proj Mgr AKM
Drawn by JR
Date 04/20/2021
Job No. 21081

SHEET:
C5.0
OF: 18

LEGEND

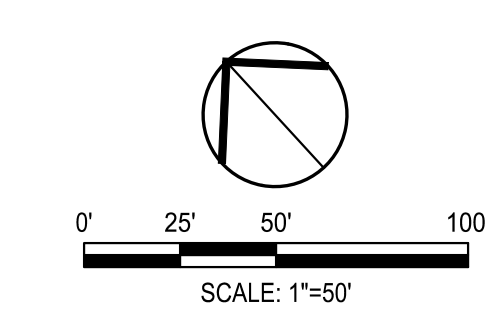
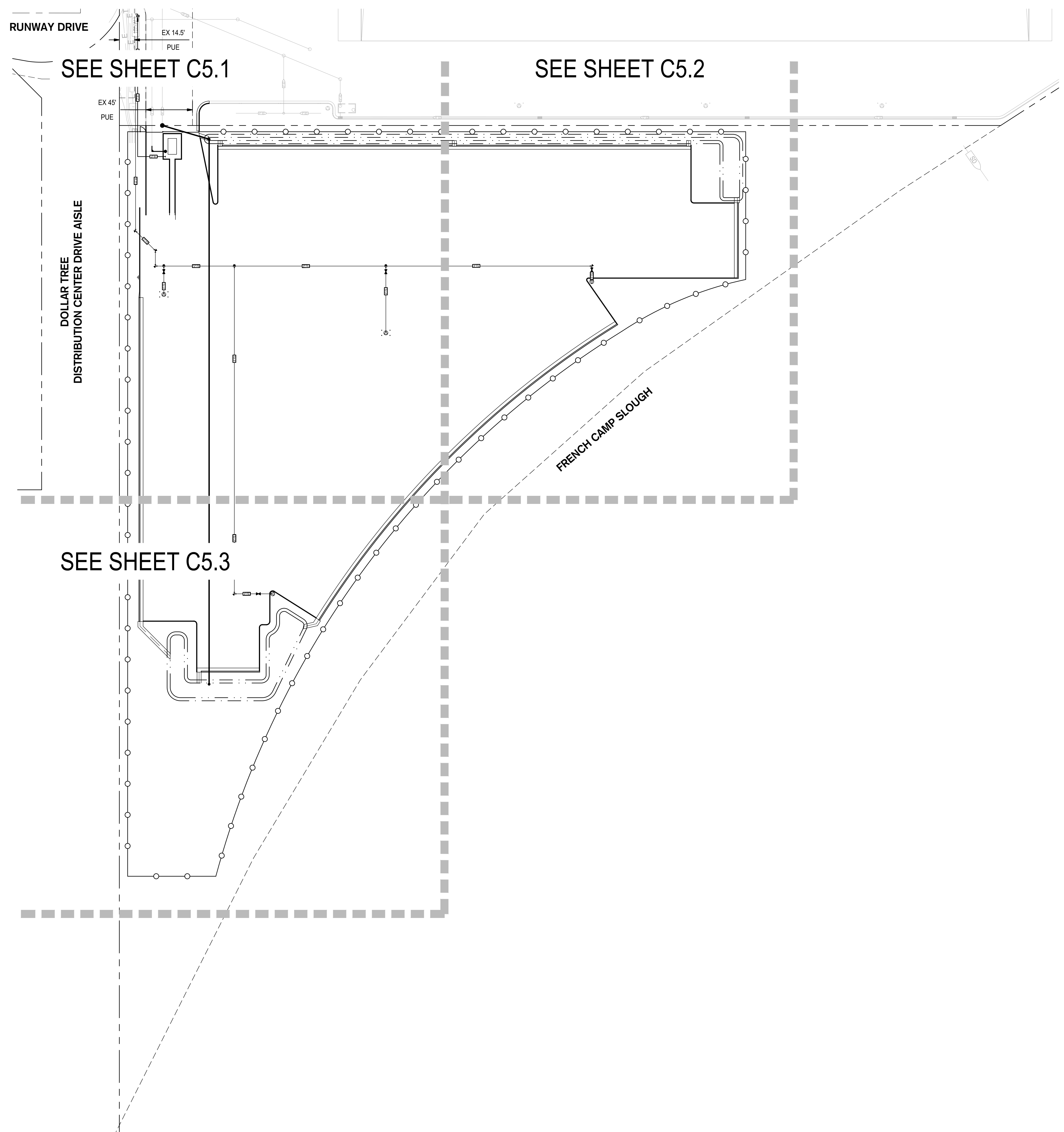
- PROPOSED MANHOLE
- PROPOSED CATCH BASIN
- PROPOSED DRAIN INLET
- PROPOSED AREA DRAIN
- PROPOSED SANITARY SEWER CLEANOUT
- ▶ SHUT OFF VALVE
- ▲ PROPOSED FIRE DEPARTMENT CONNECTION
- PROPOSED FIRE HYDRANT
- SS PROPOSED SANITARY SEWER LINE
- SD PROPOSED STORM DRAIN PIPE
- SD PERF PROPOSED PERFORATED STORM DRAIN LINE
- W PROPOSED WATER LINE
- FS PROPOSED FIRE SERVICE LINE
- FH PROPOSED FIRE HYDRANT LINE

EXISTING UTILITIES:

1. EXISTING UTILITY DATA SHOWN ON THIS LAYOUT WAS OBTAINED FROM A SURVEY OF THE VISIBLE FEATURES AT THE SITE AND PUBLIC RECORD MAPS OBTAINED FROM UTILITY COMPANIES.
2. THE CONTRACTOR SHALL NOTIFY ALL UTILITY COMPANIES WITHIN 48 HOURS PRIOR OF CONSTRUCTION TO LOCATE AND TAG THEIR UNDERGROUND FACILITIES PRIOR TO EXCAVATION.
3. THE CONTRACTOR NEEDS TO ALLOW FOR THE POSSIBILITY OF UNDETECTED UNDERGROUND UTILITIES. ALSO, THE CONTRACTOR MUST ALLOW FOR CHANGES DUE TO UTILITIES BEING IN LOCATIONS DIFFERENT FROM THOSE SHOWN ON THE UTILITY RECORD DRAWINGS. THE CONTRACTOR IS RESPONSIBLE FOR LOCATING AND EXPOSING CONFLICTS PRIOR TO CONSTRUCTION.
4. THE LOCATIONS AND DEPTHS OF EXISTING UTILITIES SHOWN ON THESE PLANS ARE APPROXIMATE ONLY. ACTUAL LOCATIONS AND DEPTHS OF UTILITIES MUST BE VERIFIED BY THE CONTRACTOR PRIOR TO CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO LOCATE UTILITY SERVICE LINES AS REQUIRED FOR CONSTRUCTION AND NOTIFY THE ENGINEER OF ANY CONFLICTS IMMEDIATELY. ANY DAMAGE BY THE CONTRACTOR TO EXISTING UTILITIES, WHETHER SHOWN ON THE PLANS OR NOT, SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO REPAIR, AT HIS EXPENSE.

UTILITY CONSTRUCTION NOTES:

1. ALL MATERIALS AND CONSTRUCTION PROCEDURES WITHIN THE SCOPE OF THIS CONTRACT SHALL CONFORM TO ALL APPLICABLE CITY OF STOCKTON STANDARD SPECIFICATIONS (LATEST EDITION) AND THE CALIFORNIA DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS (LATEST EDITION).



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- ATHLETIC FACILITY DESIGN

REVISIONS
No. Date Description

PROJECT

TRAILER STORAGE AVIATION DRIVE

6440 AVIATION DRIVE
STOCKTON, CA



SHEET TITLE

UTILITY PLAN I

Proj Mgr AKM
 Drawn by JR
 Date 04/20/2021
 Job No. 21081

SHEET:

C5.1

OF: 18

Exhibit 1

LEGEND

- PROPOSED MANHOLE
- PROPOSED DRAIN INLET
- PROPOSED SANITARY SEWER CLEANOUT
- ▶ SHUT OFF VALVE
- ⊕ PROPOSED FIRE HYDRANT
- SS— PROPOSED SANITARY SEWER LINE
- SD— PROPOSED STORM DRAIN PIPE
- W— PROPOSED WATER LINE
- FS— PROPOSED FIRE SERVICE LINE

UTILITY CONSTRUCTION NOTES:

- ALL MATERIALS AND CONSTRUCTION PROCEDURES WITHIN THE SCOPE OF THIS CONTRACT SHALL CONFORM TO ALL APPLICABLE CITY OF STOCKTON STANDARD SPECIFICATIONS (LATEST EDITION) AND THE CALIFORNIA DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS (LATEST EDITION).

EXISTING UTILITIES:

- EXISTING UTILITY DATA SHOWN ON THIS LAYOUT WAS OBTAINED FROM A SURVEY OF THE VISIBLE FEATURES AT THE SITE AND PUBLIC RECORD MAPS OBTAINED FROM UTILITY COMPANIES.
- THE CONTRACTOR SHALL NOTIFY ALL UTILITY COMPANIES WITHIN 48 HOURS PRIOR TO CONSTRUCTION TO LOCATE AND TAG THEIR UNDERGROUND FACILITIES PRIOR TO EXCAVATION.
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- THE LOCATIONS AND DEPTHS OF EXISTING UTILITIES SHOWN ON THESE PLANS ARE APPROXIMATE ONLY. ACTUAL LOCATIONS AND DEPTHS OF UTILITIES MUST BE VERIFIED BY THE CONTRACTOR PRIOR TO CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO LOCATE UTILITY SERVICE LINES AS REQUIRED FOR CONSTRUCTION AND NOTIFY THE ENGINEER OF ANY CONFLICTS IMMEDIATELY. ANY DAMAGE BY THE CONTRACTOR TO EXISTING UTILITIES, WHETHER SHOWN ON THE PLANS OR NOT, SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO REPAIR, AT HIS EXPENSE.

WATER KEY NOTES

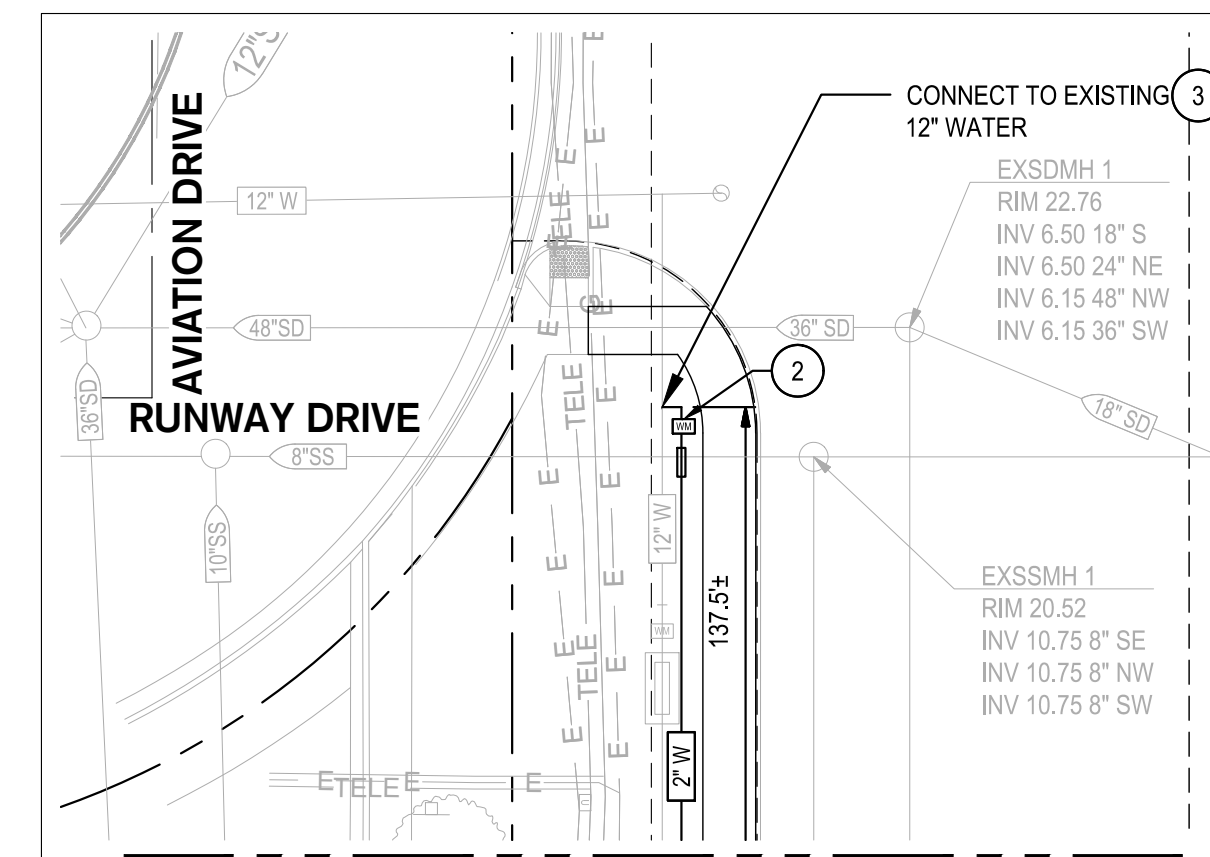
- ① FIRE HYDRANT PER C.O.S. STD. DWG. NO. W-13.
- ② 2" DOMESTIC WATER SERVICE AND METER PER C.O.S. STD. DWG. NO. W-3, WITH BACKFLOW PREVENTION DEVICE PER C.O.S. STD. DWG. NO. W-7.
- ③ CONNECT TO EXISTING 12" WATER STUB PER C.O.S. STD. DWG. NO. W-3
- ④ VALVE AND VALVE BOX PER C.O.S. STD. DWG. NO. W-11.
- ⑤ THRUST BLOCK PER C.O.S. STD. DWG. NO. W-12.
- ⑥ GUARD SHACK POINT OF CONNECTION, SEE PLUMBING PLANS FOR CONTINUATION

STORM DRAIN KEY NOTES

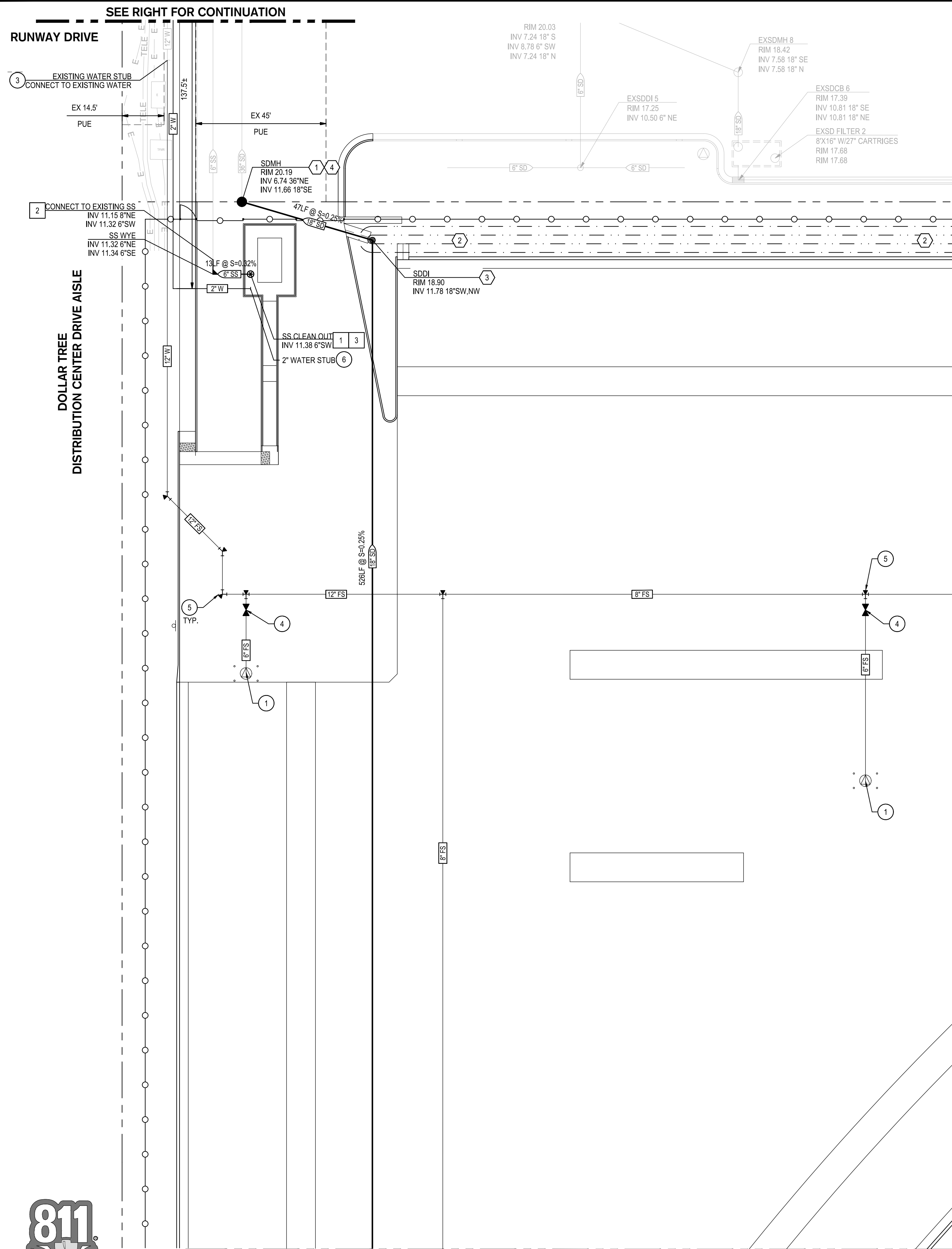
- ① STORM DRAIN MANHOLE PER C.O.S. STD. DWG. NO. S-10.
- ② BIORETENTION AREA PER DETAIL 1 SHEET C7.1
- ③ OVERFLOW DRAIN INLET PER DETAIL 1 SHEET C7.1. PROVIDE STORM DRAIN MESSAGE AND SIGNAGE PER S-1 OF THE CITY OF STOCKTON AND COUNTY OF SAN JOAQUIN FINAL STORMWATER QUALITY CONTROL CRITERIA PLAN.
- ④ CONNECT TO EXISTING 36" STORM DRAIN STUB. CONTRACTOR TO VERIFY LOCATION AND INVERT PRIOR TO CONSTRUCTION.
- ⑤ STORM DRAIN MANHOLE PER C.O.S. STD. DWG. NO. S-10.

SANITARY SEWER KEY NOTES

- ① SANITARY SEWER CLEANOUT PER C.O.S. STD. DWG. NO. S-18.
- ② CONNECT TO EXISTING 6" SANITARY SEWER STUB. CONTRACTOR TO FIELD VERIFY LOCATION AND INVERT PRIOR TO CONSTRUCTION.
- ③ GUARD SHACK POINT OF CONNECTION, SEE PLUMBING PLANS FOR CONTINUATION



SEE LEFT FOR CONTINUATION



FOR CONTINUATION SEE SHEET C5.3

FOR CONTINUATION SEE SHEET C5.2

SEE RIGHT FOR CONTINUATION



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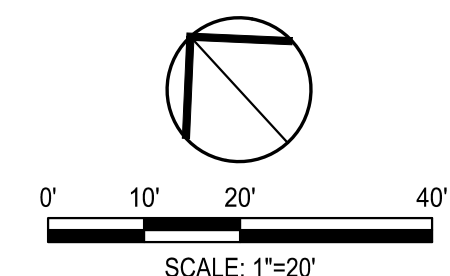


Exhibit 1



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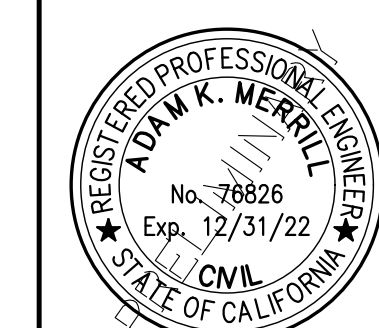
REVISIONS

No. Date Description

PROJECT

TRAILER STORAGE AVIATION DRIVE

6440 AVIATION DRIVE
STOCKTON, CA



SHEET TITLE

UTILITY PLAN II

Proj Mgr AKM

Drawn by JR

Date 04/20/2021

Job No. 21081

SHEET:

C5.2

OF: 18

LEGEND

- PROPOSED MANHOLE
- PROPOSED DRAIN INLET
- PROPOSED SANITARY SEWER CLEANOUT
- ▶ SHUT OFF VALVE
- △ PROPOSED FIRE HYDRANT
- SS PROPOSED SANITARY SEWER LINE
- SD PROPOSED STORM DRAIN PIPE
- W PROPOSED WATER LINE
- FS PROPOSED FIRE SERVICE LINE

UTILITY CONSTRUCTION NOTES:

- ALL MATERIALS AND CONSTRUCTION PROCEDURES WITHIN THE SCOPE OF THIS CONTRACT SHALL CONFORM TO ALL APPLICABLE CITY OF STOCKTON STANDARD SPECIFICATIONS (LATEST EDITION) AND THE CALIFORNIA DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS (LATEST EDITION).

EXISTING UTILITIES:

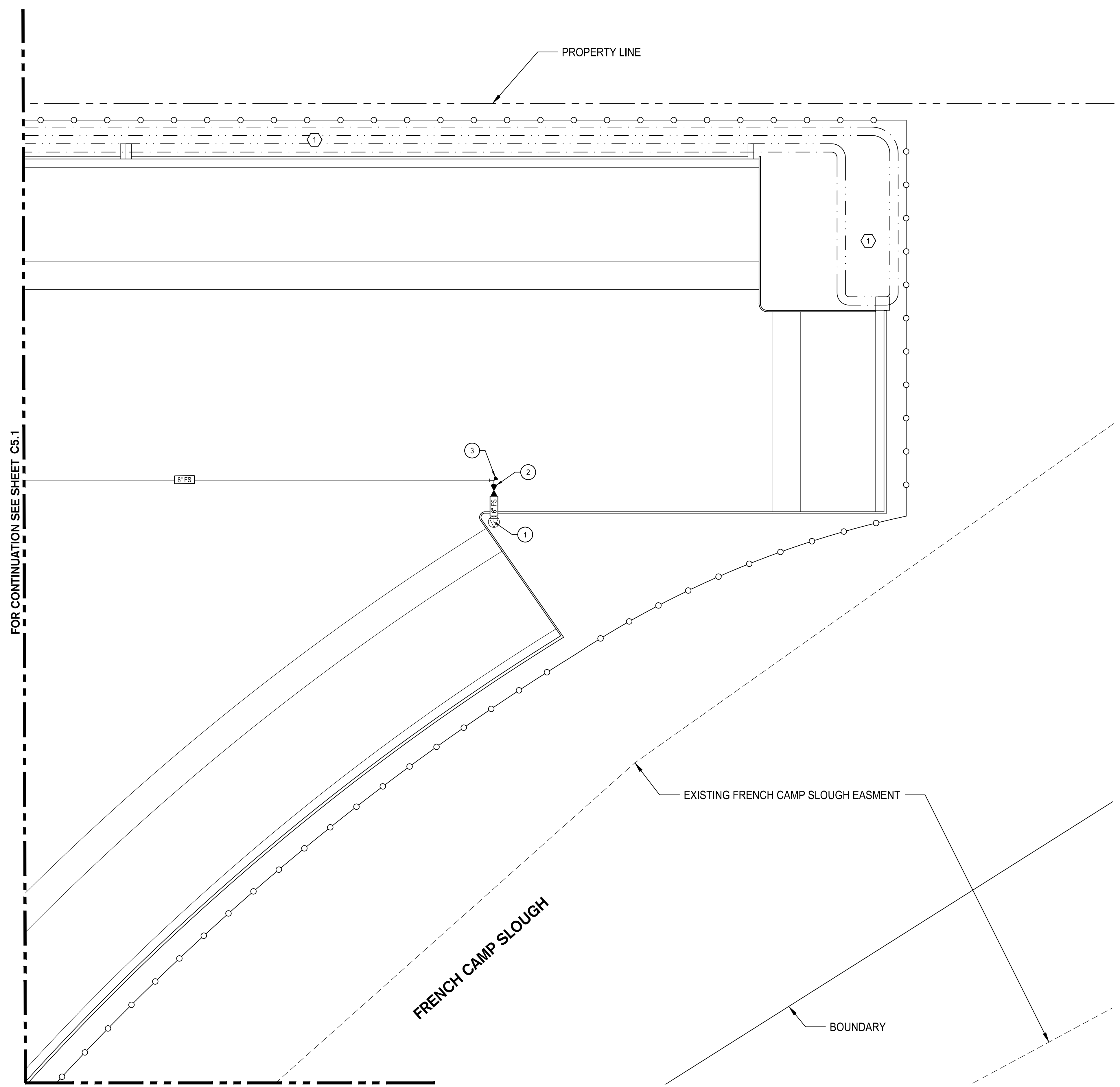
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WATER KEY NOTES

- ① FIRE HYDRANT PER C.O.S. STD. DWG. NO. W-13.
- ② VALVE AND VALVE BOX PER C.O.S. STD. DWG. NO. W-11.
- ③ THRUST BLOCK PER C.O.S. STD. DWG. NO. W-12.

STORM DRAIN KEY NOTES

- ① BIORETENTION AREA PER DETAIL 1 SHEET C7.1



FOR CONTINUATION SEE SHEET C5.1

FOR CONTINUATION SEE SHEET C5.3



Know what's below.
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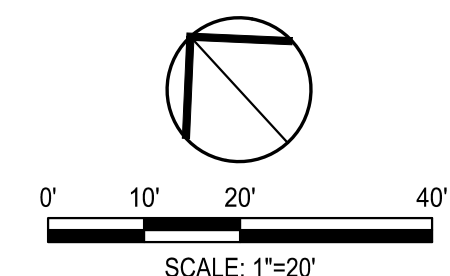


Exhibit 1



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REVISIONS
No. Date Description

PROJECT

TRAILER STORAGE AVIATION DRIVE

6440 AVIATION DRIVE
STOCKTON, CA



SHEET TITLE

UTILITY PLAN III

Proj Mgr **AKM**
Drawn by **JR**
Date **04/20/2021**
Job No. **21081**

SHEET:

C5.3

OF: 18

LEGEND

- PROPOSED MANHOLE
- PROPOSED DRAIN INLET
- PROPOSED SANITARY SEWER CLEANOUT
- ✂ SHUT OFF VALVE
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- FS PROPOSED FIRE SERVICE LINE

UTILITY CONSTRUCTION NOTES:

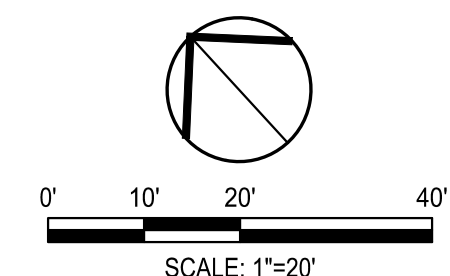
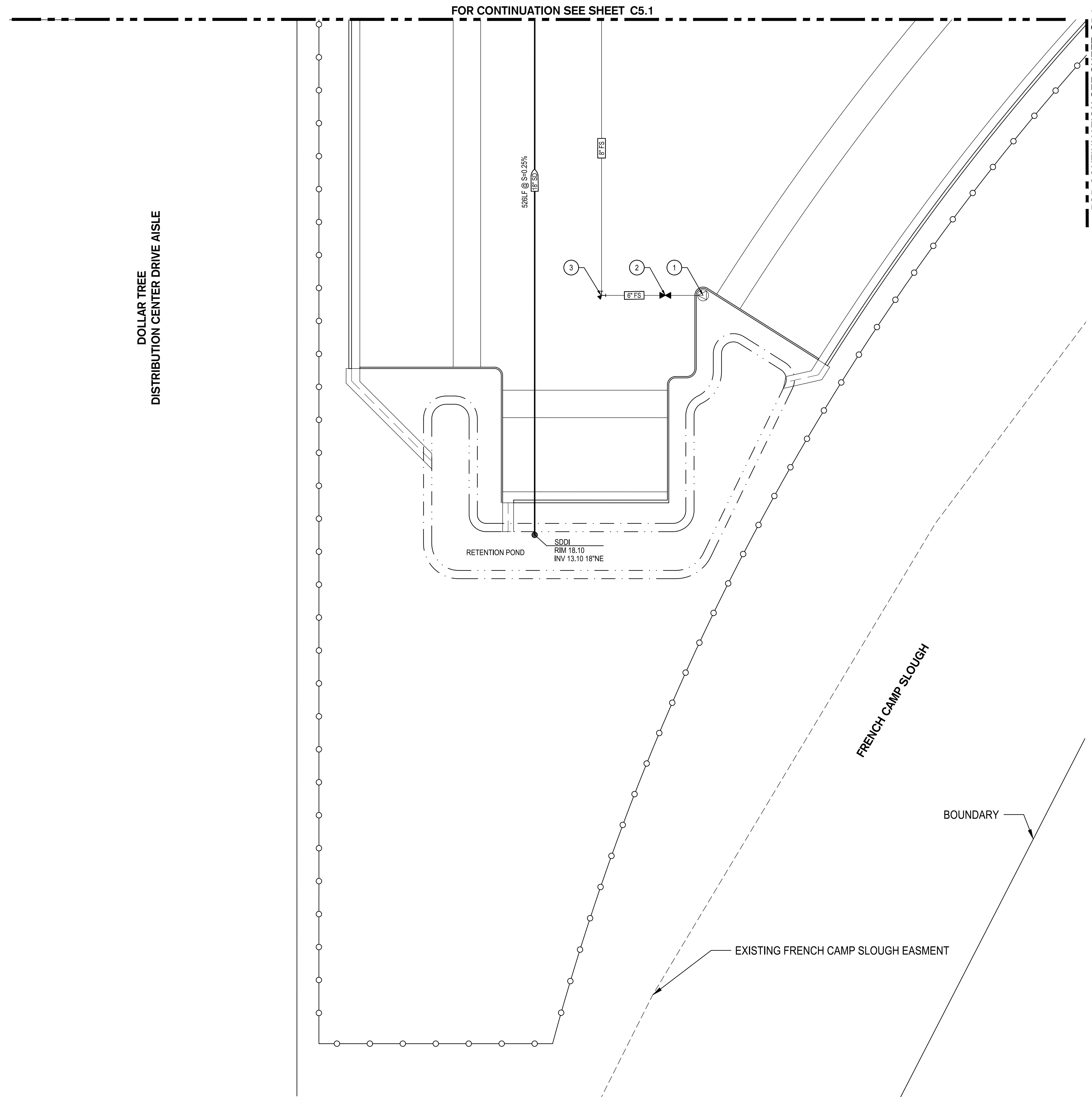
1. ALL MATERIALS AND CONSTRUCTION PROCEDURES WITHIN THE SCOPE OF THIS CONTRACT SHALL CONFORM TO ALL APPLICABLE CITY OF STOCKTON STANDARD SPECIFICATIONS (LATEST EDITION) AND THE CALIFORNIA DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS (LATEST EDITION).

EXISTING UTILITIES:

1. EXISTING UTILITY DATA SHOWN ON THIS LAYOUT WAS OBTAINED FROM A SURVEY OF THE VISIBLE FEATURES AT THE SITE AND PUBLIC RECORD MAPS OBTAINED FROM UTILITY COMPANIES.
2. THE CONTRACTOR SHALL NOTIFY ALL UTILITY COMPANIES WITHIN 48 HOURS PRIOR OF CONSTRUCTION TO LOCATE AND TAG THEIR UNDERGROUND FACILITIES PRIOR TO EXCAVATION.
3. THE CONTRACTOR NEEDS TO ALLOW FOR THE POSSIBILITY OF UNDETECTED UNDERGROUND UTILITIES. ALSO, THE CONTRACTOR MUST ALLOW FOR CHANGES DUE TO UTILITIES BEING IN LOCATIONS DIFFERENT FROM THOSE SHOWN ON THE UTILITY RECORD DRAWINGS. THE CONTRACTOR IS RESPONSIBLE FOR LOCATING AND EXPOSING CONFLICTS PRIOR TO CONSTRUCTION.
4. THE LOCATIONS AND DEPTHS OF EXISTING UTILITIES SHOWN ON THESE PLANS ARE APPROXIMATE ONLY. ACTUAL LOCATIONS AND DEPTHS OF UTILITIES MUST BE VERIFIED BY THE CONTRACTOR PRIOR TO CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO LOCATE UTILITY SERVICE LINES AS REQUIRED FOR CONSTRUCTION AND NOTIFY THE ENGINEER OF ANY CONFLICTS IMMEDIATELY. ANY DAMAGE BY THE CONTRACTOR TO EXISTING UTILITIES, WHETHER SHOWN ON THE PLANS OR NOT, SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO REPAIR, AT HIS EXPENSE.

WATER KEY NOTES

- ① FIRE HYDRANT PER C.O.S. STD. DWG. NO. W-13.
- ② VALVE AND VALVE BOX PER C.O.S. STD. DWG. NO. W-11.
- ③ THRUST BLOCK PER C.O.S. STD. DWG. NO. W-12.



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Exhibit 1



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REVISIONS
No. Date Description

PROJECT

TRAILER STORAGE AVIATION DRIVE

6440 AVIATION DRIVE
STOCKTON, CA



SHEET TITLE

EROSION CONTROL PLAN

Proj Mgr AKM

Drawn by JR

Date 04/20/2021

Job No. 21081

SHEET:

C6.0

OF: 18

EROSION CONTROL GENERAL NOTES

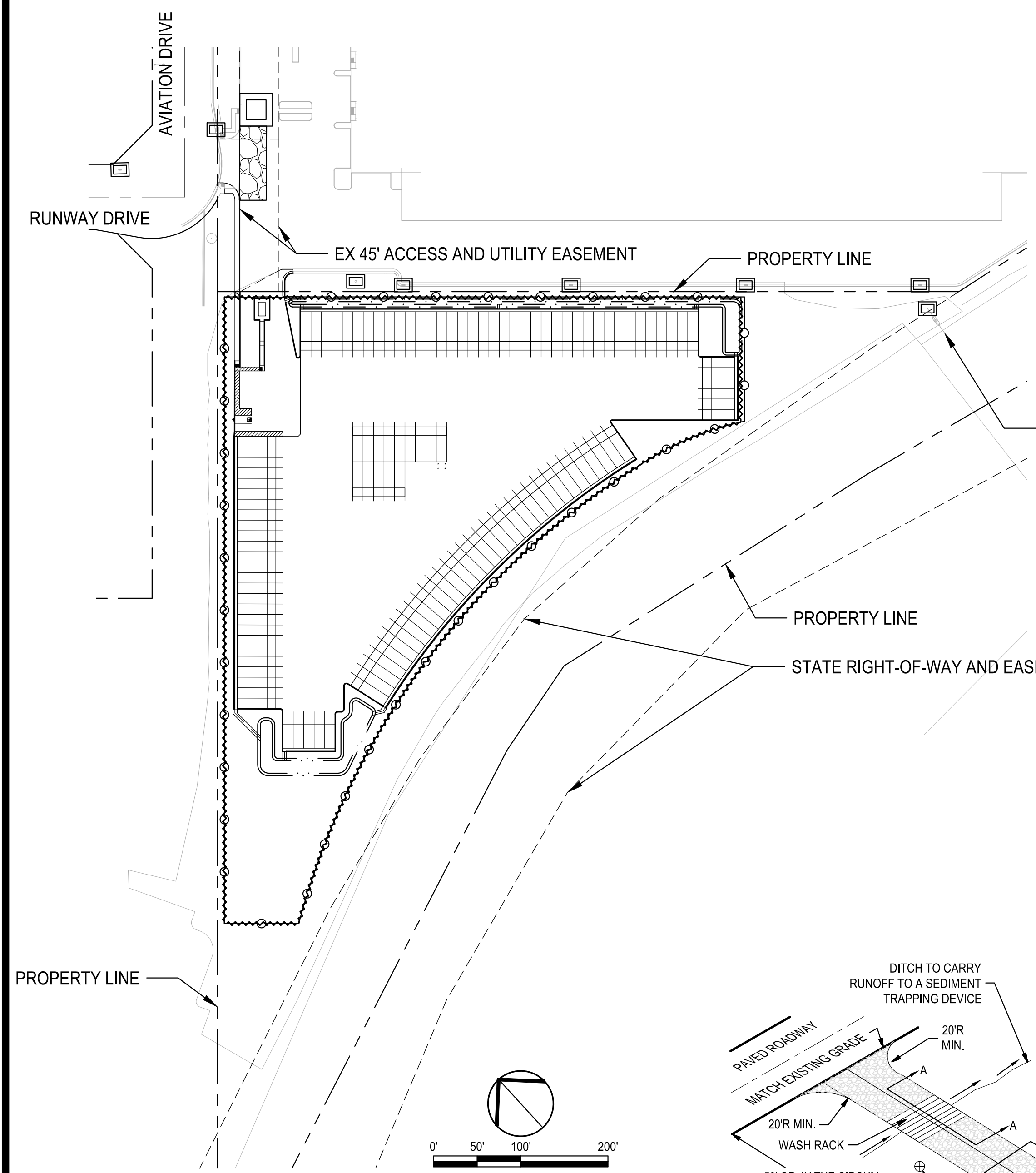
- PLANS ARE DIAGRAMMATIC AND ARE NOT INTENDED TO SHOW ALL OFFSETS. THE SITE IS DYNAMIC AND CHANGES ON A DAILY BASIS. CHANGES SHOULD BE MADE ACCORDING TO EXISTING CONDITIONS. BECAUSE IT IS IMPOSSIBLE TO PREDICT ALL POSSIBLE SITUATIONS, CONTRACTOR SHALL USE BEST MANAGEMENT PRACTICES TO ENSURE QUALITY CONTROL.
- THE CONTRACTOR SHALL REVIEW THE CURRENT STORM WATER POLLUTION PREVENTION PLAN (SWPPP). IT IS THE CONTRACTOR'S SOLE RESPONSIBILITY FOR CONDUCTING HIS/HER OPERATIONS IN ADHERENCE TO THE SWPPP. THE CONTRACTOR IS RESPONSIBLE FOR ANY FINES, DELAYS, AND/OR DAMAGES RESULTING FROM ANY STATE WATER QUALITY CONTROL BOARD SANCTIONS CAUSED BY THE OPERATION OF THE CONTRACTOR OR HIS/HER SUBCONTRACTORS.
- THE FOLLOWING PLANS ARE ACCURATE FOR EROSION CONTROL PURPOSES ONLY. THE CONTRACTOR SHALL FOLLOW THESE PLANS UNLESS FIELD CONDITIONS DICTATE MODIFICATION. IF MODIFICATION IS NECESSARY, A SWPPP AMENDMENT MUST BE DONE. THIS MAY REQUIRE MODIFICATION TO THESE DRAWINGS AND ENGINEER CONCURRENCE.
- INSPECT AND REPAIR FILTERS AFTER EACH STORM EVENT. REMOVE SEDIMENT WHEN 1/2 OF THE FILTER DEPTH HAS BEEN FILLED. REMOVED SEDIMENT SHALL BE DEPOSITED IN AN AREA TRIBUTARY TO A SEDIMENT BASIN OR OTHER FILTERING MEASURE. SEDIMENT AND GRAVEL SHALL BE IMMEDIATELY REMOVED FROM PAVEMENT OF ROAD.
- UNFINISHED AND DISTURBED ARE TO BE PROTECTED WITH AN APPLICATION OF BLOWN STRAW AND ORGANIC BINDER.

ITEM	LB/ACRE
STRAW	4,000
ORGANIC BINDER	200
- ALTERNATE INLET PROTECTION SHALL BE USED ON ROADS OPEN TO THE PUBLIC IF ANY HAZARDOUS MATERIALS OR WASTES WHICH HAVE BEEN TREATED, STORED, DISPOSED, SPILLED, OR LEAKED IN SIGNIFICANT QUANTITIES ONTO THE CONSTRUCTION SITE. THE CONTRACTOR SHALL BE RESPONSIBLE TO REMOVE THEM FROM THE SITE AND DISPOSE OF PROPERLY.
- CHLORINATED OR DECHLORINATED WATER SHALL NOT BE DISCHARGED INTO THE STORM DRAIN SYSTEM. THE CONTRACTOR MAY DISPOSE THIS WATER INTO THE SANITARY SEWER SYSTEM UPON APPROVAL BY THE GOVERNING AGENCY.
- THE CONTRACTOR SHALL KEEP MAINTENANCE, INSPECTION, AND REPAIR PROCEDURES TO ENSURE THAT ALL GRADED SURFACES, WALLS, BERMS, DRAINAGE STRUCTURES, VEGETATION, EROSION AND SEDIMENT CONTROL MEASURES, AND OTHER CONTROLS ARE MAINTAINED IN GOOD AND EFFECTIVE CONDITION AND ARE PROMPTLY REPAIRED OR RESTORED WHEN NECESSARY. ANY DEWATERING WATER SHALL NOT BE DISCHARGED DIRECTLY INTO THE STORM WATER SYSTEM, AND SHALL NOT BE DISCHARGED INTO THE SEWER SYSTEM.
- ALL DEWATERING WATER MUST BE CHANNIELED THROUGH AN APPROVED SEDIMENT BARRIER PRIOR TO THE WATER ENTERING THE STORM SYSTEM.

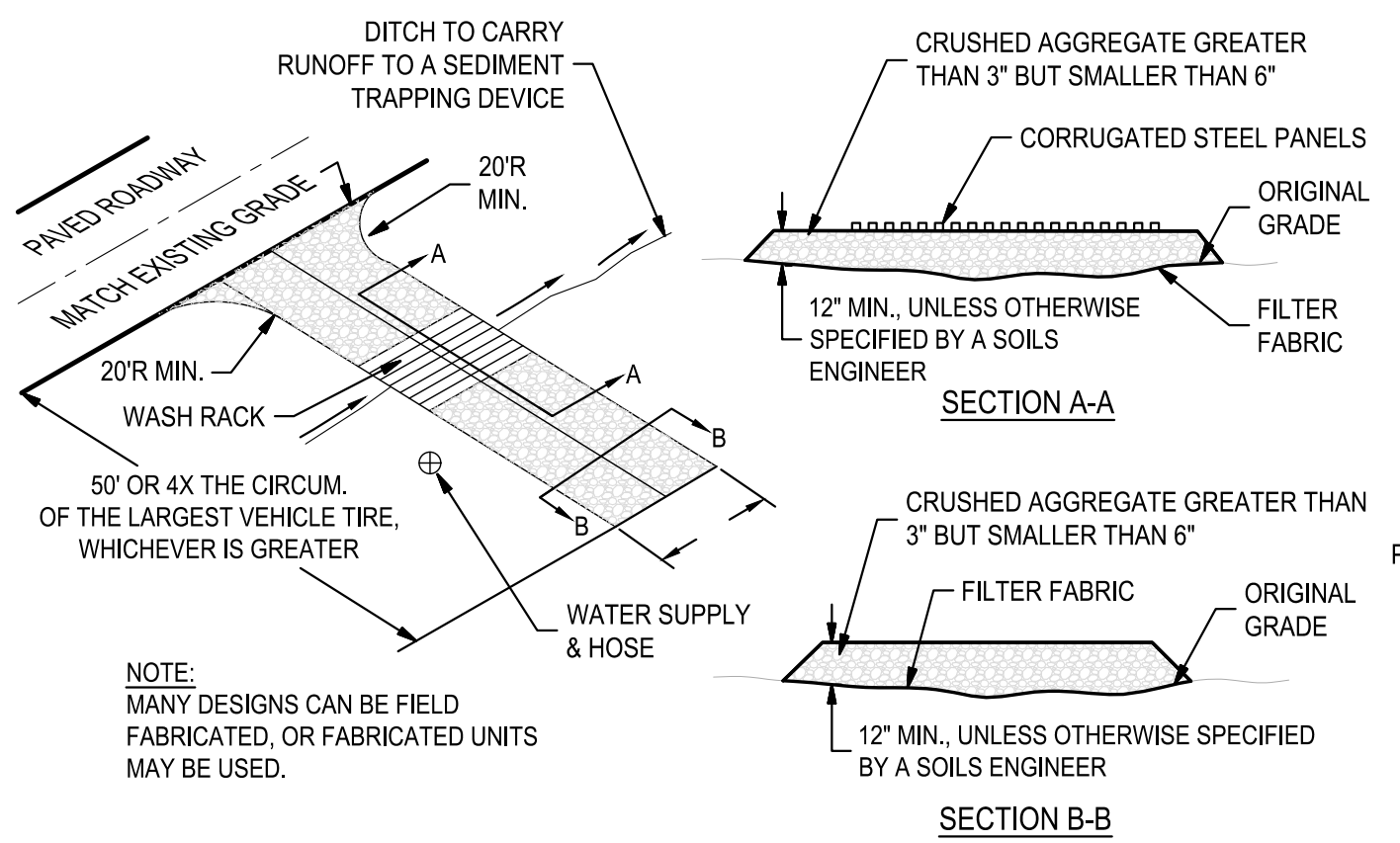
NOTE: THESE BMP'S ARE DIAGRAMMATIC ONLY. ACTUAL INSTALLATION LOCATIONS WILL BE DETERMINED ONSITE BY PROJECT QUALIFIED STORMWATER POLLUTION PREVENTION PLAN PRACTITIONER (QSP). ALTERNATIVE BMP'S THAT DO NOT ADHERE TO THESE DETAILS BUT COMPLY WITH CASQA AND THE INTENT OF THIS PLAN MAY BE SUPPLIED AND USED ON CONSTRUCTION SITE UNDER SUPERVISION OF PROJECT QSP.

EROSION CONTROL LEGEND

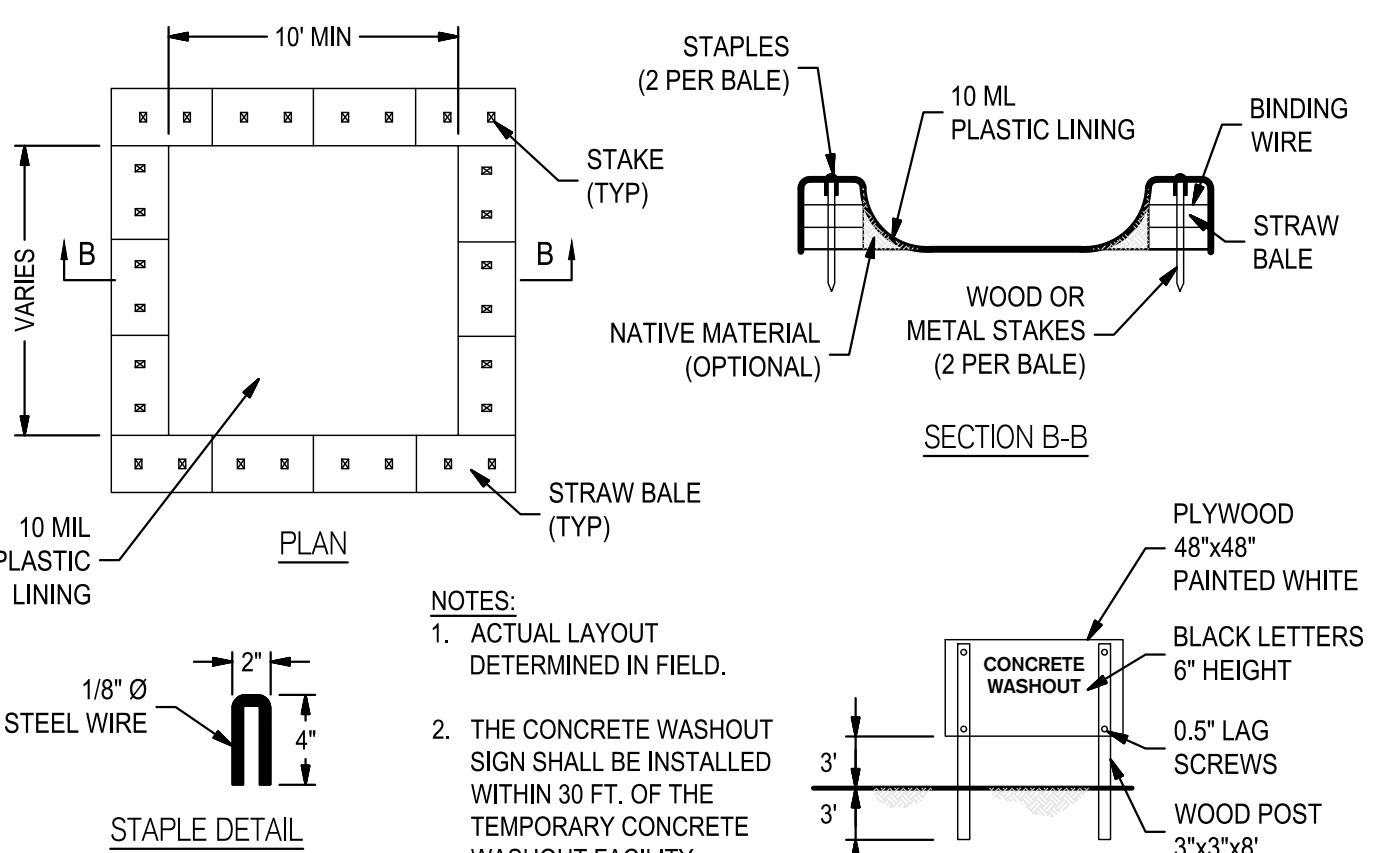
SYMBOL	DESCRIPTION
	FIBER ROLLED WATTLE, SEE DETAIL 5 THIS SHEET
	GRAVEL BAG FILTER AT DROP INLET, SEE DETAIL 3 THIS SHEET
	GRAVEL BAGS, SEE DETAIL THIS SHEET
	STABILIZED CONSTRUCTION ENTRANCE, CONTRACTOR TO DETERMINE SIZE & LOCATION, SEE DETAIL 1 THIS SHEET
	CONCRETE WASHOUT, CONTRACTOR TO DETERMINE LOCATION, SEE DETAIL 2 THIS SHEET



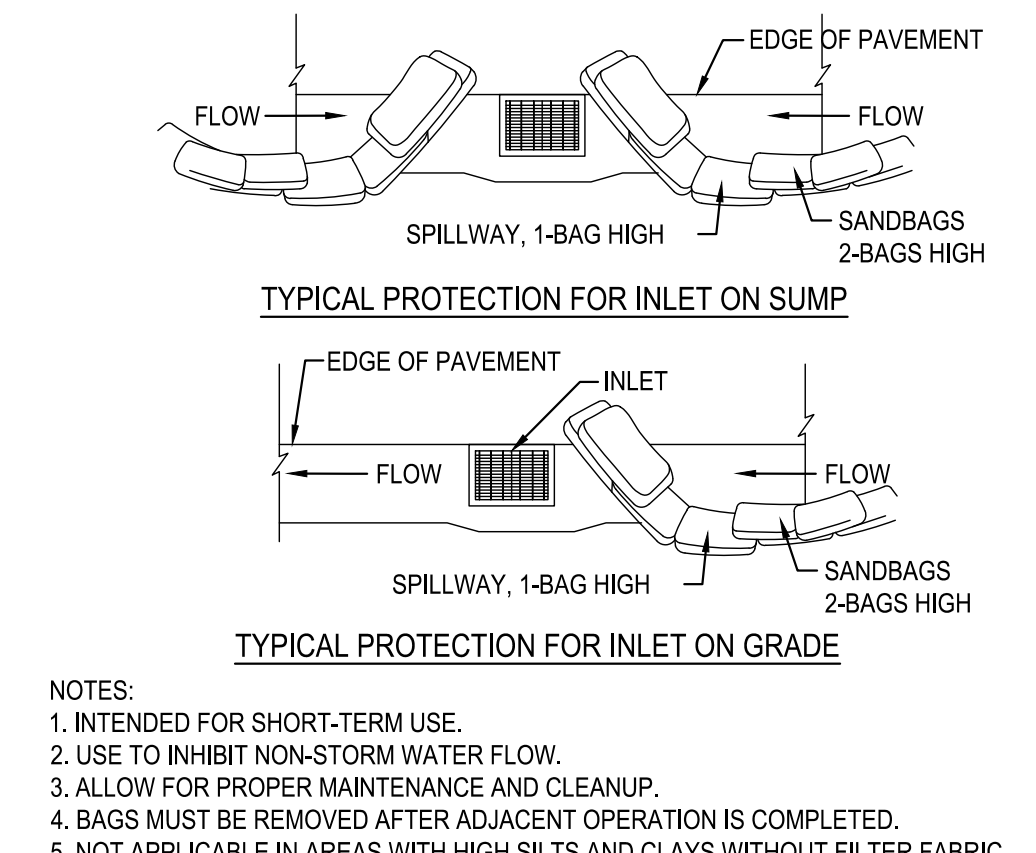
STORM DRAIN PIPE DRAINS TO FRENCH CAMP SLOUGH. CONTRACTOR TO ENSURE PIPE IS SUFFICIENTLY PROTECTED FROM DISCHARGES DURING CONSTRUCTION.



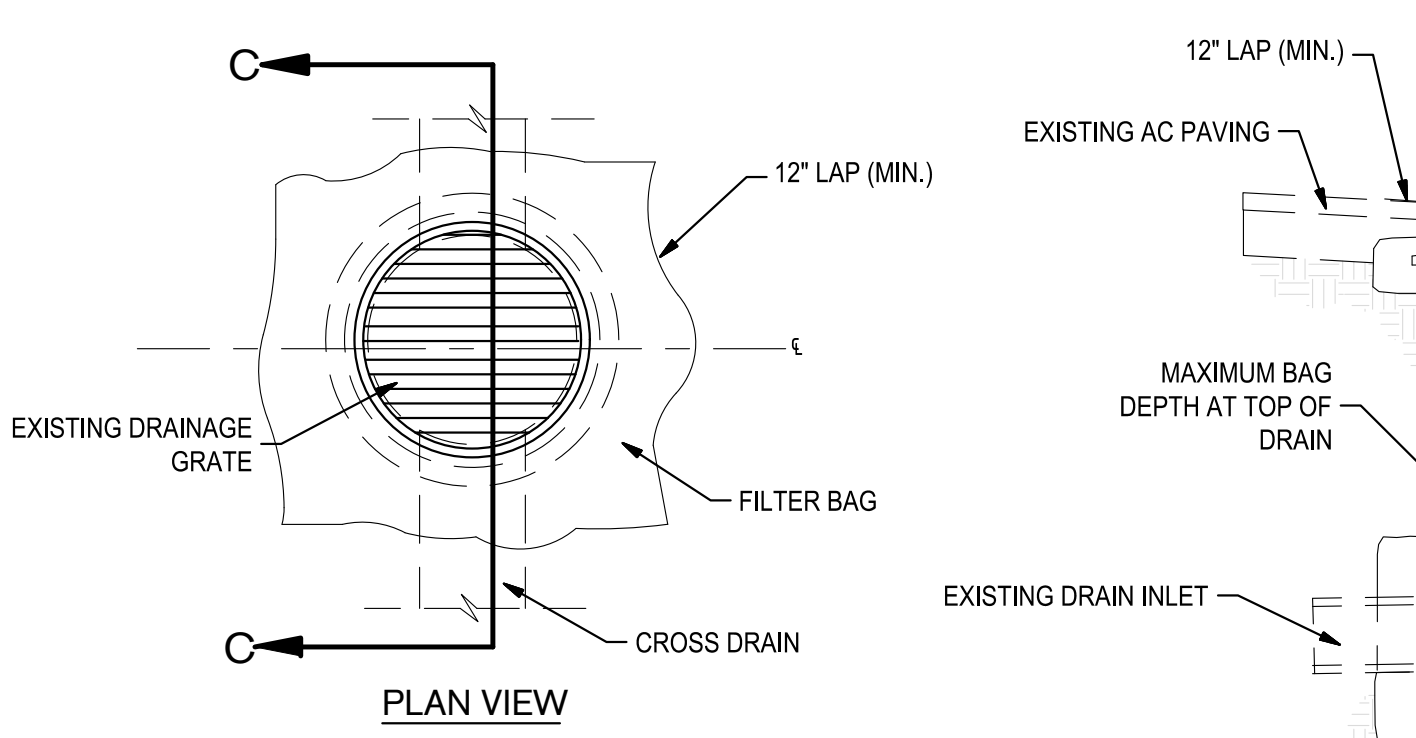
1 STABILIZED CONSTRUCTION ENTRANCE/OUTLET TIRE WASH
NO SCALE



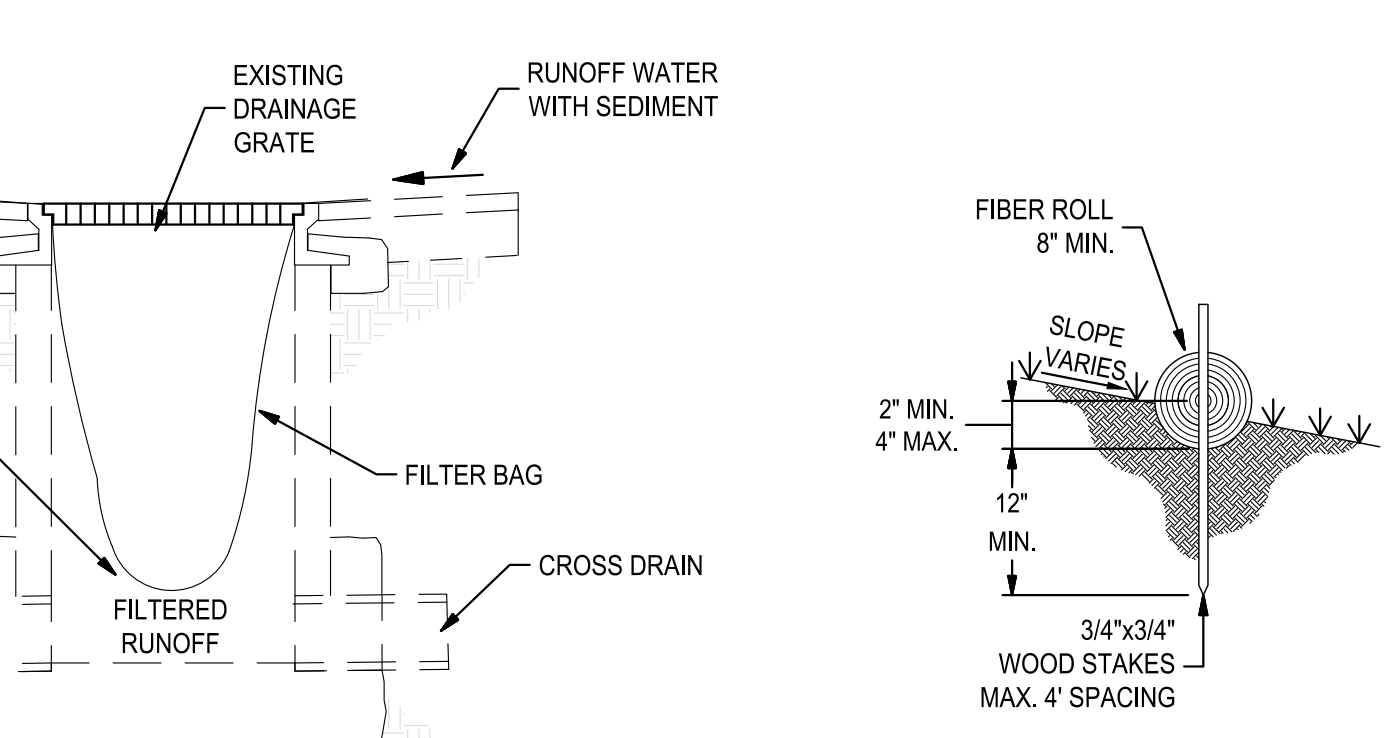
2 CONCRETE WASHOUT
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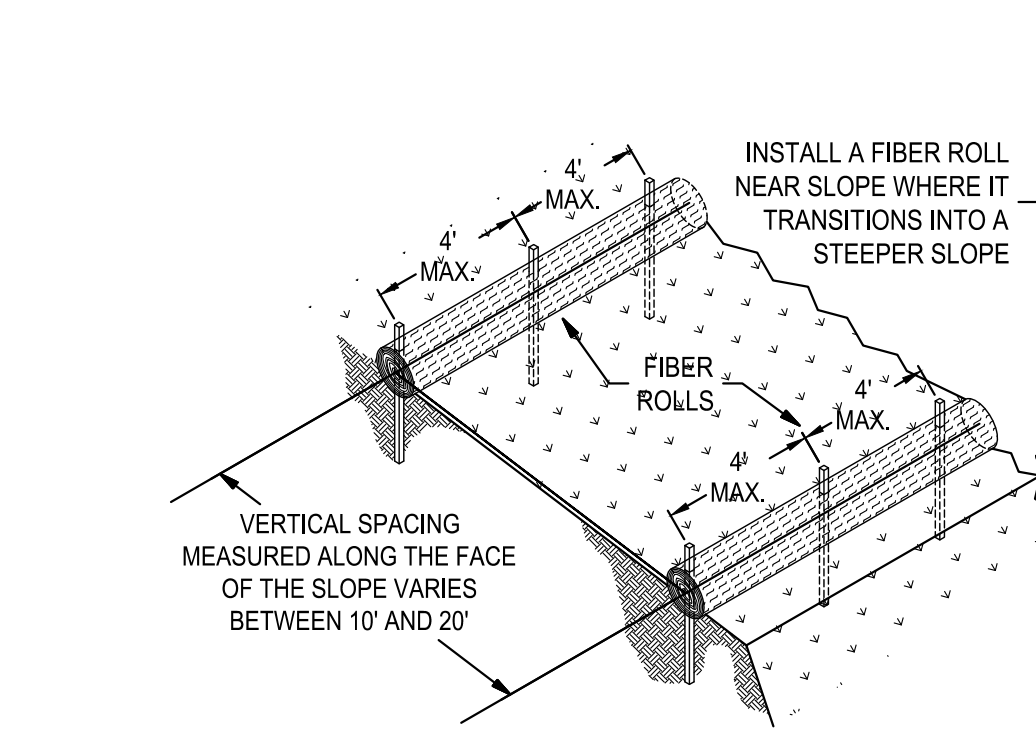
3 DI PROTECTION - TYPE 3 SE-10
NO SCALE



4 TEMPORARY GRAVEL BAG FILTER AT DROP INLET
NO SCALE



5 FIBER ROLLS
NO SCALE



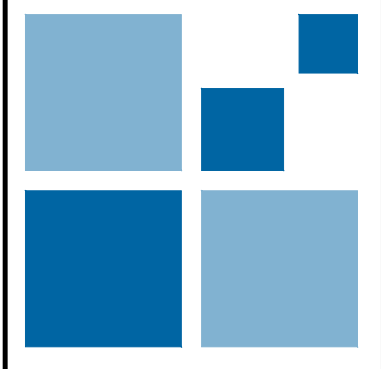
6 TYPICAL FIBER ROLL INSTALLATION

EROSION CONTROL PLAN

1" = 100'



F:\projects\21081_Aviation Drive Site Development\Plans and Graphics\Improvement Plans\CD 8 EROSION CONTROL PLAN.dwg 04/20/21



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PROJECT

TRAILER STORAGE AVIATION DRIVE

6440 AVIATION DRIVE
STOCKTON, CA



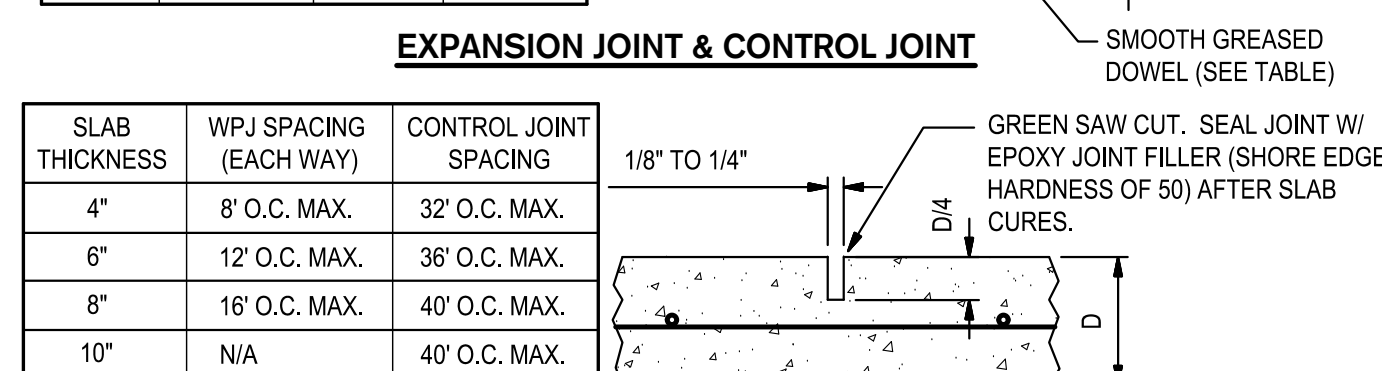
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DETAILS I

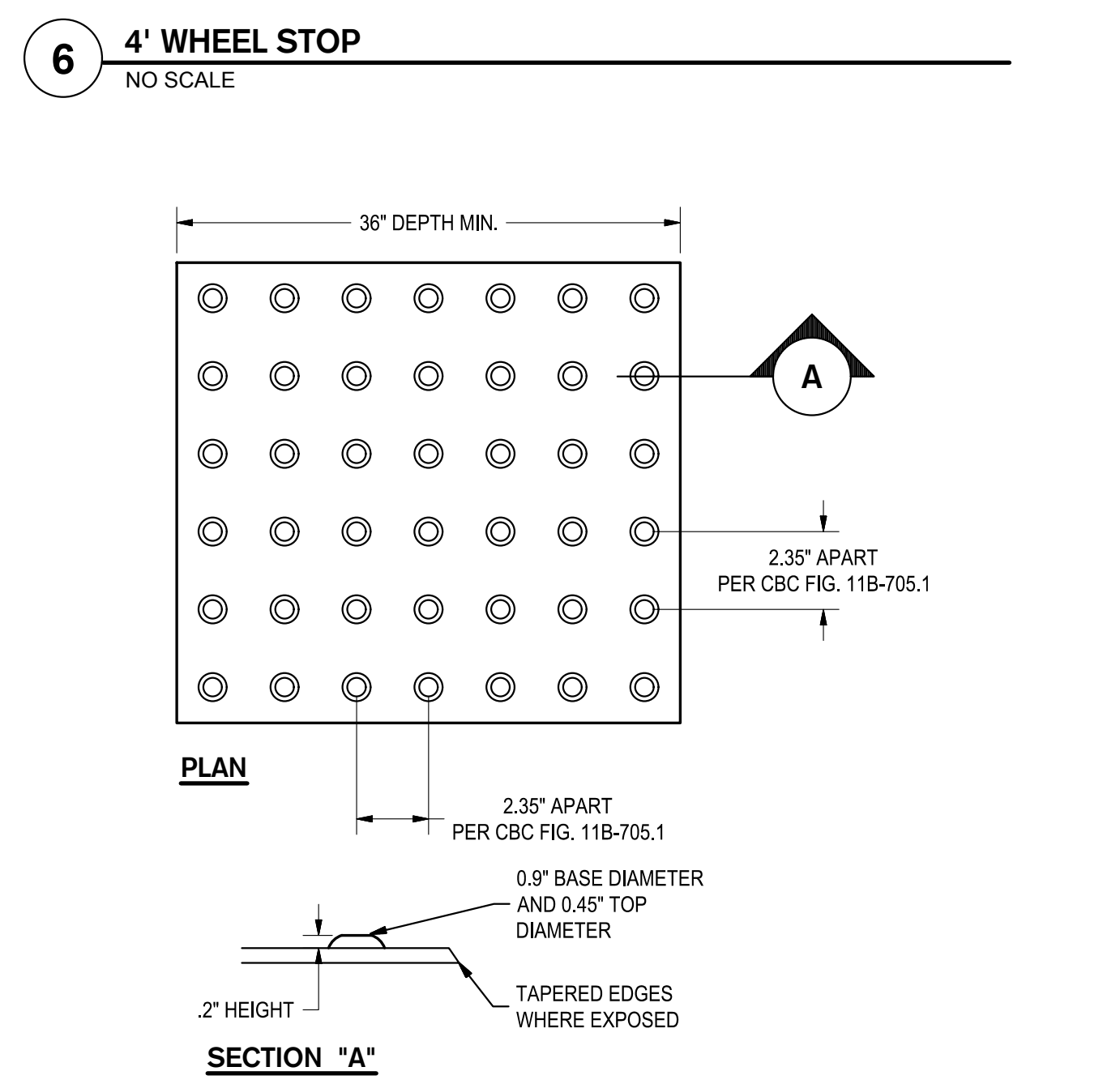
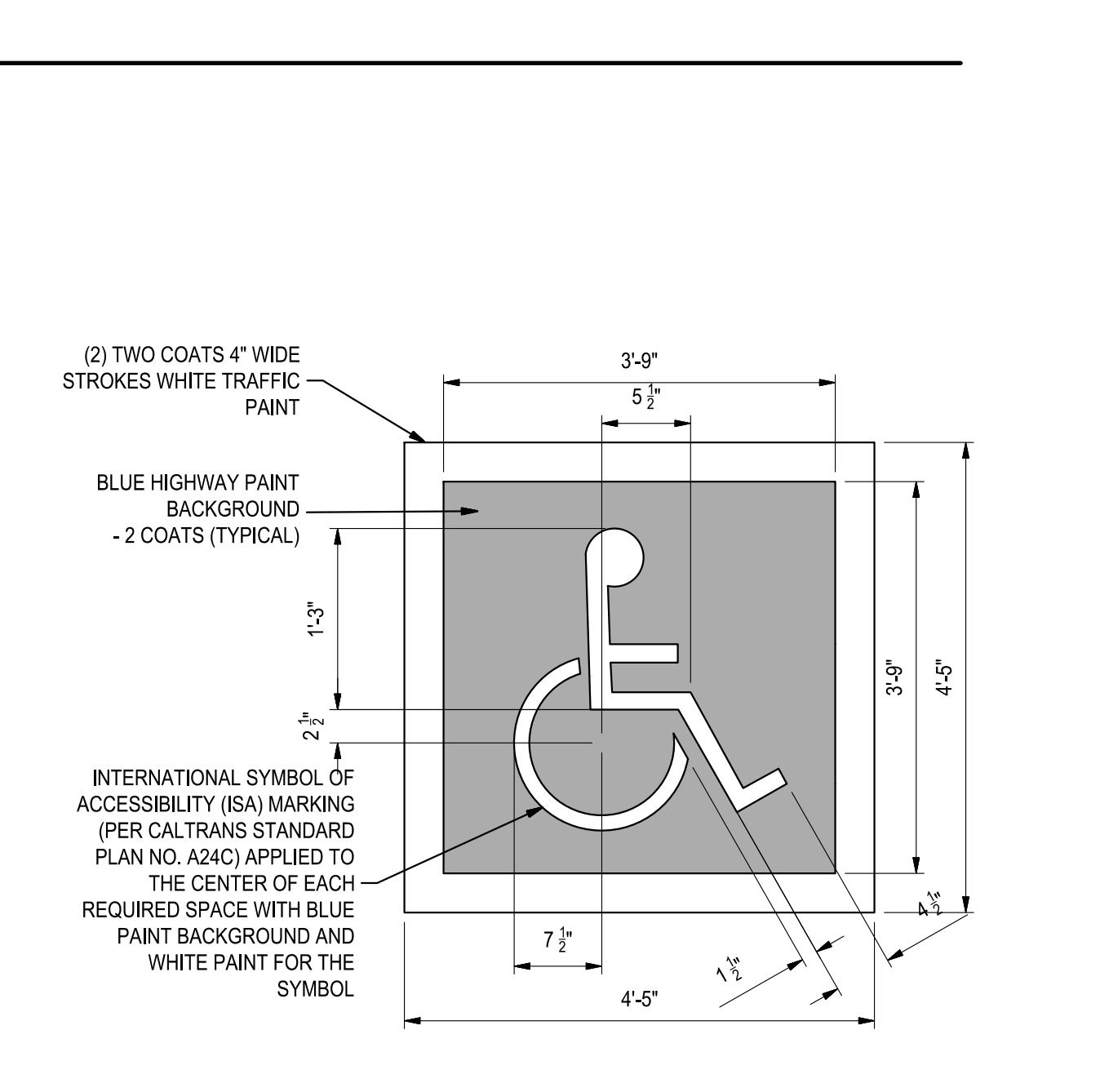
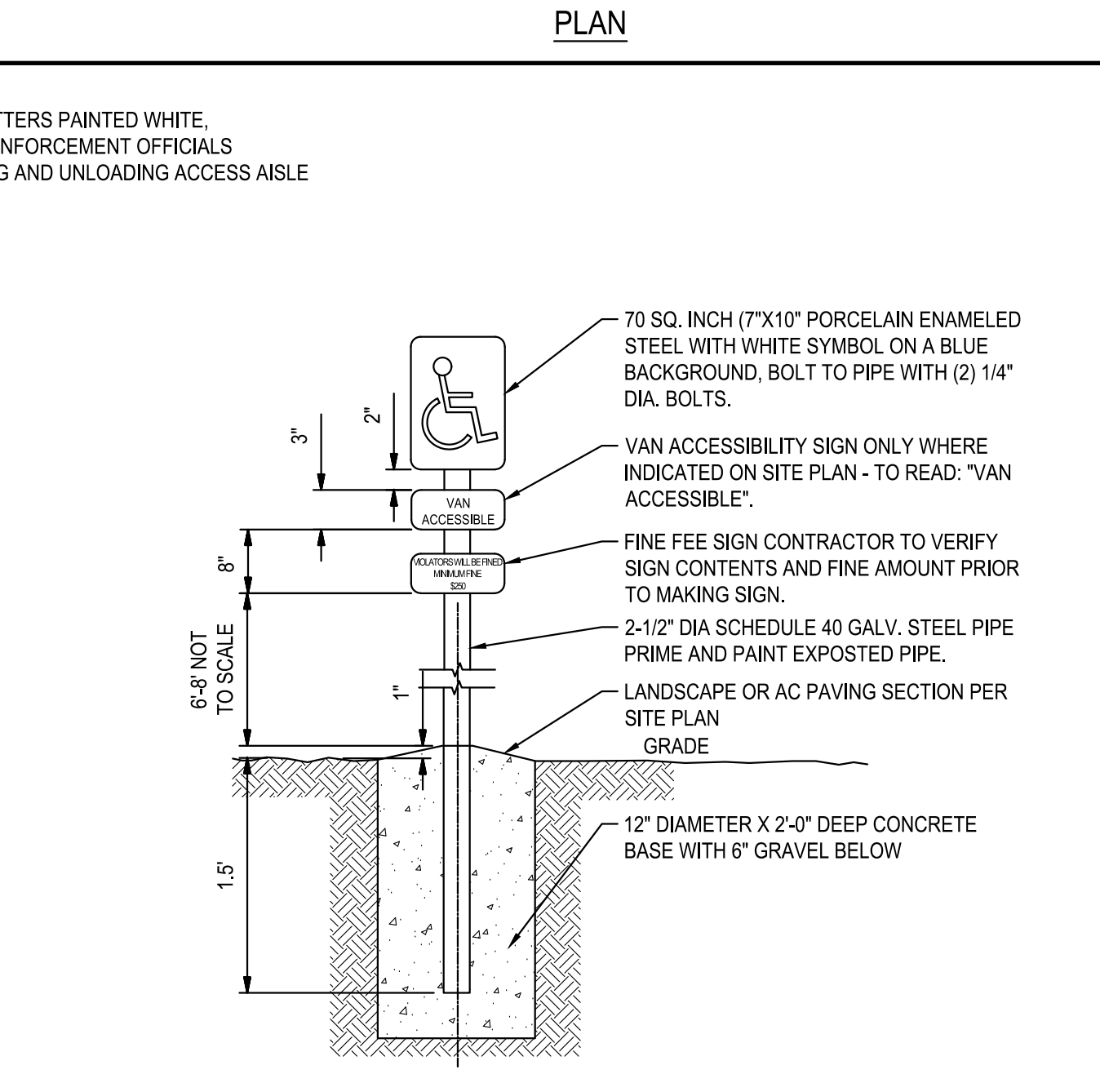
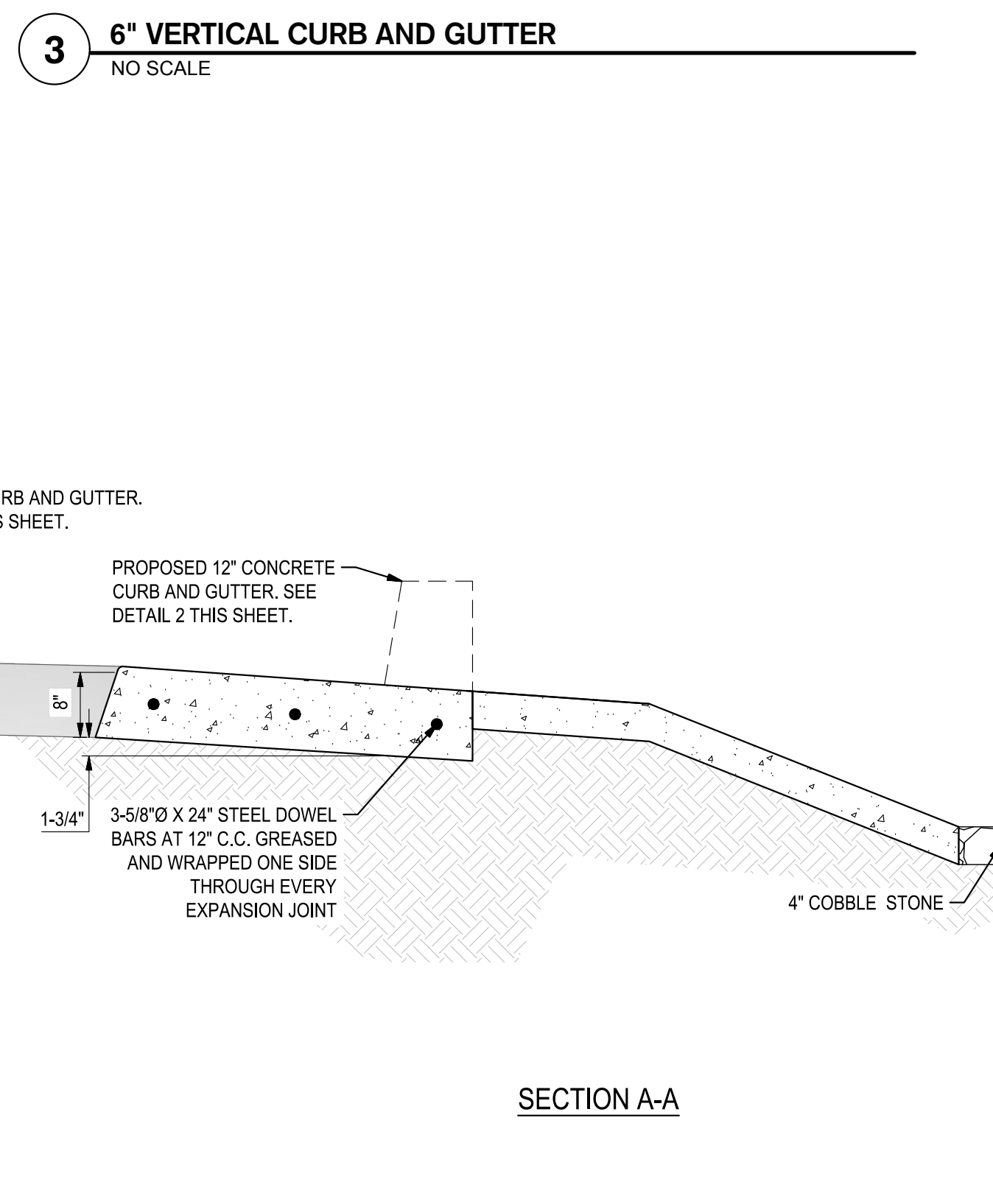
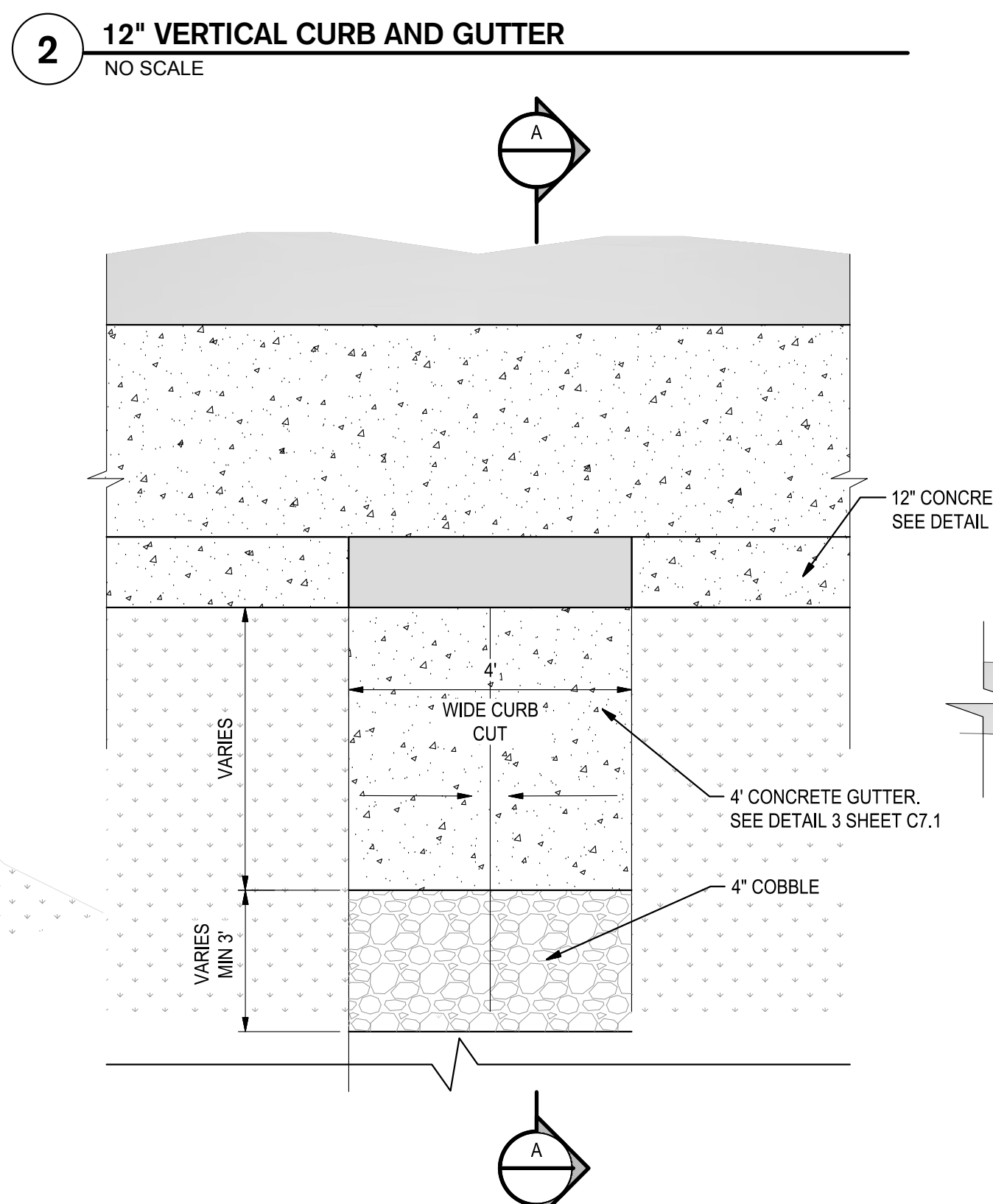
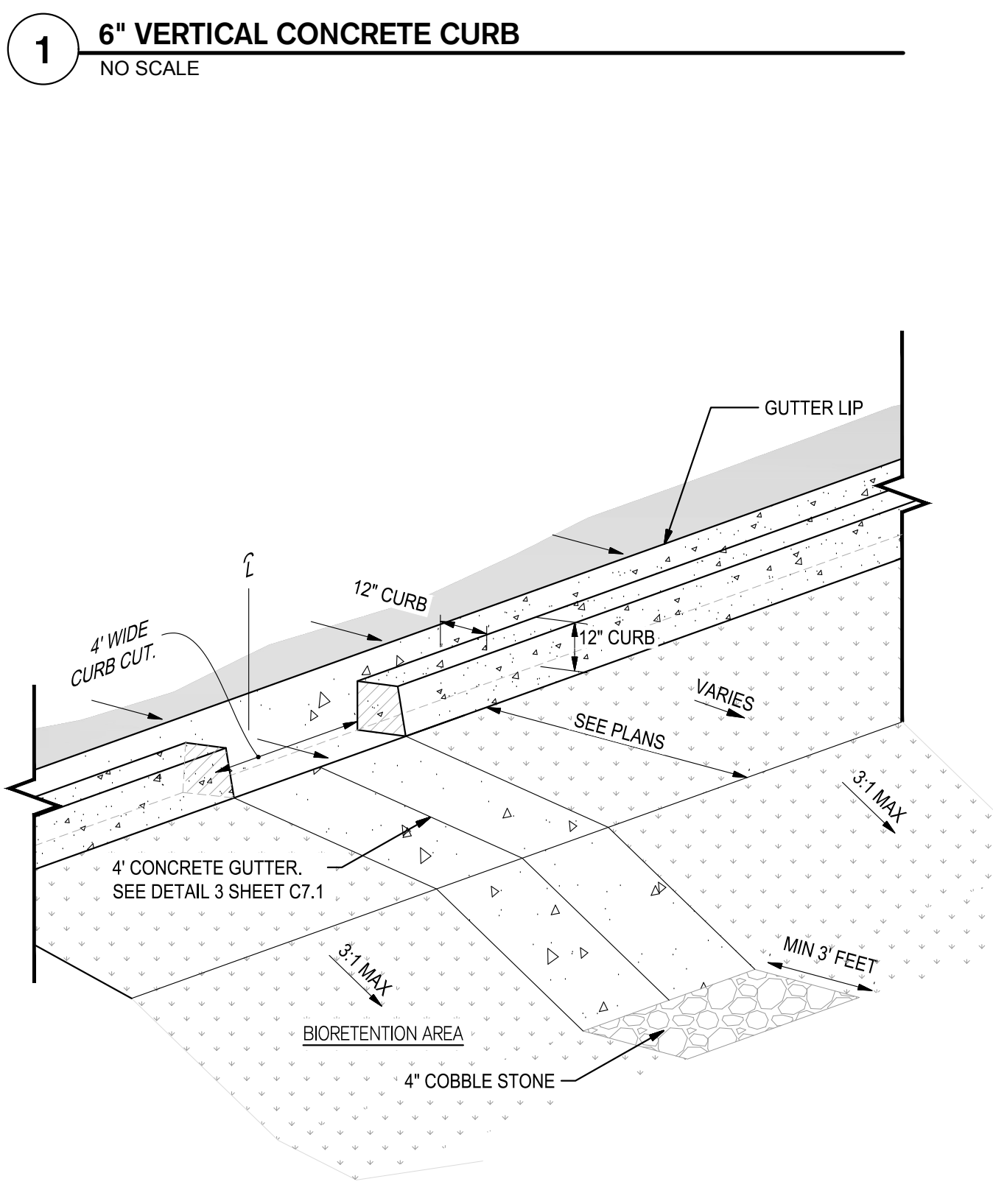
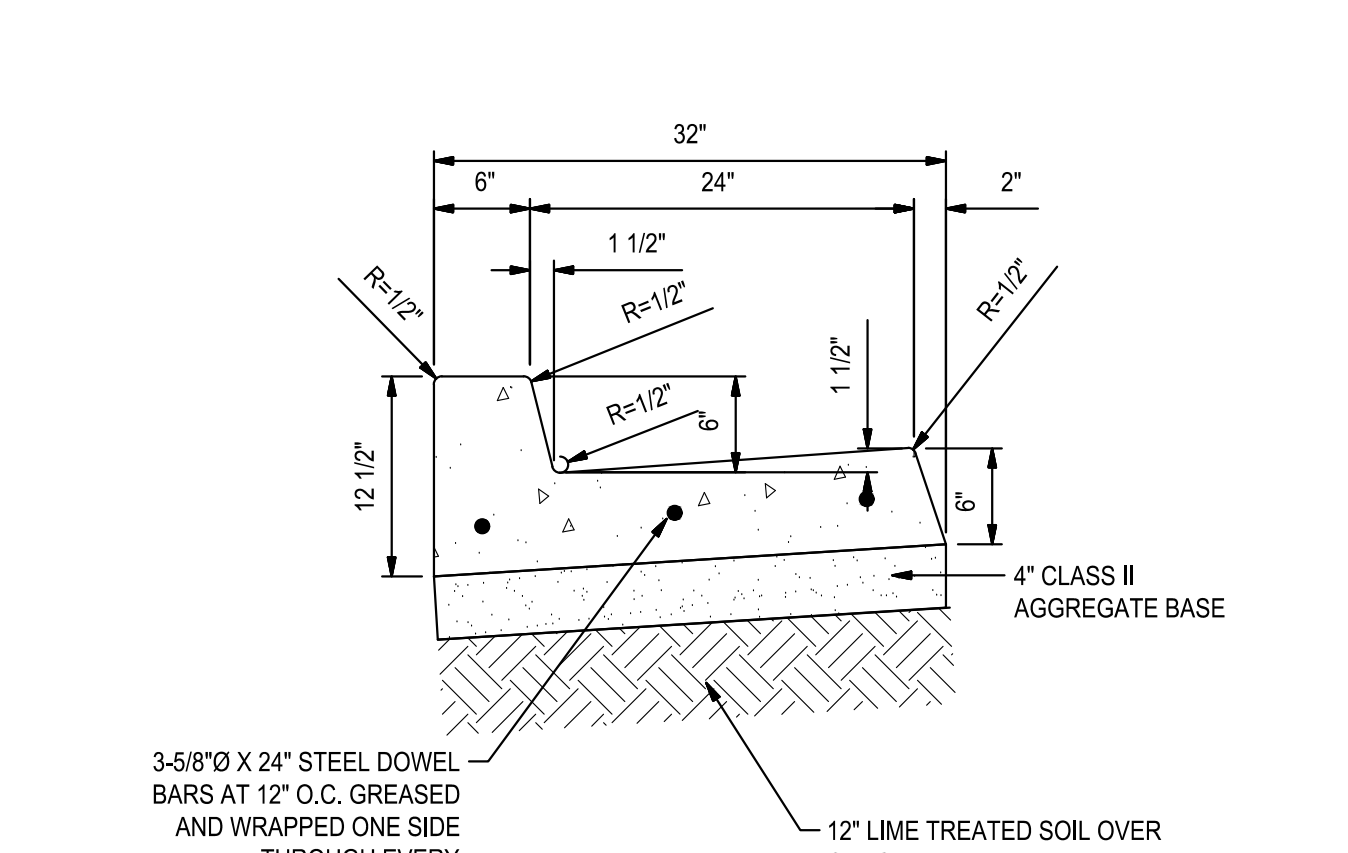
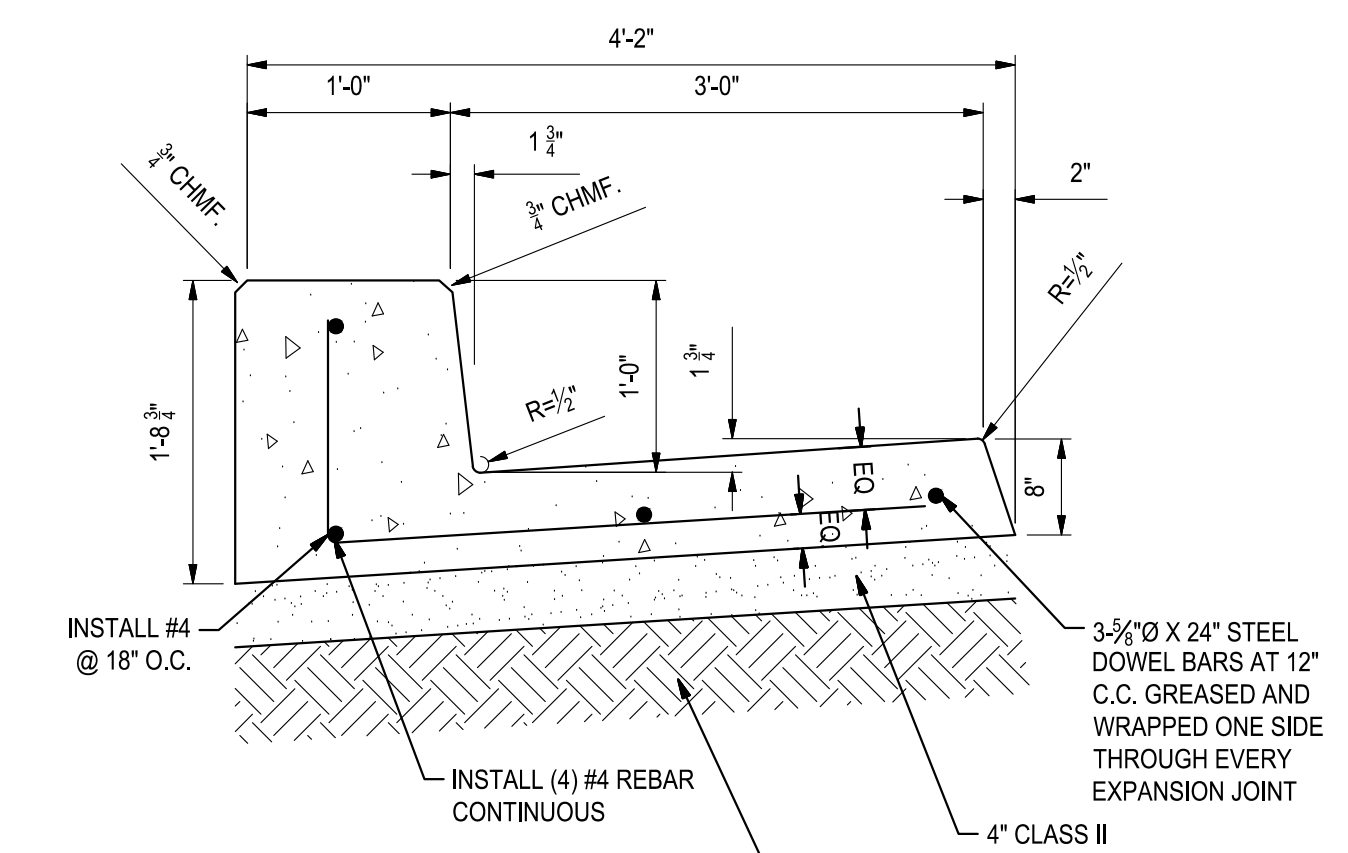
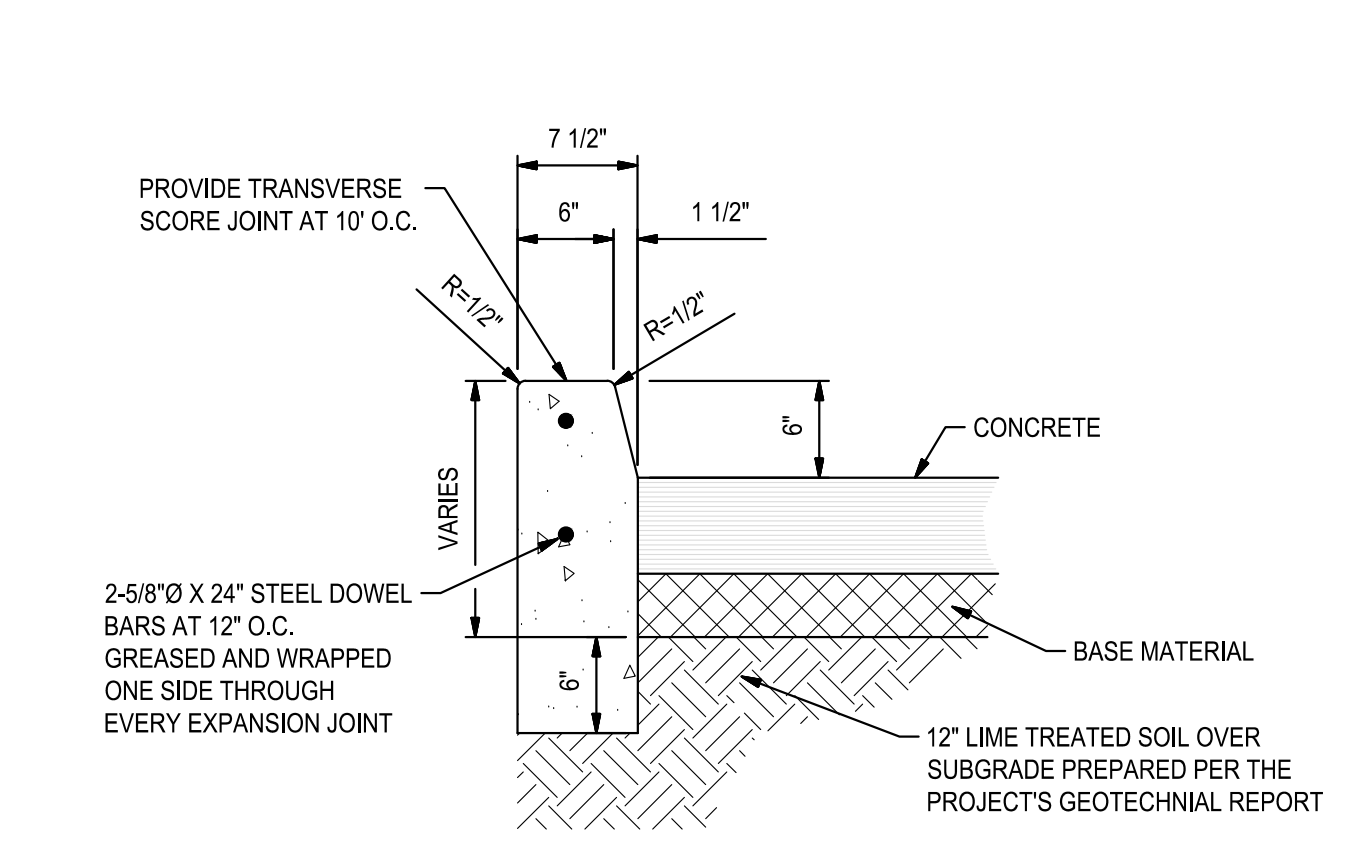
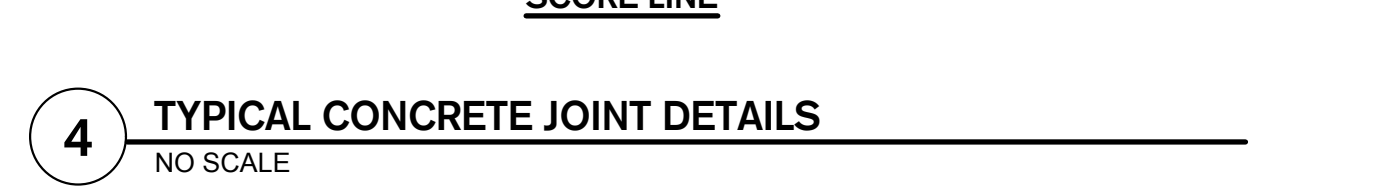
Proj Mgr AKM
Drawn by JR
Date 04/20/2021
Job No. 21081

SHEET: **C7.0**
OF: 18

DOWEL SIZE & SPACING			
SLAB DEPTH	DIAMETER	LENGTH	SPACING
4"	5/8"	12"	24" O.C.
6"	3/4"	18"	18" O.C.
8"	1"	24"	18" O.C.
10"	1"	24"	12" O.C.



WEAKENED PLANE JOINT		
SLAB THICKNESS	WPJ SPACING (EACH WAY)	CONTROL JOINT SPACING
4"	8' O.C. MAX.	32' O.C. MAX.
6"	12' O.C. MAX.	36' O.C. MAX.
8"	16' O.C. MAX.	40' O.C. MAX.
10"	N/A	40' O.C. MAX.



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SHEET TITLE

DETAILS II

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Drawn by JR

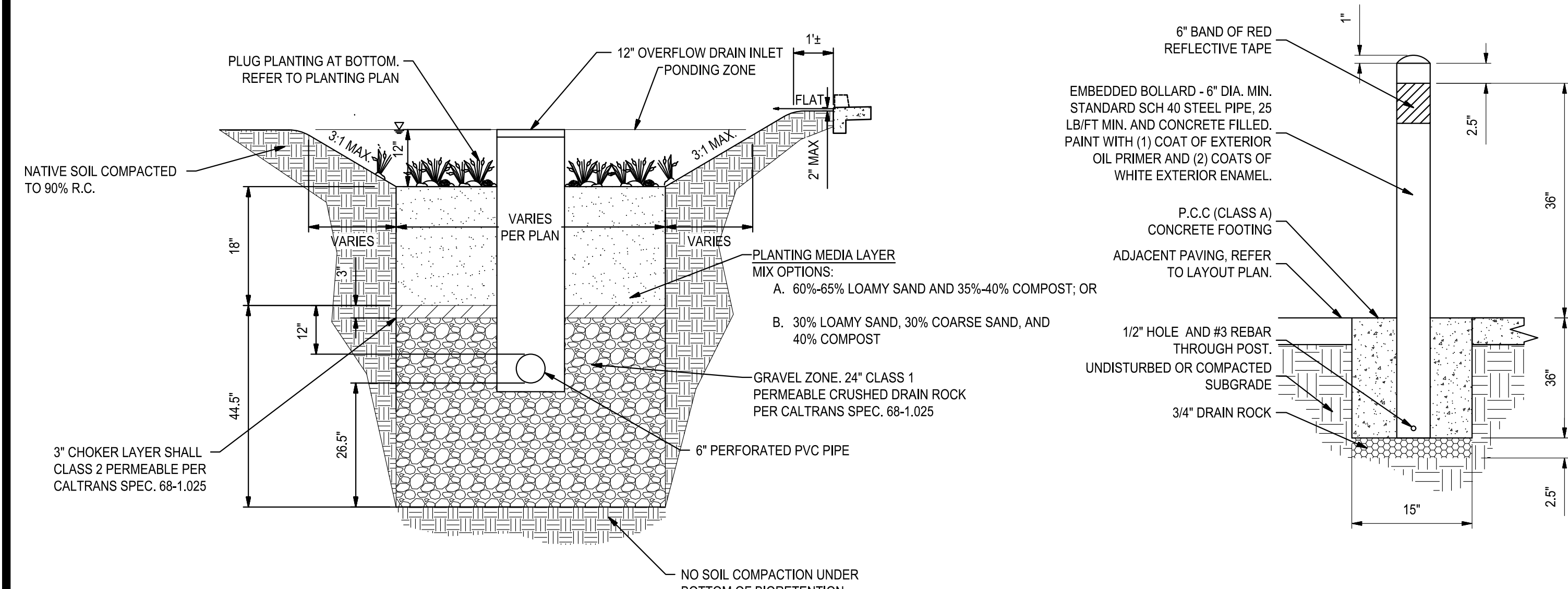
Date 04/20/2021

Job No. 21081

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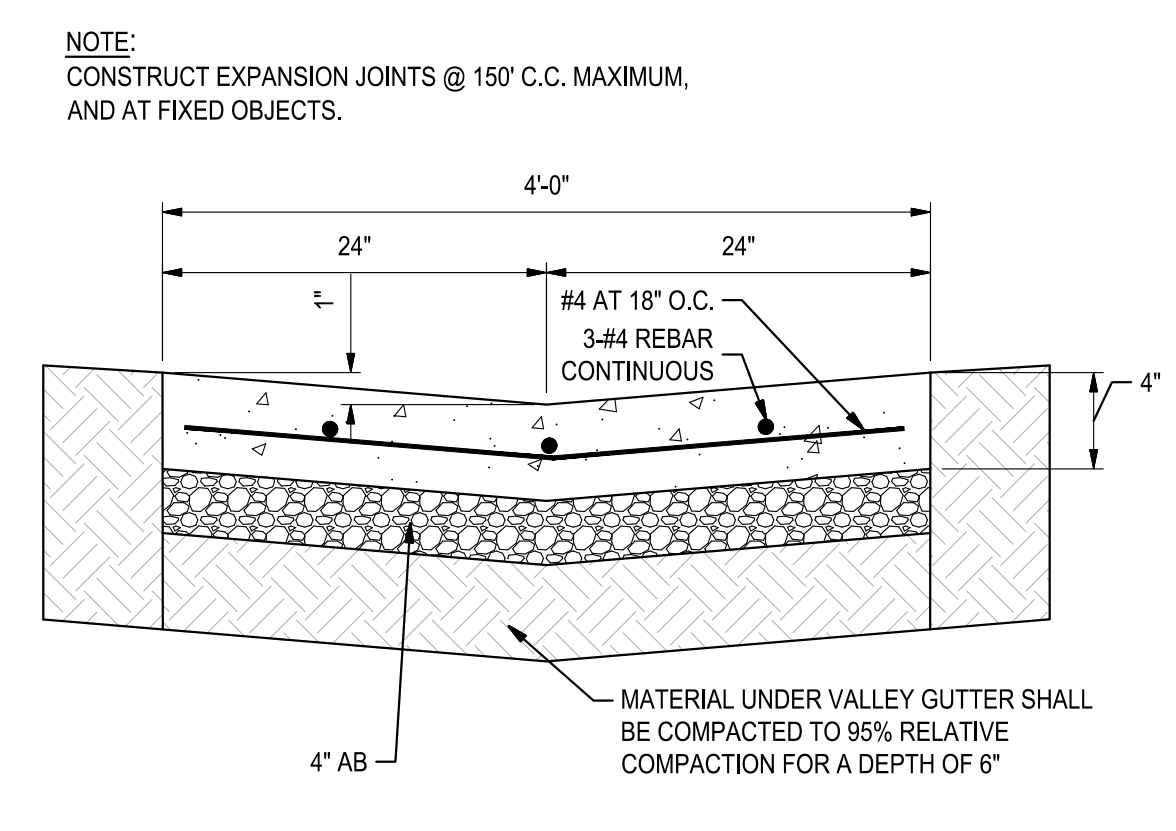
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OF 18

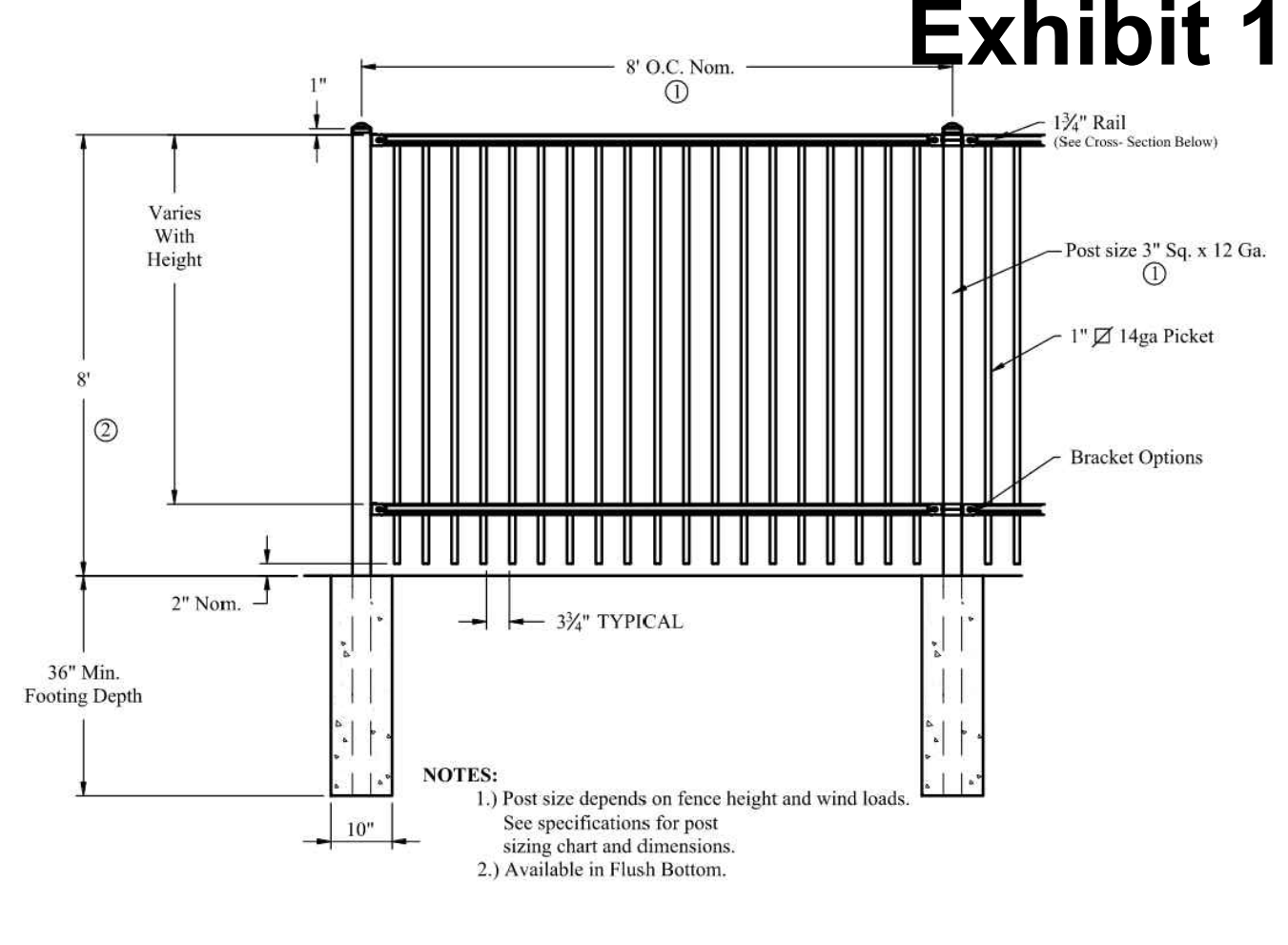


1 BIORETENTION DETAIL OPTION
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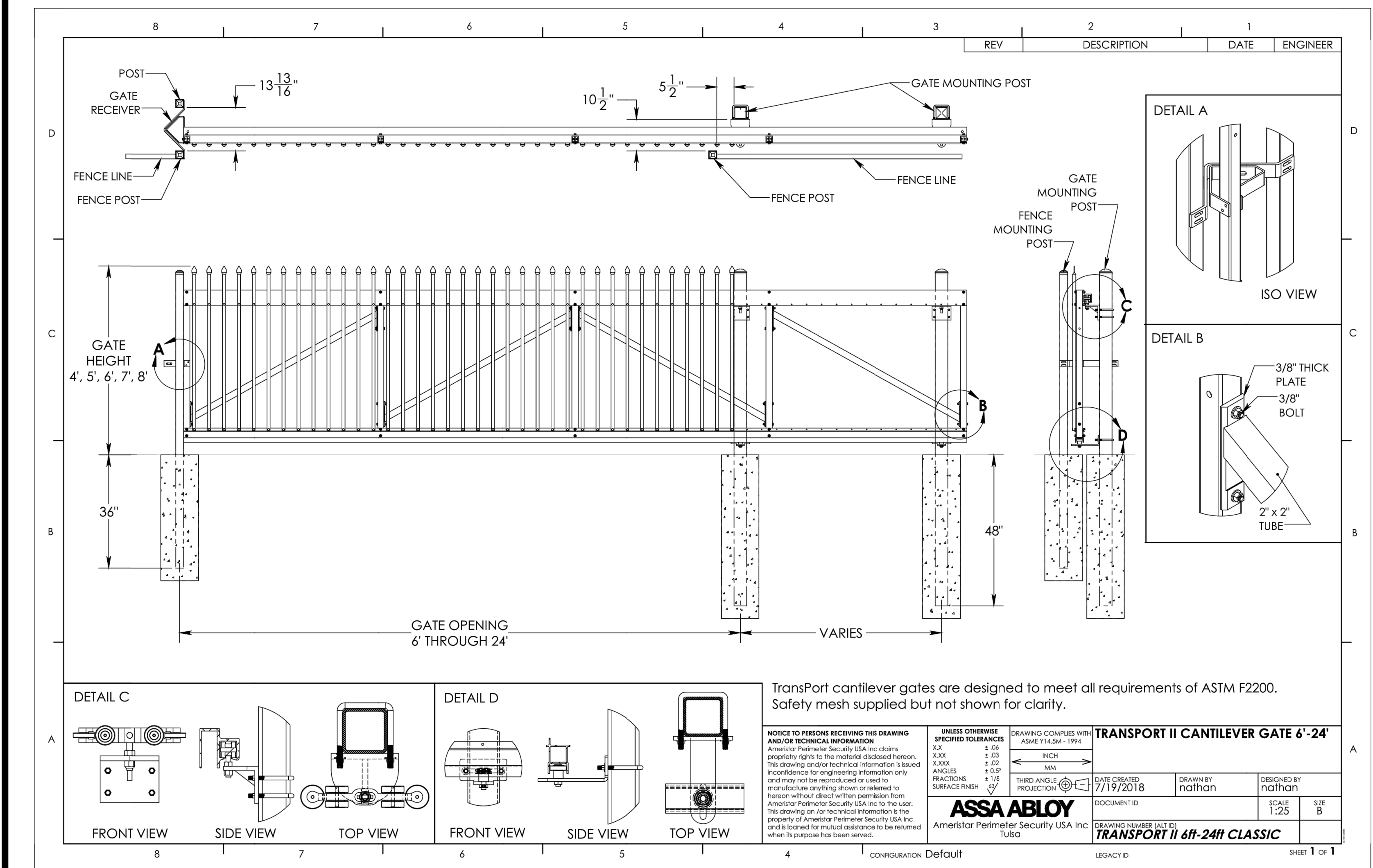
2 STEEL BOLLARD
NO SCALE



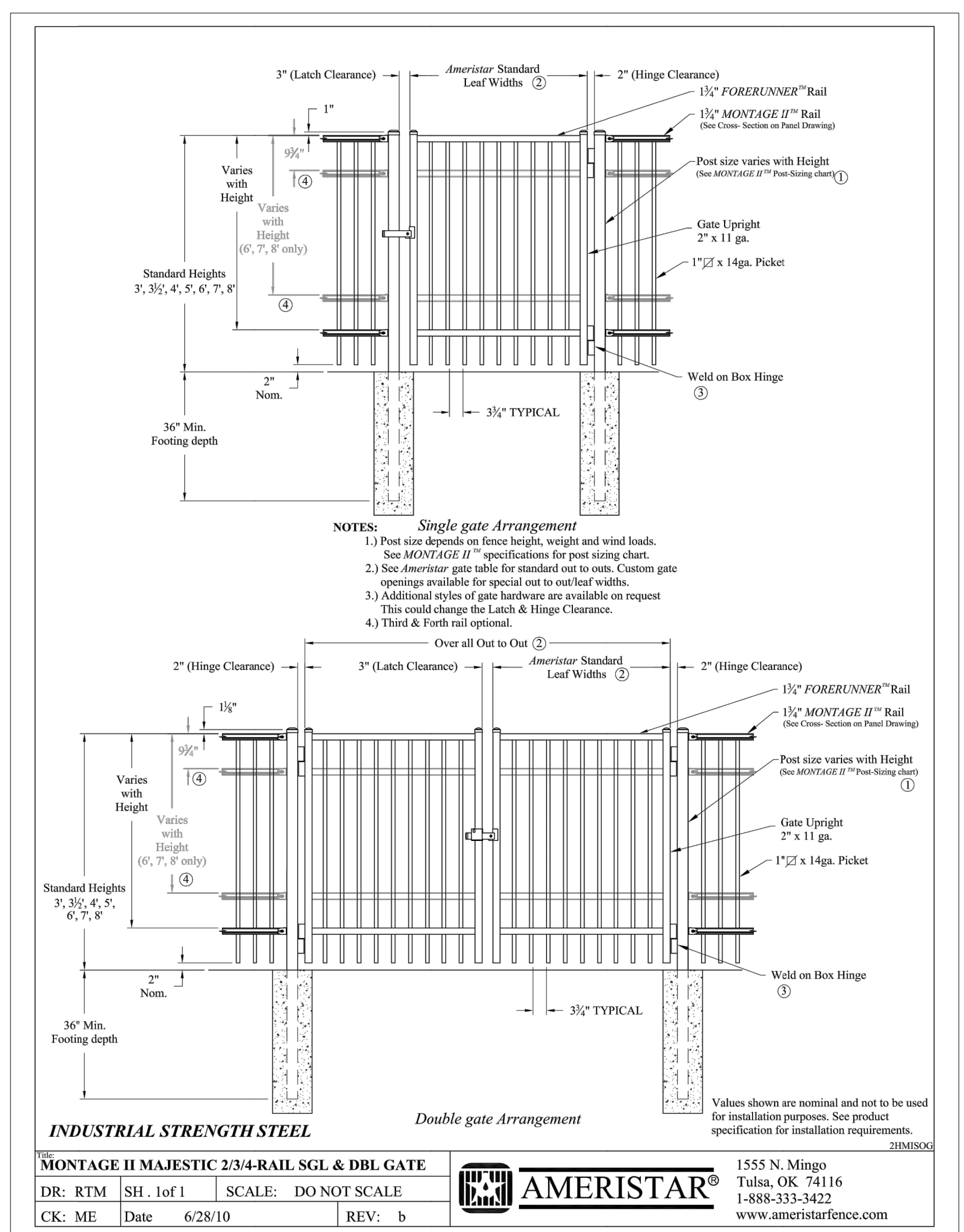
3 4' CONCRETE VALLEY GUTTER
NO SCALE



4 8' TALL AMERISTAR MONTAGE II FENCE WITH 2 RAILS
N.T.S.



5 ORNAMENTAL IRON ROLL GATE DETAIL
NO SCALE



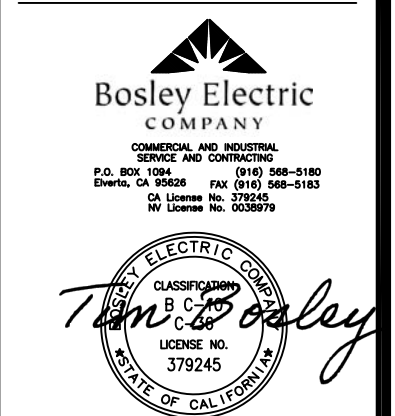
6 ORNAMENTAL IRON ROLL GATE DETAIL
NO SCALE

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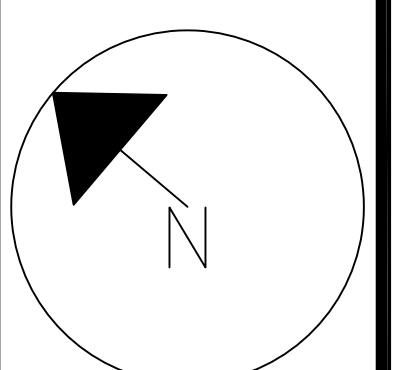
REVISIONS
Date Description

No.	Date	Description

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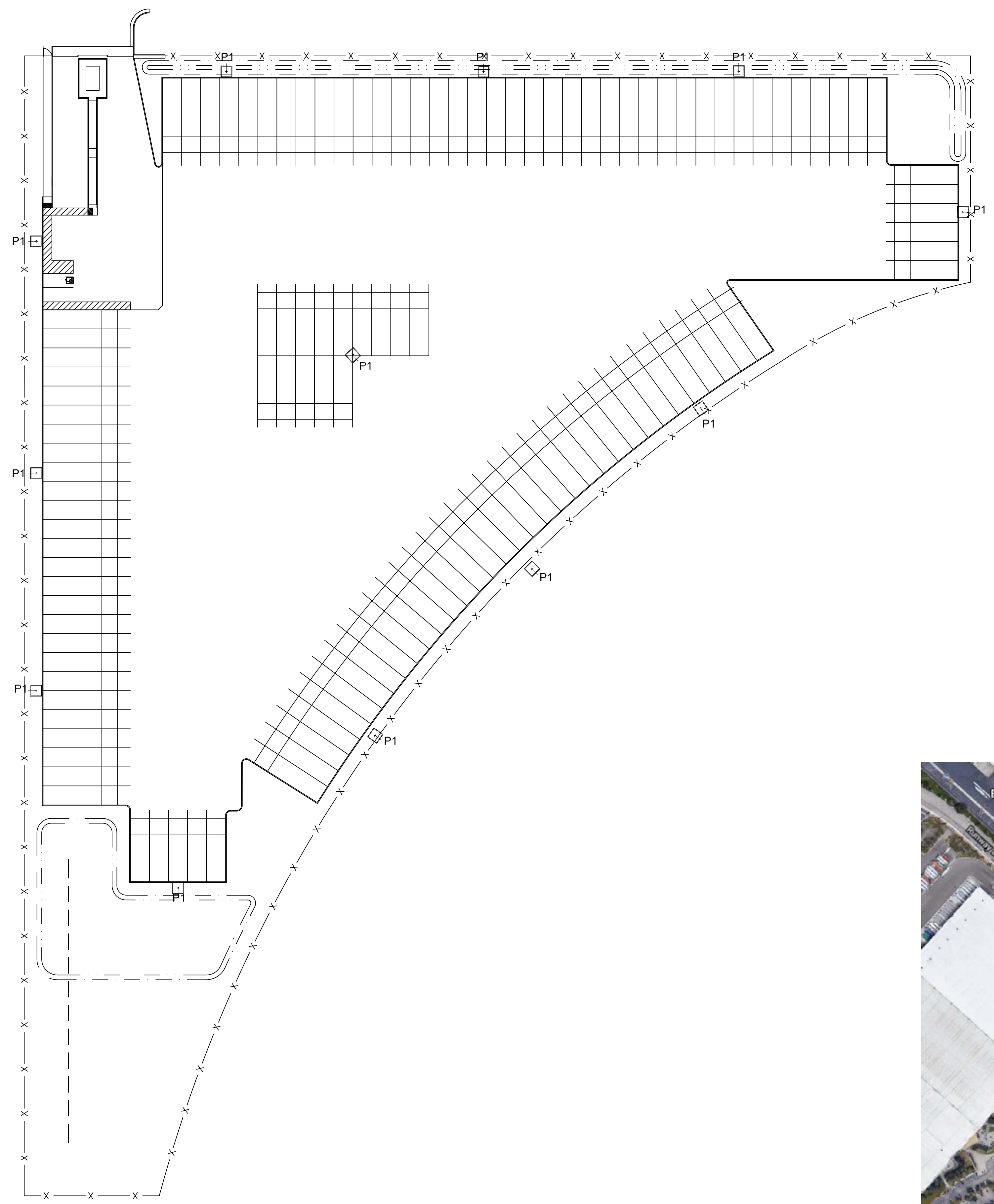
TRAILER STORAGE AVIATION DRIVE

AVIATION DRIVE STOCKTON, CA



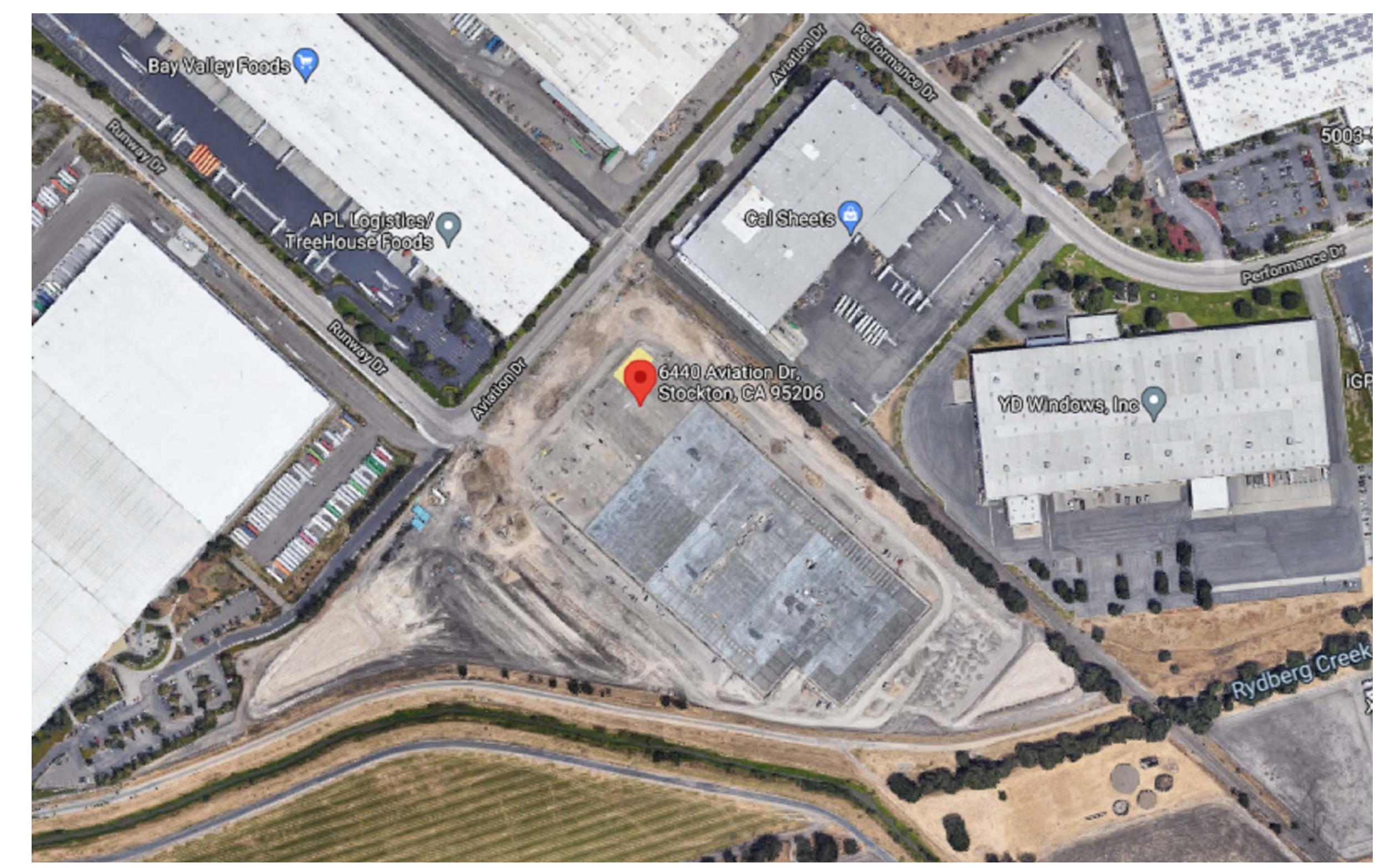
SHEET TITLE

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Drawn MV
By
Date 04/19/2021
Job 21081
SHEET: E-1



SITE PLAN- AVIATION TRAILER YARD

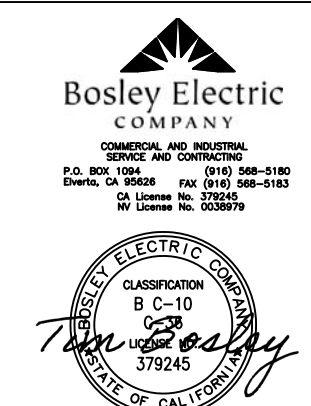
SCALE: 1"=40'-0"





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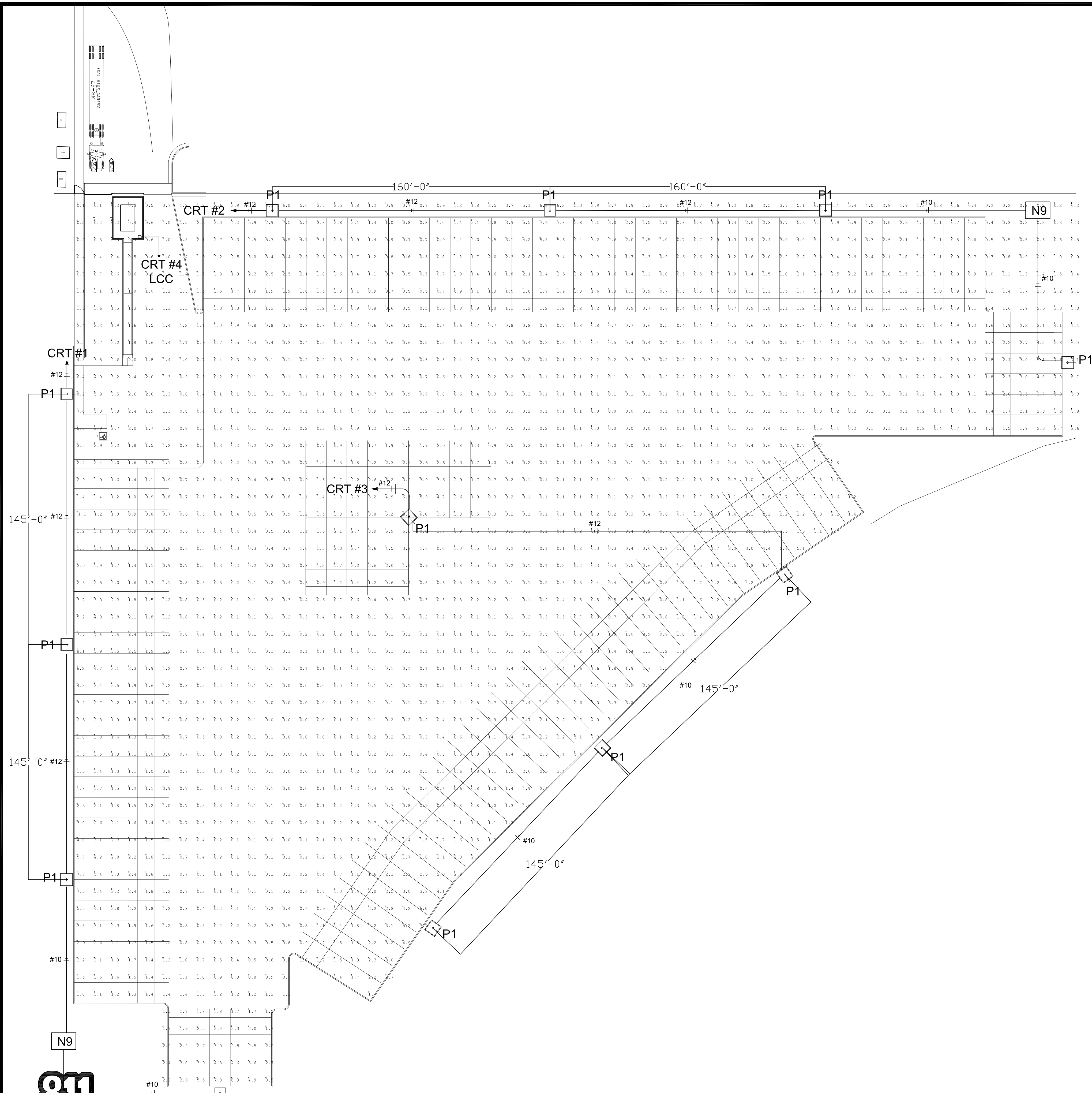
AVIATION DRIVE STOCKTON, CA

SHEET TITLE

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By
Date 04/19/2021


Job 21081

SHEET E-2



SITE LIGHTING- AVIATION TRAILER YARD

SCALE: 1"=30'-0"



RSX2 LED Area Luminaire

Category: RSX2-LED-P5-60K-R5-MVOLT-SPA-DBBXD

Series: P1

Specifications

EPA (ft²@0°): 0.69 ft² (0.06 m²)

Length: 29.3" (74.4 cm) (SPA mount)

Width: 13.4" (34.0 cm)

Height: 3.0" (7.6 cm) Main Body
7.2" (18.3 cm) Arm

Weight: 30.0 lbs (13.6 kg) (SPA mount)

Introduction

The new RSX LED Area family delivers maximum value by providing significant energy savings, long life and outstanding photometric performance at an affordable price. The RSX2 delivers 11,000 to 31,000 lumens allowing it to replace 250W to 1000W HID luminaires.

The RSX features an integral universal mounting mechanism that allows the luminaire to be mounted on most existing drill hole patterns. This "no-drill" solution provides significant labor savings. An easy-access door on the bottom of mounting arm allows for wiring without opening the electrical compartment. A mast arm adaptor, adjustable integral slipfitter and other mounting configurations are available.

Ordering Information **EXAMPLE: RSX2 LED P6 40K R3 MVOLT SPA DBBXD**

Series	Performance Package	Color Temperature	Distribution	Voltage	Mounting
RSX2 LED	P1	300K 3000K	R2	120V (277V) ¹	SPA
	P2	40K 4000K	R3	MVOLT (247V-480V) ¹	RPA
	P3	50K 5000K	R5	low specific voltage (see options as needed)	RPA
	P4		R4	120V 277V	IS
	P5		R5	347V	WBA
	P6		R5	347V 480V	WBASC

Options

Shipped Installed	Shipped Separately (requires some field assembly)	Finish
HS House size shield ¹	ST Standalone and Network Sensors/Controls (factory default settings, see table page 9)	DBBD Dark Bronze
PE Patented external lens style ¹²	NIZAR2 Light XIR generator 2 ^{13,14,15}	BLK Black
PEX Patented external lens style ¹²	PRH Network, 0-10V motor/ambient sensor (for use with NEMR2) ^{16,17}	NA Natural Aluminum
PSD Strip-wire basic photocell sensor only (no controls) ^{18,19,20}	*Note: PRH with Light Air can be used as a standalone dimming sensor with out-of-box settings or as a wireless networked solution. See factory default settings table. Sensor coverage pattern is affected when luminaire is tilted.	WH White
CE3A Cordul array (20, 277, 347) ¹		DBLX Dark Bronze
SP Single face (20, 277, 347) ¹		DBLX Textured Black
DF Double face (20, 277, 347) ¹		DBLX Textured Natural Aluminum
SPDQV 201V Single pack (20, 277, 347) ¹		DWGD Textured White
FRD FRD adjustable-wire ²¹		
DMG 0-10V dimming control (back of housing for external control) (control ordered separately) ²²		
DS Dual switching ²³		

PHOTOMETRIC SUMMARY

CALC TYPE	UNITS	AVG	MAX	MIN
ILLUMINANCE	FC	0.94	8.30	0.0

LITHONIA LIGHTING One Lithonia Way • Conyers, Georgia 30213 • Phone: 800.706.7376 • www.lithonia.com Lithonia R50 Area LED Rev. 08/24/20 Page 1 of 9





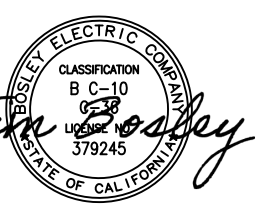
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ELECTRICAL CONTRACTORS
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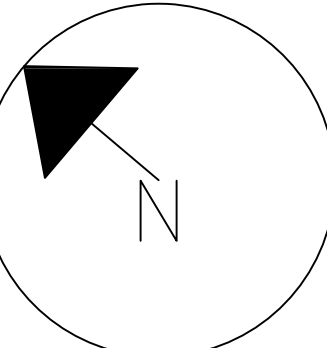
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AVIATION DRIVE STOCKTON, CA



SHEET TITLE

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Drawn MV

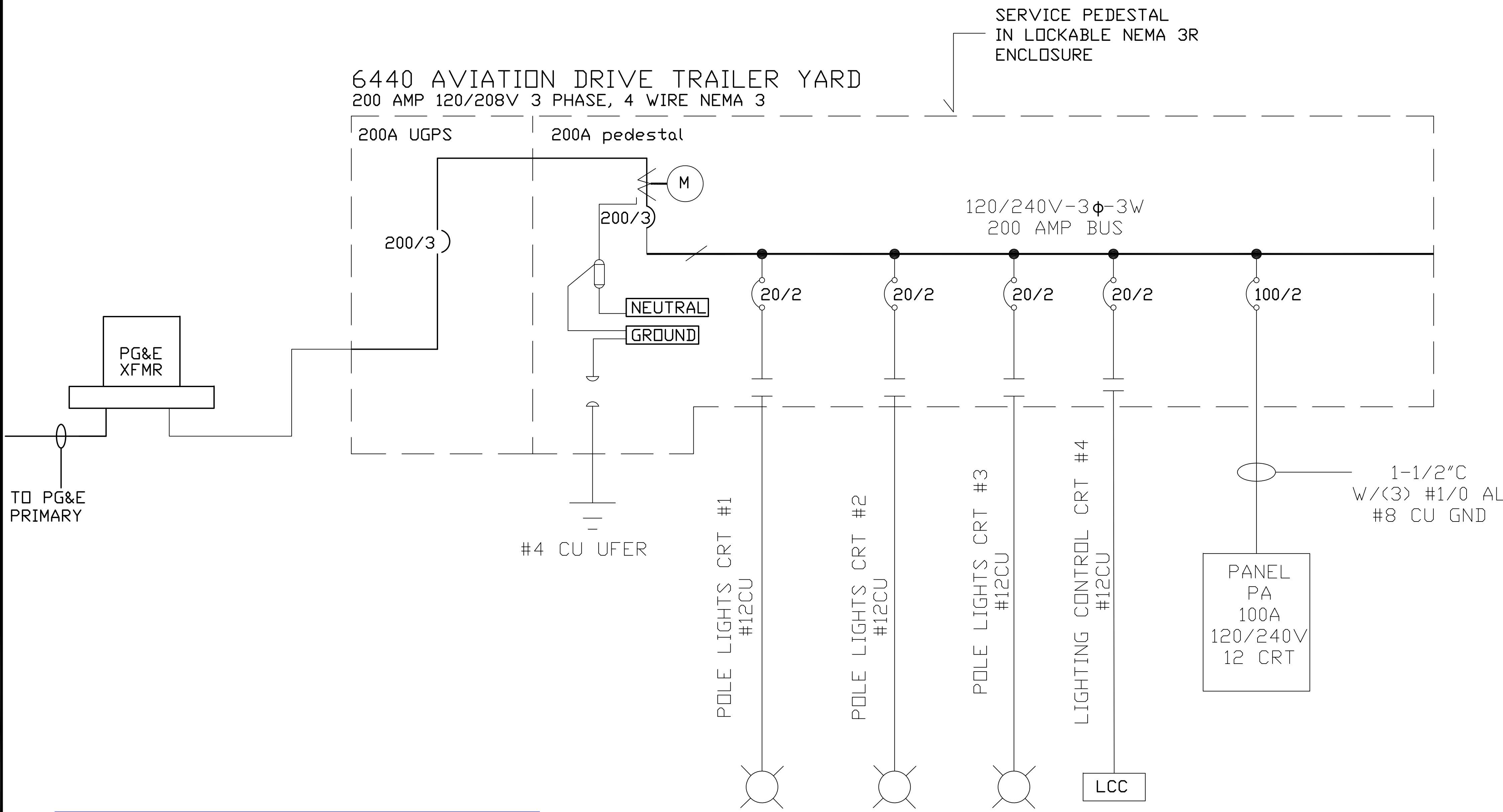
By Date 04/19/2021

Job 21081

NO. OF SHEETS: E-3

3 OF 4

6440 AVIATION DRIVE TRAILER YARD
200 AMP 120/208V 3 PHASE, 4 WIRE NEMA 3



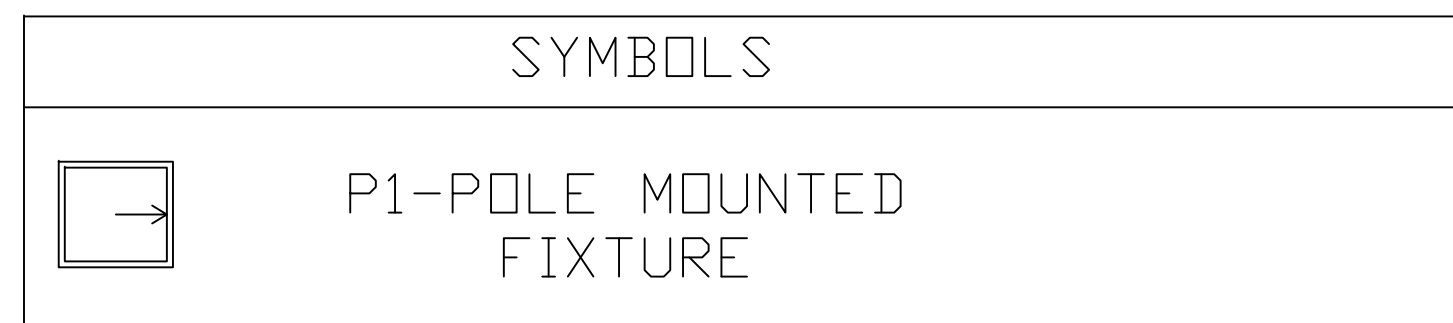
(N)PANEL: PA 100 A. 120/240 V. 1 Phase 3 Wire
TYPE: NEMA 1 X MLO
TRIM: SURFACE MAIN BKR.

LOAD	KVA	CB	NO	S	N	NO	CB	KVA	LOAD
HVAC	3.27	25	1			2	20/1	0.53	LGT/EXT LGT/EXFAN
			2			4	20/1	0.50	
WH	5.86	20	5			6	20/1	4.00	
			7			8			
			9			10			
			11			12			

CONNECTED LOAD: 14.2 KVA
25% LIGHTING: KVA
25% LARGEST MTR.: KVA
TOTAL: 14.16 KVA / .360 = 39.34 AMPS

LIGHT FIXTURE SCHEDULE

TYPE	DESCRIPTION	VOLTS	LAMPS	INPUT WATTS	MOUNTING	REMARKS
P1	RSX2LED-P5-S0K-R5-MVOLT-SPA-DOBXD	M-VOLT	LED	210	POLE	BUG RATING= B5-UO-G4



ONE-LINE/PANEL SCHEDULES- AVIATION TRAILER YARD

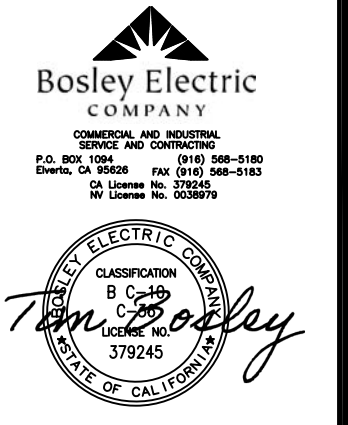
SCALE: NO SCALE

Exhibit 1



3428 Brookside Road
Stockton, California 95219
209-943-2021
Fax: 209-942-0214
www.siegfriedeng.com

CIVIL
 STRUCTURAL
 LANDSCAPE ARCHITECTURE
 SURVEYING
 PLANNING
 ATHLETIC FACILITY DESIGN



THESE PLANS HAVE BEEN PREPARED BY ENGINEER BOISLEY ELECTRIC COMPANY, INC. UNDER CONTRACT TO THE CLIENT. THESE PLANS DO NOT REPRESENT THE ENGINEER'S LIABILITY FOR THE DESIGN OF THE PROJECT OR THE QUALITY OF THE WORKMANSHIP OF THE CONTRACTOR.

STATE OF CALIFORNIA
Outdoor Lighting
NRC-02-4

CALIFORNIA ENERGY COMMISSION
NRC-02-4
Page 1 of 8

Project Name: Aviation Trailer Yard
Project Address: 640 Aviation Dr
Date Prepared: 11/16/2021

A. GENERAL INFORMATION

01 Project Location (City)	Stockton	04 Total Illuminated Landscape Area (ft ²)	18438
02 Climate Zone	31	05	

B. PROJECT SCOPE

C. OUTDOOR LIGHTING FUTURE BEHAVIOR

01 New Lighting System	Must Comply with Allowance from 146.2	02	
03 Altered Lighting System	Is your alteration increasing the connected lighting load (Watts)?	Yes	No

Registration Number: CA Building Energy Efficiency Standards - 2019 Nonresidential Compliance
Report Version: 2018.1.008
Report Date/Version: 2021-04-13 14:43:01

STATE OF CALIFORNIA
Outdoor Lighting
NRC-02-4

CALIFORNIA ENERGY COMMISSION
NRC-02-4
Page 1 of 8

Project Name: Aviation Trailer Yard
Project Address: 640 Aviation Dr
Date Prepared: 11/16/2021

C. COMPLIANCE RESULTS

01	02	03	04	05	06	07	08	09
General Allowance (146.2) (Watts)	Per Application (146.2)(2) (See Table J)	Sales Franchise (146.2)(2) (See Table J)	Ornamental (146.2)(2) (See Table J)	Per Specific Area (146.2)(2) (See Table M)	Existing Power Allowance (146.2)(2) (See Table N)	Total Allowed (Watts)	Total Actual (Watts)	07 must be >= 08
9,944.8				1,490.8	2,406.4	7,854.4	9,540	COMPLIES

Registration Number: CA Building Energy Efficiency Standards - 2019 Nonresidential Compliance
Report Version: 2018.1.008
Report Date/Version: 2021-04-13 14:43:01

STATE OF CALIFORNIA
Outdoor Lighting
NRC-02-4

CALIFORNIA ENERGY COMMISSION
NRC-02-4
Page 1 of 8

Project Name: Aviation Trailer Yard
Project Address: 640 Aviation Dr
Date Prepared: 11/16/2021

F. OUTDOOR LIGHTING FEATURE SCHEDULE

01	02	03	04	05	06	07	08	09	10
Name or Item Tag	Complete Luminaire Description	Watts per luminaire ¹	How is Wattage Determined?	Total number of luminaires?	Luminaire Status ²	Excluded watt (146.2)(2)	Design Watts	Count Item - 5,000 initial lumens output (146.2)(2) 1	Field Inspector
P1	single head pole fixture	Linear	M/L Spec	12	New		2,130	N/A	Pass

Registration Number: CA Building Energy Efficiency Standards - 2019 Nonresidential Compliance
Report Version: 2018.1.008
Report Date/Version: 2021-04-13 14:43:01

STATE OF CALIFORNIA
Outdoor Lighting
NRC-02-4

CALIFORNIA ENERGY COMMISSION
NRC-02-4
Page 1 of 8

Project Name: Aviation Trailer Yard
Project Address: 640 Aviation Dr
Date Prepared: 11/16/2021

H. OUTDOOR LIGHTING CONTROLS

01	02	03	04	05
Area Description	Shut-Off (146.2)(2)	Auto-Schedule (146.2)(2)	Motion Sensor (146.2)(2)	Field Inspector
Trailer Yard P1				Pass

Registration Number: CA Building Energy Efficiency Standards - 2019 Nonresidential Compliance
Report Version: 2018.1.008
Report Date/Version: 2021-04-13 14:43:01

STATE OF CALIFORNIA
Outdoor Lighting
NRC-02-4

CALIFORNIA ENERGY COMMISSION
NRC-02-4
Page 2 of 8

Project Name: Aviation Trailer Yard
Project Address: 640 Aviation Dr
Date Prepared: 11/16/2021

I. LIGHTING POWER ALLOWANCE (per 146.2)

L. LIGHTING ALLOWANCE: PER APPLICATION

01	02	03	04	05	06	07	08	09	10
Area Description	Surface Type	Area (ft ²)	Permitted Area (ft ²)	Area Allowance Density (W/ft ²)	Area Allowance (Watts)	Area Allowance Length (ft)	Area Allowance Density (W/ft ²)	Linear Allowance (Watts)	Total General Allowance (Watts)
Trailer Yard	Asphalt	18438	18438	0.22	4056.4	512	0.2	131.2	4898.8

Registration Number: CA Building Energy Efficiency Standards - 2019 Nonresidential Compliance
Report Version: 2018.1.008
Report Date/Version: 2021-04-13 14:43:01

STATE OF CALIFORNIA
Outdoor Lighting
NRC-02-4

CALIFORNIA ENERGY COMMISSION
NRC-02-4
Page 3 of 8

Project Name: Aviation Trailer Yard
Project Address: 640 Aviation Dr
Date Prepared: 11/16/2021

M. LIGHTING ALLOWANCE: PER SPECIFIC AREA

01	02	03	04	05	06	07	08	09	10
Area Description	Specific Area Type (Table I)	Specific Area (ft ²)	Permitted Area (ft ²)	Area Allowance Density (W/ft ²)	Area Allowance (Watts)	Luminaire Name or Item Tag	Watts per Luminaire	# of Luminaires	Design Watts
Trailer yard	Guardrail	7684	7684	0.3	2496.8	P1	310	11	2,130

Registration Number: CA Building Energy Efficiency Standards - 2019 Nonresidential Compliance
Report Version: 2018.1.008
Report Date/Version: 2021-04-13 14:43:01

STATE OF CALIFORNIA
Outdoor Lighting
NRC-02-4

CALIFORNIA ENERGY COMMISSION
NRC-02-4
Page 4 of 8

Project Name: Aviation Trailer Yard
Project Address: 640 Aviation Dr
Date Prepared: 11/16/2021

P. DECLARATION OF REQUIRED CERTIFICATES OF ACCEPTANCE

Yes	No	Form/Title	Field Inspector
<input checked="" type="checkbox"/>	<input type="checkbox"/>	NRC-420-02-4 - Must be submitted for all outdoor lighting control except for alterations where controls are added to <= 50 luminaires.	Pass

Registration Number: CA Building Energy Efficiency Standards - 2019 Nonresidential Compliance
Report Version: 2018.1.008
Report Date/Version: 2021-04-13 14:43:01

STATE OF CALIFORNIA
Outdoor Lighting
NRC-02-4

CALIFORNIA ENERGY COMMISSION
NRC-02-4
Page 5 of 8

Project Name: Aviation Trailer Yard
Project Address: 640 Aviation Dr
Date Prepared: 11/16/2021

Q. DOCUMENTATION AUTHOR'S DECLARATION STATEMENT

R. RESPONSIBLE PERSON'S DECLARATION STATEMENT

Registration Number: CA Building Energy Efficiency Standards - 2019 Nonresidential Compliance
Report Version: 2018.1.008
Report Date/Version: 2021-04-13 14:43:01

REVISIONS
No. Date Description

PROJECT

TRAILER STORAGE AVIATION DRIVE
AVIATION DRIVE STOCKTON, CA

SHEET TITLE

Proj Mgr TB
Drawn MV
By Date 04/19/2021
Job 21081
NO SHEET: E-4
4 OF 4



TITLE 24 OUTDOOR LIGHTING- AVIATION TRAILER YARD

SCALE: NO SCALE



* 1 3 8 7 *

TM3 - 97

MOORE BIOLOGICAL CONSULTANTS

Fax Transmittal

TO MIKE NIBLOCK - CITY OF STOCKTON (FAX 937 8893)
FROM DIANE MOORE
PAGES 1
SUBJECT DOLLAR TREE PROJECT SWAINSON S HAWK SURVEYS
DATE 3/19/01
HARD COPY WILL OR WILL NOT FOLLOW BY MAIL

BP 01-406
BP 01-1332

1 3 1 3 1 1 1 1

MAR 19

Mike

This memo is intended to update you on the Swainson s hawk surveys we have been retained to conduct this year at the Dollar Tree Warehouse site We conducted an initial nesting Swainson s hawk survey this morning with negative results

We have also been retained to conduct follow up surveys every two weeks starting today through mid May as nesting has not commenced for this year (the hawks have just began showing up from Mexico in the last 2 weeks) By mid-May any hawks nesting this year in the greater project vicinity should be on nesting We will inform you immediately if we have positive survey results so that we can work together to figure out what if anything needs to be done If the surveys continue to be negative we will forward a copy of our survey report once the surveys are completed

I hope this update helps for now Please call me at (209) 365 6828 to discuss if necessary

Thank you

Diane Moore

RL/JL AN

MOORE BIOLOGICAL CONSULTANTS

June 27 2001

Aug - 3

Mr David Haggerty
Senior Vice President
CB Richard Ellis Industrial Consulting Team
1776 West March Lane Ste 170
Stockton CA 95207

* ORIGINAL TO FOLLOW IN MAIL *

Post It [®] Fax Note	7671	Date	8/1/01	# of pages	▶ 3
To	Jerry Liao	From	Diane Moore		
Co./Dept	City of Stockton	Co	MBC		
Phone #	937-8316	Phone	(209) 365-6828		
Fax #	- 8893	Fax #	- 6829		

Subject SWAINSON'S HAWK PRE CONSTRUCTION SURVEYS FOR PARCEL 6 AIRPORT
GATEWAY STOCKTON CALIFORNIA

Dear David

Thank you for contracting with Moore Biological Consultants for the subject work. This survey was conducted in order to fulfill the City of Stockton pre-construction survey requirement for nesting Swainson's hawk (*Buteo swainsoni*) in the vicinity the subject project. This letter provides a summary of the survey methods and results.

Methods

Early morning surveys were conducted on June 19 and 22, 2001. The surveys included searching the immediate project site as well as a 0.25-mile buffer area around the project site for nest trees and presence of Swainson's hawks (Figure 1). The surveys were conducted on foot and by vehicle using binoculars and a spotting scope to detect potential nest trees and nesting Swainson's hawks.

Results

No Swainson's hawk nests were observed in the immediate project vicinity or within 0.25 miles of the project site. No Swainson's hawks were observed flying or perching in trees in the project vicinity.

Conclusions

- Since there are no active Swainson's hawk nests located within 0.25 miles of the project site, it is unlikely the construction would result in impacts to this species.
- If construction is delayed until next Spring, a follow-up pre-construction survey for Swainson's hawk would be necessary.

Thank you for contracting with Moore Biological Consultants to perform this survey. Please let me know if we can provide further assistance on this or any other projects. Also, please call me at (209) 365-6828 with any questions.

Sincerely,

A handwritten signature in black ink, appearing to read "Diane S. Moore". The signature is fluid and cursive, with a large initial "D" and "M".

Diane S. Moore, M.S.
Principal Biologist

AIRPORT GATEWAY CENTER



Scale Not To Scale
Source Panattoni



N

MOORE BIOLOGICAL

**FIGURE 1
PROJECT LOCATION**

MOORE BIOLOGICAL CONSULTANTS

1 F () E 11 W 12

June 27 2001

AUG 1 2001

Mr. David Haggerty
Senior Vice President

CB Richard Ellis Industrial Consulting Team
1776 West March Lane Ste 170
Stockton CA 95207

Subject SWAINSON'S HAWK PRE CONSTRUCTION SURVEYS FOR PARCEL 6, AIRPORT
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* ORIGINAL TO FLOW IN MAIL *

Post It? Fax Note	7671	Date	8/1/01	# of pages	3
To	Jenny Lane	From	Diana Moore		
Cu/Dept	City of Stockton	C	MBC		
Phone #	937-8316	Phone	(209) 365-6828		
Fax #	-8893	Fax #	-6829		

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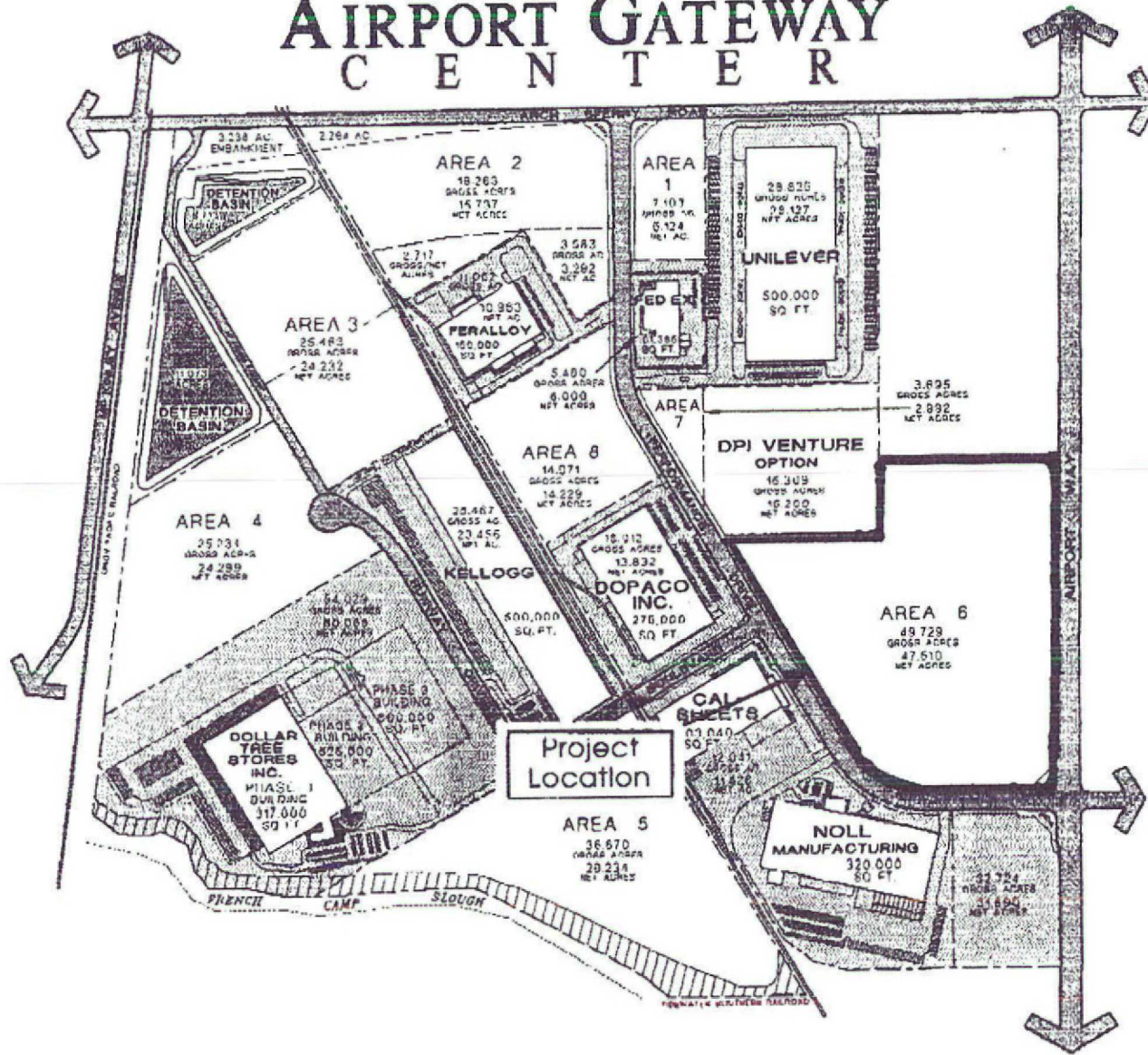
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Sincerely,



Diane S. Moore, M.S.
Principal Biologist

AIRPORT GATEWAY CENTER



Scale: Not To Scale
Source: Panallont

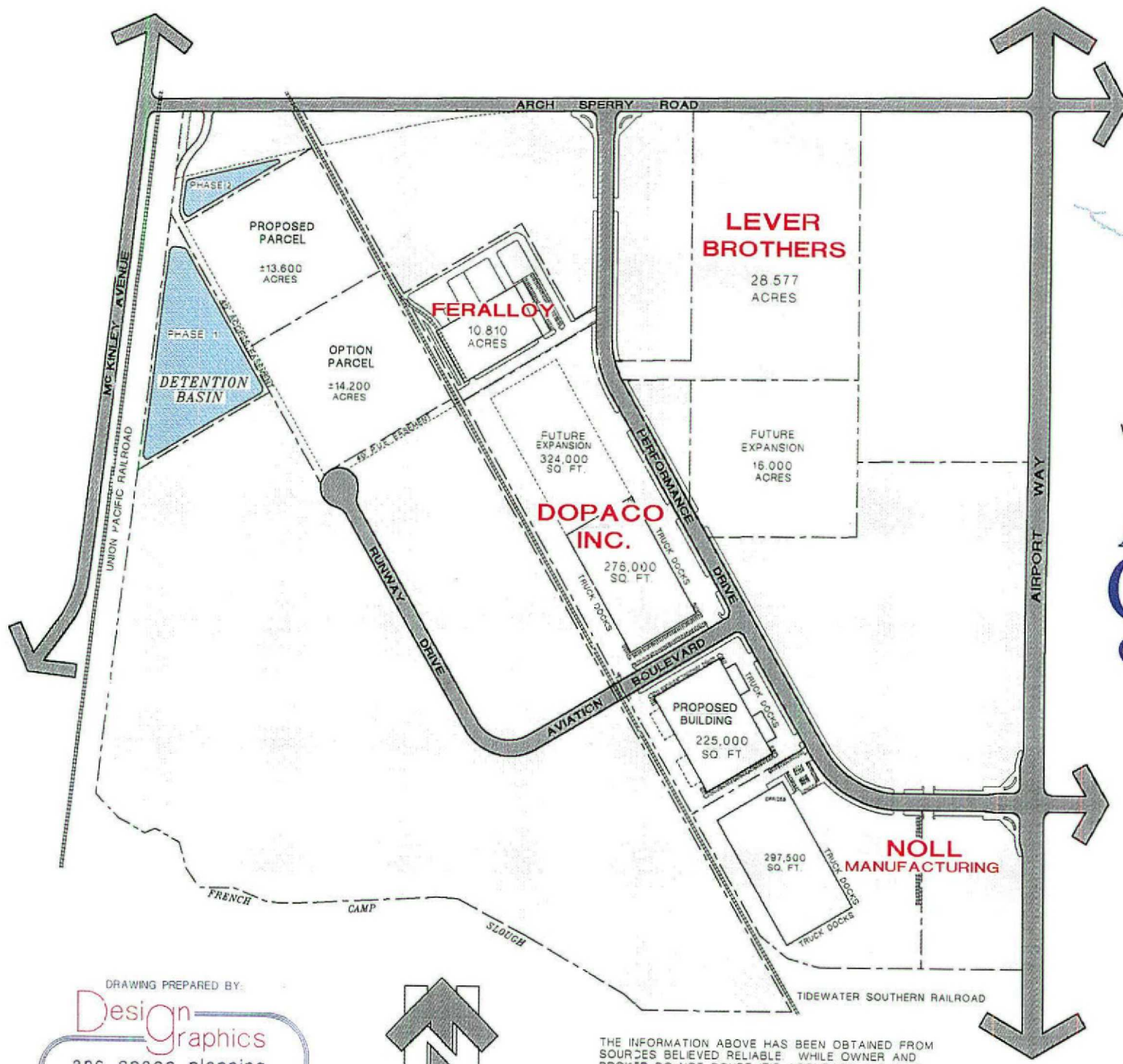
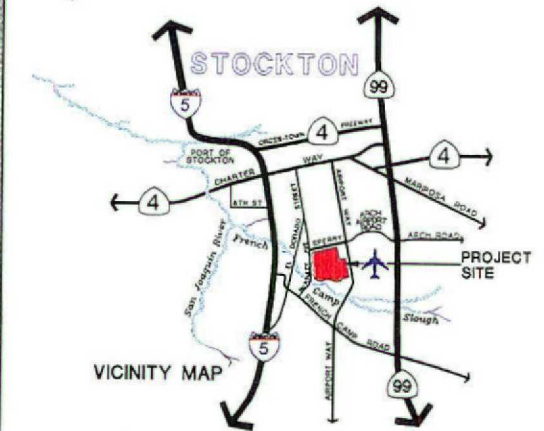


MOORE BIOLOGICAL

FIGURE 1
PROJECT LOCATION

Another Project By:

PANATTONI DEVELOPMENT COMPANY



AIRPORT GATEWAY CENTER

FOR FURTHER INFORMATION CONTACT:

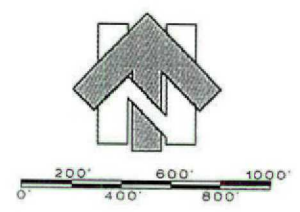
GREGORY O'LEARY
Vice President
OR
DAVE HAGGERTY
Vice President



CB COMMERCIAL
REAL ESTATE GROUP, INC.
BROKERAGE AND MANAGEMENT
BROKERAGE SERVICES

1776 MARCH LANE
SUITE 170
STOCKTON, CA 95207
TEL : (209) 476-2999
FAX : (209) 476-2960

DRAWING PREPARED BY:
Design Graphics
and space planning
1449 NEW ENGLAND DRIVE
ROSEVILLE, CALIFORNIA 95661
(913) 782-6200 FAX: 782-6202

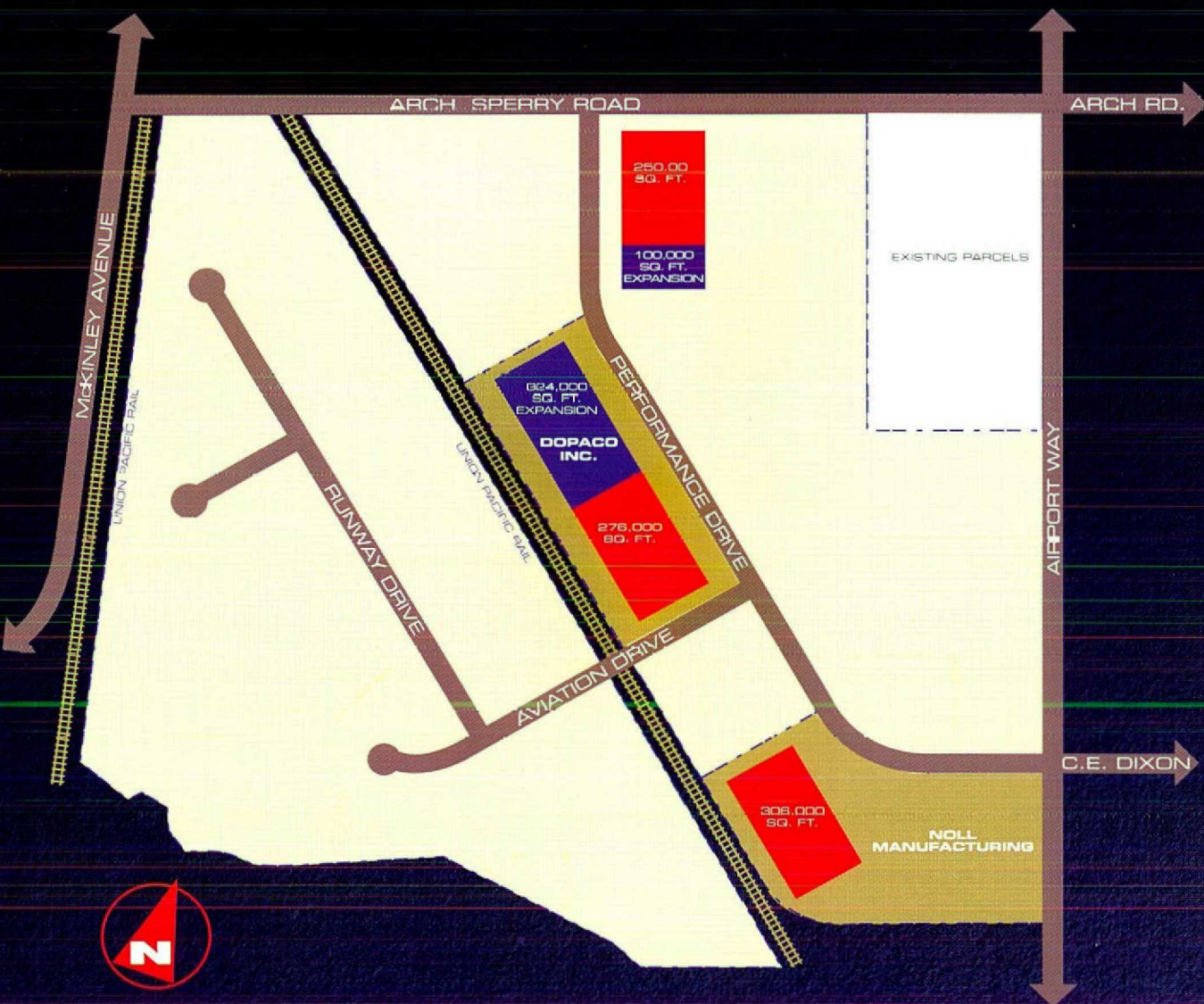


THE INFORMATION ABOVE HAS BEEN OBTAINED FROM SOURCES BELIEVED RELIABLE. WHILE OWNER AND BROKER DO NOT DOUBT IT'S ACCURACY, WE HAVE NOT VERIFIED IT AND MAKE NO GUARANTEE, WARRANTY, OR REPRESENTATION ABOUT IT. IT IS THE PURCHASER AND/OR LESSEE'S RESPONSIBILITY TO INDEPENDENTLY CONFIRM IT'S ACCURACY AND COMPLETENESS.

Airport Gateway Center is a 416-acre master-planned business park located directly west of the Stockton Metropolitan Airport. Panattoni-Catlin Venture of Sacramento has developed Stockton's premier location for warehouse distribution and manufacturing operations. The Park provides excellent access to Interstate 5, Highway 99 and the Crosstown Freeway. San Joaquin County and the City of Stockton have been selected by numerous industries as the preferred business location serving the Bay Area, Northern California, and the West Coast.

Project Features

- Attractive business park environment.
- Experienced and responsive development team capable of providing design build-to-suit facilities for sale or lease.
- Warehouse distribution and manufacturing space available from 45,000 sq. ft. to 600,000 sq. ft. and greater.
- Fully improved sites ranging from 1 to 150 acres.
- Union Pacific Rail and non-rail served sites. Union Pacific Railroad's state-of-the-art intermodal facility is conveniently located 3 miles to the south.
- Zoned E-P (Enterprise Performance), City of Stockton.
- Excellent transportation systems for trucking, rail, air service and shipping (Port of Stockton).
- CC&R's to protect long-term integrity of the Business Park.
- State of California Enterprise Zone.



Services

Sewer	City of Stockton
Water	City of Stockton
Storm Drainage	City of Stockton
Gas & Electric	PG&E

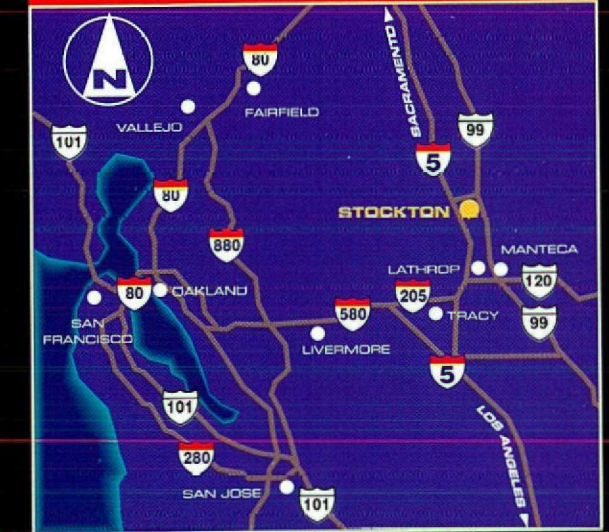
Mileage to Major Markets

NORTH		EAST	
Sacramento	49	Reno	177
Portland	632	Salt Lake City	697
Seattle	804	Denver	1,210

SOUTH		WEST	
Fresno	137	Oakland	73
Los Angeles	339	San Jose	76
Phoenix	709	San Francisco	83



Regional Map



Area Map



The information above has been obtained from sources believed reliable. While owner and broker do not doubt its accuracy, we have not verified it and make no guarantee, warranty or representation about it. It is purchaser and or lessee's responsibility to independently confirm its accuracy and completeness. Any projections, opinions, assumptions or estimates used are for example only and do not represent the current or future performance of the property. The value of this transaction to you depends upon tax and other factors which should be evaluated by your tax, financial and legal advisors. You and your advisors should conduct a careful, independent investigation of the property to determine to your satisfaction the suitability of the property for your need.

*Airport Gateway Center is a 416 Acre Master Planned
Business Park located in Stockton, California*

AIRPORT GATEWAY C E N T E R

Stockton, California



*Industrial Sites Available
Buildings For Sale, Lease or Build to Suit*

Contact: Gregory O'Leary
Vice President



Rail service provided by:



Contact: Gregory O'Leary
Vice President



Another project by:

Panattoni-Catlin
Venture

Airport Gateway Center is a 416 Acre Master Planned Business Park located in Stockton, California



*Contact: Gregory O'Leary
Vice President*



Rail service provided by:

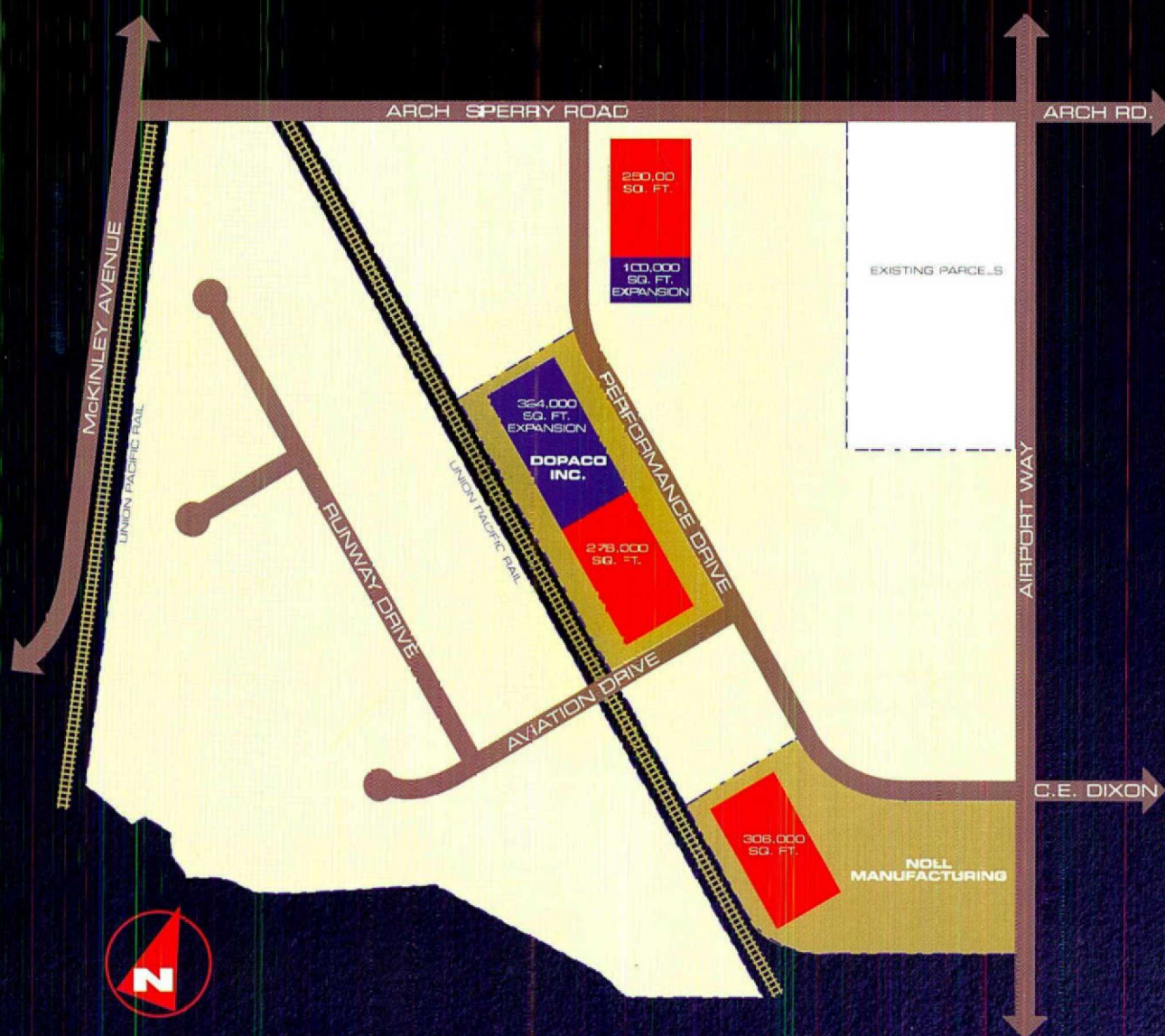


1776 W. March Lane, Suite 170 • Stockton, CA 95207 • Tel: (209) 476-2908 • Fax: (209) 476-2960

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AIRPORT GATEWAY C E N T E R

Stockton, California



*Industrial Sites Available
Buildings For Sale, Lease or Build to Suit*

Contact: Gregory O'Leary
Vice President



Another project by:

Panattoni-Catlin

Venture

1776 W. March Lane, Suite 170 • Stockton, CA 95207 • Tel: (209) 476-2908 • Fax: (209) 476-2960

WORK ROUTING AND SCHEDULE

TM3-97

Property Owner Airport gateway

File _____

- Proposal (circle). Architectural Review
 Code Amendment
 Conformity with GP
 C-4, C-1, Location & Dev Plan
 General Plan, Amendments
 Home Occupation Permit
 PURD
 Rezoning, Prezoning
 Specific Plan, Amendments
 Street Name Change
Tentative Map
 Use Permit, Designated 20 (16-084)
 Use Permit, District Use (16-088)
 Use Permit, Special Use (16-084 1)
 Use Permit, Temporary Use (16-070 2)
 Variance

Received by _____

Date _____

DIR JC DPTY [Signature]

INDEX _____ TECH _____

Environmental Action _____

Procedure (circle & date)
 Public hearing scheduled April 23, 1998
 14-day period ending on _____
 (designated 20 uses)
 Immediate consideration _____
 (temporary and district uses)

Names of Property Owners/Occupants - 300 feet
 Other _____

Notifications (circle)
 Notices to Owner/Occupants _____ Home Owner Association
 Newspaper Ad _____
 Agenda and Staff Report to Owner and Applicant _____
 Notices - Other Than Above _____

 Agenda, Attorney, etc

Graphics Required (Circle)
 Zoning Map _____ Plot Plan _____ General Plan _____
 Land Use Map _____ Elevation _____
 Other _____

Committee Consideration None Date 4/7/98

Referrals (circle)
 Downtown Stockton Assoc _____ Building Division
 Alcoholic Beverage Control _____ Fire Department
 S J Local Health District _____ Police Department
 Public Works Department _____ Housing/Redevelopment
 Dev Review/Project Design _____ S J County Planning
 Tech Planning/Permit Admin _____ COG/ALUC
 Other _____
 Date Referral Sent 2-13-98
 Date Referral Due Back 3-10-98

CITY OF STOCKTON

Sundry Field Receipt

CUSTOMER

237541

Dept COO / Div Planning

Received of Reno West I meters - Stockton

four thousand four hundred eighty ⁰⁰/₁₀₀ Dollars

Cash Money Order Check 4480

Remarks Supplemental GTR 3-821
ISS-97

Account # - - -

By JS Polk

Date 1/28/98 Print Name JS POLK

PAY CODE (S)

\$ 4480 - (90)

\$ ()

\$ ()

CITY OF STOCKTON

Sundry Field Receipt

DEPARTMENT

224238

Dept CDD / Div Planning

Received of Norma Thomas (of her New Venture,
Three thousand four hundred ten Dollars

Cash Money Order Check

Remarks ~~XXXXXXXXXX~~ (District walking
11/3-97 project)

Account # _____

By _____

Date 4/9/00 Print Name JENNY CLARK

PAY CODE (S)

\$ 3,410.00 ()

\$ _____ ()

\$ _____ ()

DEVELOPMENT REVIEW COMMITTEE MINUTES

April 7, 1998

Committee Members Present Bob Sivell, Gregg Meissner for Paul Sensibaugh and Sam Mah

Committee Members Absent Victor Machado and Mark Madison

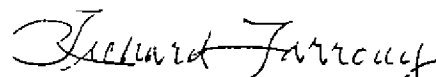
Staff Members Present Richard Larrouy, Guy Petzold, Kathy Tomura and David Stagnaro

A Minutes The minutes of the meeting of March 31, 1998 were approved

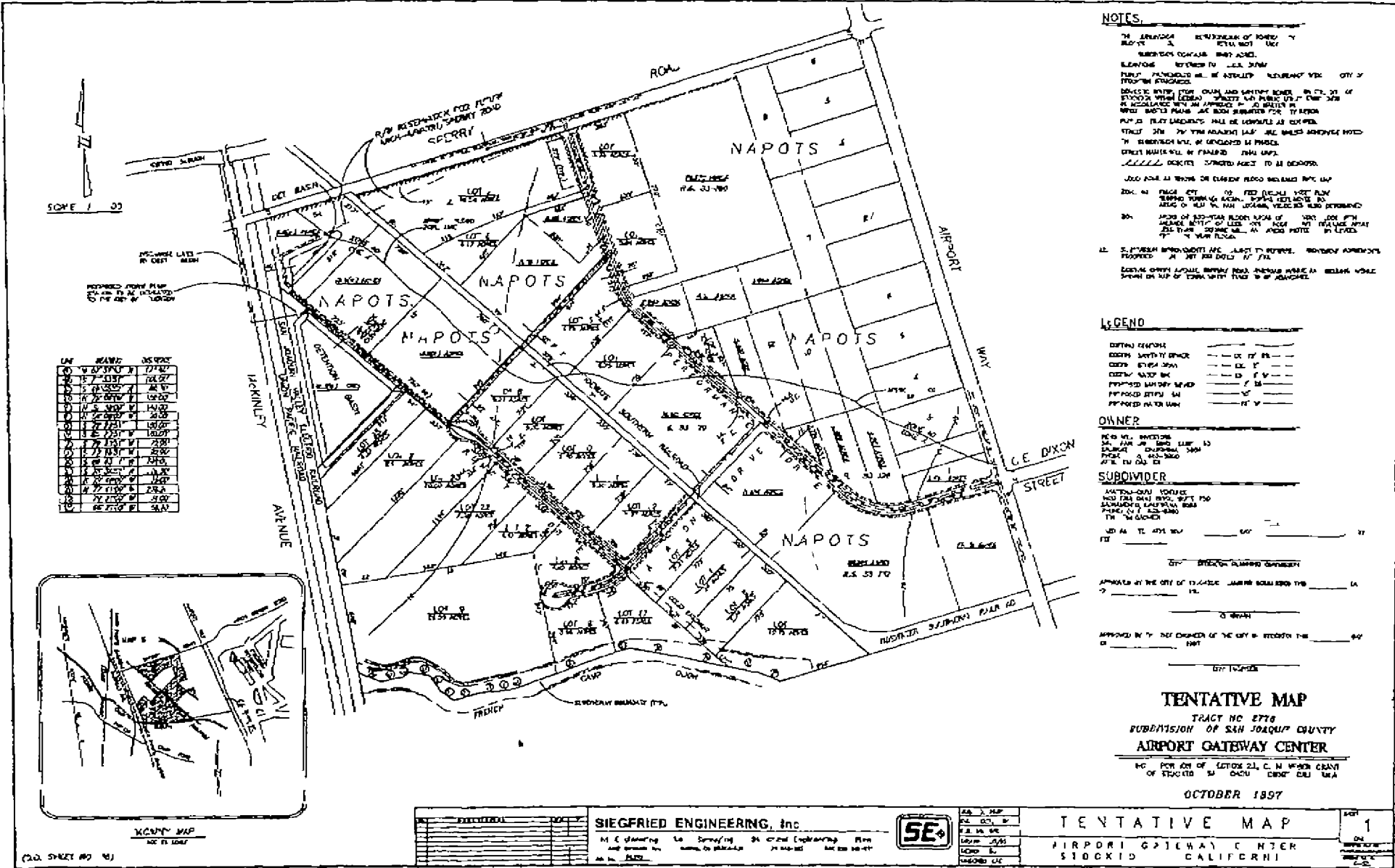
B Development Review

Item B-1 Tentative Map - Request of Panattoni-Catlin Venture for a tentative map to subdivide 190 87-acre parcel into twenty-five parcels for property located south of Sperry Road, west of Airport Way, north of French Camp Slough and east of McKinley Avenue (TM3-97--Airport Gateway Center) A motion (Meissner/Mah) was made and approved to recommend approval of TM3-97 based on the findings and subject to the proposed conditions contained in the DRC staff report and those submitted at the meeting by the Public Works Department

There was also a general discussion regarding mitigation measures in the environmental document and the deferred improvement agreement for the site In a telephone conversation during the meeting Mark Madison provided information about the site's storm drainage


RICHARD LARROUY
SECRETARY

20 1 1707348892 08 781 09 14 44 15 19 33 35 37 38 39 40 41 42 43 44 45 46 47 48 49 50 51 52 53 54 55 56 57 58 59 60



NOTES

1. ALL DISTANCES ARE IN FEET UNLESS OTHERWISE NOTED.
2. ELEVATIONS ARE IN FEET UNLESS OTHERWISE NOTED.
3. ALL CORNERS ARE TO BE MONUMENTED.
4. ALL DISTANCES ARE TO BE MEASURED AS SHOWN ON THIS MAP.
5. ALL DISTANCES ARE TO BE MEASURED AS SHOWN ON THIS MAP.
6. ALL DISTANCES ARE TO BE MEASURED AS SHOWN ON THIS MAP.
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18. ALL DISTANCES ARE TO BE MEASURED AS SHOWN ON THIS MAP.
19. ALL DISTANCES ARE TO BE MEASURED AS SHOWN ON THIS MAP.
20. ALL DISTANCES ARE TO BE MEASURED AS SHOWN ON THIS MAP.

LEGEND

OWNER
 SUBDIVIDER
 APPROVED BY THE CITY OF STOCKTON
 APPROVED BY THE BOARD OF SUPERVISORS
 APPROVED BY THE COUNTY ENGINEER
 APPROVED BY THE COUNTY CLERK

TM3-99

Note Some of the City Departments/Divisions that received this referral were unable to provide comments to the Planning Division in time to be included in this DRC staff report. Please be prepared to provide at the DRC meeting, any conditions/notes that your Department/ Division needs to have included as part of the tentative map.

Item B1 Tentative Map Request of Panattoni-Catlin Venture (TM3-97)

Data Nature of Review Subdivision of one parcel into twenty-five (25) parcels

Size and Location 190.87 acres located south of Sperry Road, west of Airport Way, north of French Camp Slough and east of McKinley Avenue

General Plan Industrial uses

Environmental Clearance The environmental consequences of developing this site for industrial uses were examined in SEIR/IS 8-97

Existing Zoning E-P, Enterprise Performance District.

Existing Land Use Partially developed with industrial/warehouse uses

Public Utilities All are available

Public Services Police and fire protection services are available

Flood Protection The Federal Emergency Management Agency (FEMA) has completed the flood insurance re-studies in San Joaquin County and the City of Stockton. These existing San Joaquin County and City of Stockton Flood Insurance Studies, which delineate flood risks, flood-plains, and floodway areas are being re-evaluated. It is possible that the re-evaluation may delineate additional flood risks which may impact development in Stockton and this project.

Discussion The proposal is to subdivide one parcel of approximately 190.87 acres into twenty-five (25) parcels varying in size from 1.75 to 29 acres. Access to and egress from the proposed parcels will be provided via Performance (existing), Aviation (proposed) and Runway Drives (proposed).

Consensus Agencies responding to date have recommended approval of this tentative map subject to the proposed conditions in the staff report.

Basis For Approval

- 1 The map complies with all applicable provisions of the California State Map Act and the City of Stockton standards and regulations
- 2 None of the grounds for denial as stipulated in Section 66474 of the State of California Subdivision Map Act are applicable on this tentative map
- 3 The proposed project is consistent with the City of Stockton General Plan for the location of Industrial uses
- 4 The environmental consequences of developing this site for Industrial uses were examined in SEIR/IS 8-97

Proposed Conditions

- 1 Comply with applicable Federal, State, County and City codes, regulations and adopted standards and pay all applicable fees
- 2 This tentative map is subject to the provisions of the Deferred Improvement Agreement (Recorded on July 23, 1997 - Instrument No 97071908, affecting the subject property) Said agreement shall be binding on the owners, developers and/or successors-in-interest
- 3 The owners, developers and successors-in-interest shall comply with the mitigation measures identified in EIR3-82 and SEIR/IS8-97
- 4 The owners, developers and/or successors-in-interest shall obtain permits and shall comply with all requirements of all governmental agencies with jurisdiction over French Camp Slough including, but not limited to, dedication of right of way, access easements, fencing, outfall structure and necessary improvements to the French Camp Slough

The Parks and Recreation Department and Municipal Utilities Department/Stormwater Division, respectively, have recommended that the following notes be added to the tentative map

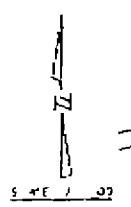
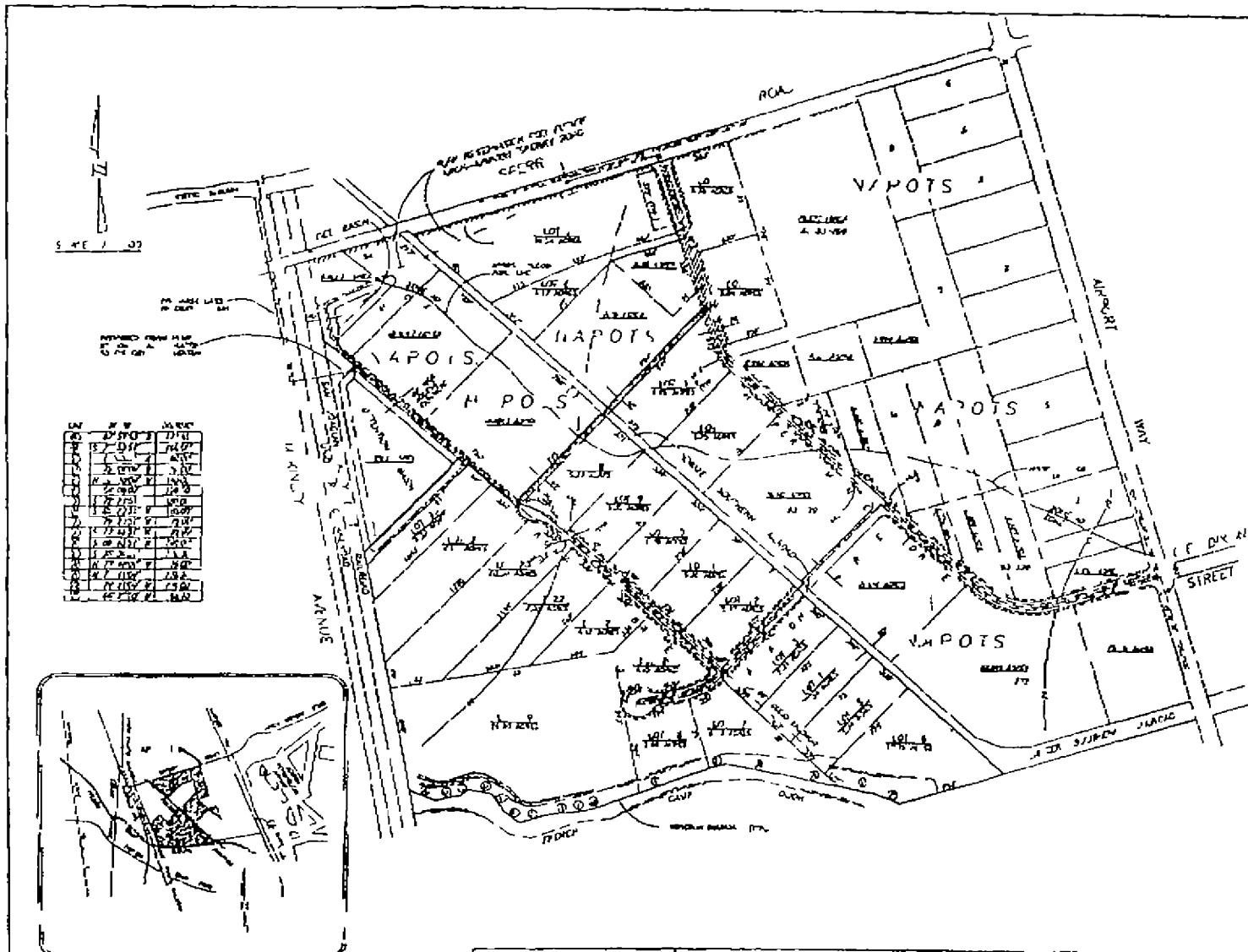
- 1 Proposed oak tree removals shall be subject to mitigation measures as described in SEIR/IS8 97 (proposed Note No 14)

- 2 The property owners, developers and/or successors-in-interest shall comply with the provisions of the California General Construction Activity Storm Water Permit and State Water Resources Control Board Order Number 92-08-DWQ Compliance is mandatory per the City of Stockton's Grading and Erosion Control Ordinance, Chapter 13, Part V, Sections 13-500 through 13 513 of the Stockton Municipal Code (Proposed Note No 15)**

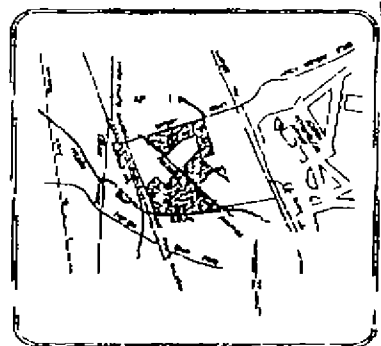
In addition, the following information should be forwarded to the owners/developers

- 1 Show the location of Heritage Oak Trees, including trunk diameter**
- 2 Show the location of other oak trees, which can be depicted in groupings, if applicable**
- 3 The engineer shall prepare the final map for this tentative map area using CCS-83 datum**
- 4 The engineer shall be reminded that direct taps are not permitted in 18 inch sewer lines**
- 5 Any on-site wells and septic tanks shall be abandoned and destroyed as required by the San Joaquin County Department of Environmental Health prior to the issuance of building permits**
- 6 A minimum of a 10-foot wide Public Utility Easement shall be provided along all street frontages within the subdivision**
- 7 A soils report shall be completed prior to issuance of building permit**
- 8 Street names shall be subject to approval by the Community Development Department**

April 3, 1998



Lot #	Acres	Area (sq. ft.)	Area (sq. ft.)
1	0.21	9,000	9,000
2	0.21	9,000	9,000
3	0.21	9,000	9,000
4	0.21	9,000	9,000
5	0.21	9,000	9,000
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8	0.21	9,000	9,000
9	0.21	9,000	9,000
10	0.21	9,000	9,000
11	0.21	9,000	9,000
12	0.21	9,000	9,000
13	0.21	9,000	9,000
14	0.21	9,000	9,000
15	0.21	9,000	9,000
16	0.21	9,000	9,000
17	0.21	9,000	9,000
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97	0.21	9,000	9,000
98	0.21	9,000	9,000
99	0.21	9,000	9,000
100	0.21	9,000	9,000



NOTES:

- THE AREA SHOWN IS THE PRESENT STATE OF THE PROPERTY AS IT NOW EXISTS.
- THE PROPERTY IS OWNED BY THE STATE OF CALIFORNIA.
- THE PROPERTY IS BEING OFFERED FOR SALE BY THE STATE OF CALIFORNIA.
- THE PROPERTY IS BEING OFFERED FOR SALE BY THE STATE OF CALIFORNIA.
- THE PROPERTY IS BEING OFFERED FOR SALE BY THE STATE OF CALIFORNIA.

LEGEND

- CORNER MARKER ---
- EASEMENT ---
- PROPERTY LINE ---
- LOT CORNER ---
- PROPERTY CORNER ---
- PUBLIC ROAD ---
- RAILROAD ---
- RIVER OR STREAM ---
- UNPLANNED AREA ---
- UNPLANNED AREA ---

DINNER

BY THE ENGINEER

SUBDIVIDER

BY THE ENGINEER

BY THE ENGINEER

BY THE ENGINEER

BY THE ENGINEER

TENTATIVE MAP
 TRACT NO. 2778
 SUBDIVISIONS OF SAN JOAQUIN COUNTY
AIRPORT GATEWAY CENTER
 E. M. KAY, CIVIL ENGINEER, S.F. CALIF.

OCTOBER 1997

<p>NO. 1 MAP</p> <p>DATE</p> <p>SCALE</p> <p>SHEET</p> <p>REVISION</p>	<p>SIEGFRIED ENGINEERING Inc</p> <p>1600... STREET, STOCKTON, CALIF. 95210</p> <p>TEL. (209) 922-1234 FAX. (209) 922-1234</p>	<p>TENTATIVE MAP</p> <p>AIRPORT GATEWAY CENTER</p> <p>STOCKTON, CALIFORNIA</p>	<p>SHEET 1</p> <p>TOTAL SHEETS</p>
--	--	---	---

TM3-97

NOTES 2/2/98
 CALLED M HAKEEM TO REMIND HIM OF REQ'D T M APPLICATION REVISION BEFORE WE CAN PROCEED. LEFT MESSAGE
 2/3/98
 TIC w/ m H - HE WILL REVISE MAP APP REQUESTED MTK. w/ ME 2 9 00 2-4-98 DJS

CONTACT PERSONS AND/OR OTHERS INTERESTED PARTIES

NAME	ADDRESS	PHONE/FAX	COMMENTS
Stan Oshita		943-2021	
Kam Luangrath		943-2021	

CITY OF STOCKTON
NOTICE OF DETERMINATION

TO X County Clerk
San Joaquin County

Office of Planning and Research
1400 10th Street
Sacramento CA 95814

RECORDED
COUNTY CLERK
JAMES
99 JUN -3 10 57
SAN JOAQUIN COUNTY

FROM Lead Agency
City of Stockton
c/o Community Development Department
Planning Division
425 North El Dorado Street
Stockton CA 95202 1997

BY Patricia Paulsen
DEPUTY

Contact Person David Stagnaro, Associate Planner Phone (209) 937 8266

SUBJECT NOTICE OF DETERMINATION PURSUANT TO PUBLIC RESOURCES CODE SECTION 21152 AND CAL CODE OF REGULATIONS, TITLE 14, SECTIONS 15075, 15094, AND/OR 15096(i)

Project Title Reno West Investors, Ptp, et al - Tentative Map (Revised)
Initial Study File No Addendum/Initial Study (IS6 99) to a previously certified Supplemental EIR (SEIR3 82/IS8 97)
State Clearinghouse No SCH#82120217 Discretionary Application(s) File No (s) TM3 97 (Revised)
Project Applicant Hakeem, Ellis, Simonelli and Marengo
Project Description/Location Revise Mitigation Measures related to the revision of Tentative Map (TM3 97) Condition No 12 and delete Condition Nos 13 and 14 for the Airport Gateway Center Project, located south of Sperry Road and West of Airport Way

DETERMINATIONS This is to advise that the City of Stockton approved the above described project/action on MAY 27, 1999, and has made the following determinations regarding the project

- 1 The project (X will) (will not) have a significant effect on the environment
- 2 X An environmental impact report was prepared and certified for this project pursuant to the provisions of CEQA
 A Negative Declaration was prepared and adopted for this project pursuant to the provisions of CEQA
- 3 Mitigation measures (X were) (were not) incorporated as part of the approval of the project
- 4 Mitigation Monitoring and Reporting Program (X was) (was not) adopted for this project
- 5 Statement of Overriding Considerations (X was) (was not) adopted for this project
- 6 Pursuant to Cal Code of Regulations Title 14 Sections 753 5(a) or 753 5(c)
 California Department of Fish and Game (CDFG) fees are required as applicable and will be filed with this Notice of Determination (NOD) or
 This project is exempt from the CDFG fees and a Certificate of Fee Exemption has been prepared and will be filed with this NOD
X CDFG Fees were filed with a prior NOD for this project/proposal (see attached receipt)

This is to certify that the environmental documentation and determinations for the project/action and any related mitigation measures monitoring provisions findings and statements of overriding consideration have been adopted on the basis of the whole record before the City and reflect the City's independent judgement and analysis The environmental review period and record of project approval may be examined at the above-noted Lead Agency address

JOHN CARLSON DIRECTOR
COMMUNITY DEVELOPMENT DEPARTMENT

By 
David Stagnaro, Associate Planner

Date 6-1-99

AFFIDAVIT OF FILING AND POSTING

I declare that on the date stamped above I received and posted this notice as required by California Public Resources Code Section 21152(c) Said notice will remain posted for 30 days from the filing date

Signature Patricia Paulsen Title DEPUTY COUNTY CLERK

Posting Period Ending Date 7/7/99



DEPARTMENT OF FISH AND GAME
 ENVIRONMENTAL FILING FEE CASH RECEIPT
 DFG 753.5a (8-91)

49249

Lead Agency City of Stockton Date 4/27/98
 County/State Agency of Filing San Joaquin Document No _____
 Project Title Duck Creek Ind. General Plan Amend/Rezoning
 Project Applicant Name John & K. Verner et al Phone Number _____
 Project Applicant Address 8980 Marston Dr Stockton 95212
 Project Applicant (check appropriate box) Local Public Agency School District Other Special District
 State Agency Private Entity

CHECK APPLICABLE FEES

- Environmental Impact Report \$850 00 \$ _____
- Negative Declaration ISI-97 \$1 250 00 \$ 1250.00
- Application Fee Water Diversion (State Water Resources Control Board Only) \$850 00 \$ _____
- Projects Subject to Certified Regulatory Programs \$850 00 \$ _____
- County Administrative Fee \$25 00 \$ 25.
- Project that is exempt from fees

TOTAL RECEIVED \$ 1275.00

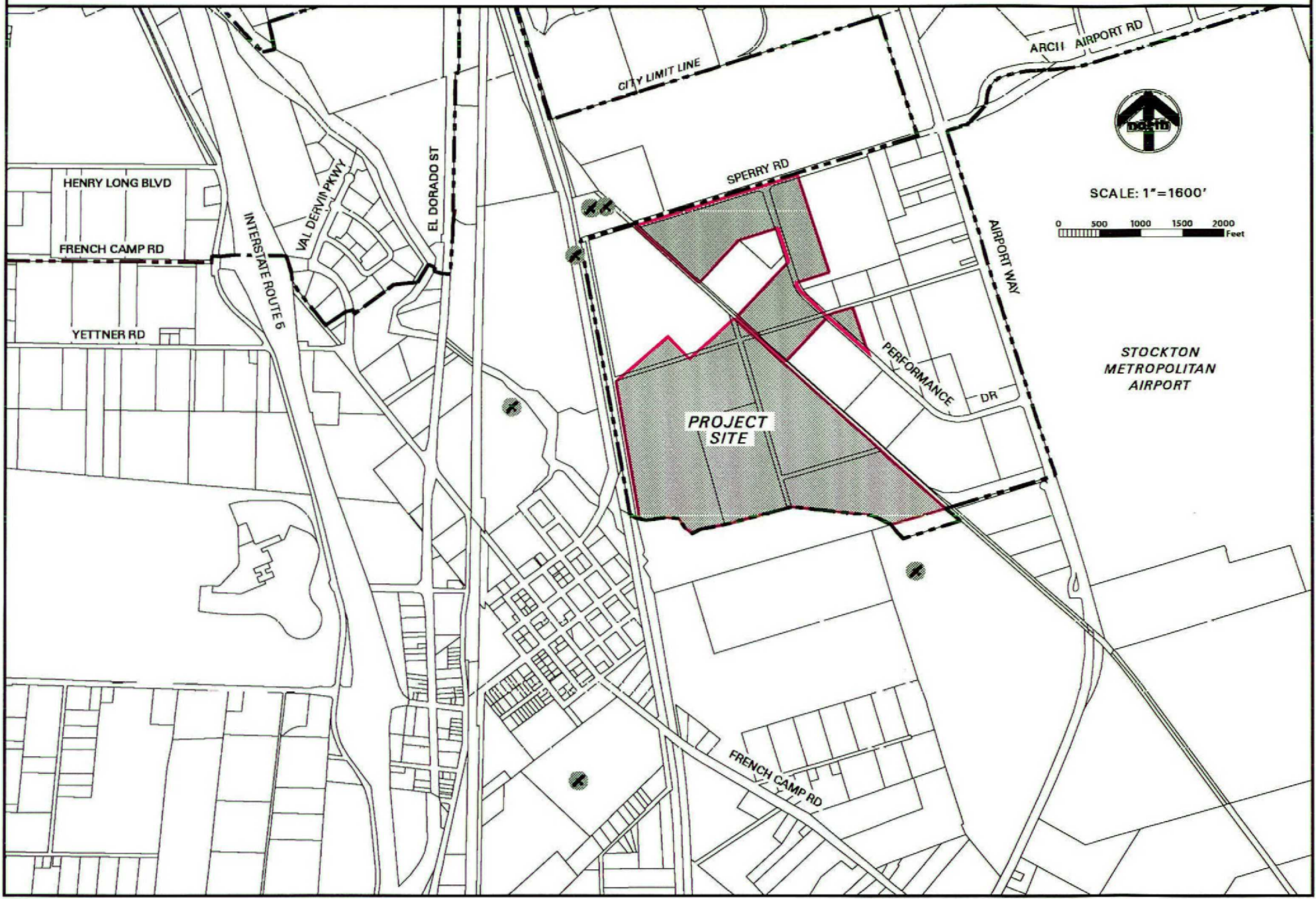
Signature and title of person receiving payment Patricia Paulsen, Dep. Co. Clerk

FIRST COPY-PROJECT APPLICANT SECOND COPY DFG/FASB THIRD COPY-LEAD AGENCY FOURTH COPY-COUNTY/STATE AGENCY OF FILING

Signature and title of person receiving payment
 FIRST COPY-PROJECT APPLICANT SECOND COPY DFG/FASB

TM3-97 Revised

AIRPORT GATEWAY PROJECT - SWAINSON'S HAWK NESTING SITES



From Dora Sidrian
To Stagnaro David
Date 5/21/99 9 34AM
Caller Laura Mahoney
Company Canaponi Dev
Phone 916 362 5571 FAX 916 362 0161

Telephoned
 Will call again
 Wants to see you
 Urgent

Please call
 Returned your call
 Came to see you

Is missing the Post approval letter with conditions of approval of subdivision signed by JC for TM3 97

Do you have the file? She asked that I fax it to her

*letter dated 5-5-98 faxed to her
as requested Dms*

TM3-97

Associate Planner Jenny Liaw summarized the staff report for the record

Delores B Peterson applicant appeared in support of the request for a HOP and indicated that she would like the Commission to move forward with her request She submitted a letter in support of the proposed Home Occupation Permit from the homeowners association and noted that Ann Lopez President of the homeowners association is present should the Commission want to ask her any questions She also stated that customers will not be coming to her home to do business

No one else desired to be heard and the public hearing was closed

MOTION It was duly moved (Bestolarides) seconded (Cusumano) and carried 6 to 0 (Lowery absent) to approve the request of Delores B Peterson for a Home Occupation Permit (HOP24 98) based on the findings and subject to the condition as listed in the staff report

Item E-3(a). Public Meeting Certification of the Final Supplemental Environmental Impact Report/Initial Study for the Airport Gateway Center Project (SEIR3 82/IS8 97) and adoption of the "Findings of Fact and Mitigation Monitoring Program" for a tentative map to subdivide 190.87± acres into twenty-five parcels varying in size from 1.75 to 29 acres located south of Sperry Road, west of Airport Way, north of French Camp Slough and east of McKinley Avenue. and



Item E 3(b). Public hearing regarding the request of Reno West Investors, et al, for a tentative map to subdivide 190.87± acres into twenty five parcels varying in size from 1.75 to 29 acres located south of Sperry Road, west of Airport Way, north of French Camp Slough and east of McKinley Avenue (TM3 97-Airport Gateway Center).

The public hearing was declared open and the Affidavits of Mailing and Publication were filed

Associate Planner David Stagnaro summarized the staff report for the record He noted that the applicant s attorney will introduce some new language meant to clarify proposed Conditions Nos 12 13 and 14

Director Carlson referred the Commission to the two pieces of correspondence from environmental consultant Charlie Simpson pertaining to the errata for the environmental document and a memorandum from the City s Economic Development Department in support for the project

Mike Hakeem attorney representing the applicants introduced the engineers and environmental consultant for the project He gave a brief history of the proposed project site and distributed a pamphlet highlighting the project s features and the businesses that will be occupying the center He discussed the new language which has the concurrence of staff clarifying proposed Conditions Nos 12 13 and 14 pertaining to traffic which would allow the City to reevaluate the applicability of mitigation measures and associated Conditions of Approval

No one else desired to be heard and the public heanng was closed

MOTION. It was duly moved (Panizza) seconded (Giovanetti) and carried 6 to 0 (Lowery absent) to certify the Final Supplemental Environmental Impact Report/Initial Study for the Airport Gateway Project (SEIR3 82/IS8-97) including the Additional Errata dated April 23 1998

MOTION. It was duly moved (Bestolarides) seconded (Cusumano) and carried 6 to 0 (Lowery absent) to approve the tentative map request of Reno West Investors et al (TM3 97 Airport Gate Center Project) based on the findings and subject to the conditions as listed in the staff report including new language to clarify proposed Condition Nos 12 13 and 14 pertaining to trip generation assumptions derived from various land use mixes which would allow the City to reevaluate the traffic characteristics and traffic impacts of land uses in Airport Gateway--new information may be used by the City to reevaluate the applicability of mitigation measures and associated Conditions of Approval

Item E 5. Public hearing regarding the request of Sun Chor and Betty Gong, et al, for a Use Permit to construct a commercial parking lot on a residentially zoned parcel located at 1327 South Monroe Street (UP10 98).

The public hearing was declared open and the Affidavit of Mailing was filed

Assistant Planner Dean Ruiz summarized the staff report for the record

Glen Bouday representing Kragen Auto Parts 465 East Missoun Avenue Phoenix Arizona highlighted the features of the proposal He stated that the reason for the request is because they feel they cannot operate the facility with the minimum amount of parking stalls He stated that there are approximately 150 stores throughout the United States and it has been their experience that a building of this size will need to have a minimum of 35 parking stalls They plan to enhance the area provide landscaping and provide a safe area The hours of operation for the store will be from 8 a m to 9 p m Monday thru Friday 8 a m to 8 p m Saturday and 8 a m to 7 p m on Sunday and there will be a store manager on site at all times

(Commissioner Panizza stepped down due to a conflict of interest.)

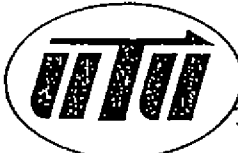
Commissioner Blanchard asked the applicant if he wanted to continue the public hearing due to only six Commissioners being present Mr Bouday replied that he would like the Commission to move forward with the public hearing

Tim Gong 1320 South Van Buren spoke in opposition to the request and was concerned that this use could have a negative impact on the adjacent residential neighborhood He was especially concerned that customers might work on their vehicles in the parking lot dropping excess oil onto the parking lot and which could pose hazardous health situation

In rebuttal Mr Bouday indicated that oil is sold in a closed container they are very conscientious about not allowing customers to work on their vehicles in the parking lot nor do they offer auto repair service at any of their stores

No one else desired to be heard and the public hearing was closed

To: Planning / Greg / Mike Hakeem - in 60 FL/DSS/file SF 6 15 98



united transportation union

CALIFORNIA LEGISLATIVE BOARD AFL - CIO

1005 12TH STREET SUITE 4 SACRAMENTO CALIFORNIA 95814 3920 (916) 441 2051 FAX (916) 441 2054

Text on map

RECEIVED
JUL 21 1998

RECEIVED
JUL 22 1998
CITY CLERK
CITY OF STOCKTON

July 21 1998

City of Stockton
425 North El Dorado Street Room 314
Stockton California 95202
CITY OF STOCKTON
UNIFIED LABOR UNION

Dear Sir

I am writing with regard to Application 98-07 016 filed with the Public Utilities Commission for permission to construct a one grade crossing of the Tidewater Southern Pacific Company at Aviation Drive within the Airport Gateway Center in the City of Stockton

Please accept this letter as a formal request that our organization be furnished with a copy of the application in this matter, in addition to any exhibits or supplementary material. Additionally, I would like to request that our organization be included on your mailing list for any future correspondence (e.g. amendments) you may transmit relative to this matter.

Our organization represents among others, the operating crews of the Tidewater southern Railroad Company, and it is on behalf of our members that we have taken an interest in this application.

Very sincerely

James (J P) Jones
State Legislative Director

JPJ/ab
appreq 001

MSCL 7 LIB
GP 1 MTS
CITY 1 MUD
HRD 1 PW 1

FROM KEN MOECKEL, SR. CIVIL ENGINEER

DEPT PUBLIC WORKS

DATE 5-5-98

85715

TO JOHN CARLSON, DIRECTOR

DEPT COMMUNITY DEVELOPMENT

FOR YOUR INFORMATION ACTION COMMENT APPROVAL RECOMMENDATION ESTIMATE REPORT RETURN

SUBJECT APPROVED TENTATIVE MAP - AIRPORT GATEWAY CENTER (TM3-97)

Attached for your files are the following

1 A copy of the letter to the owner approving the tentative map and a copy of the tentative subdivision map statement,

2 A copy of the Development Review Committee staff report and minutes,

3 Comments from P.G. & E, San Joaquin County Public Works, City of Stockton Building, Public Works, Parks & Recreation, and HUD, and

4 Sepia

JC rcw

Attachments

SIGNED *John Carlson* rcw
JOHN CARLSON, DIRECTOR

REPLY

*Thank you
David & Carey*

REPLY DATE

DEPT

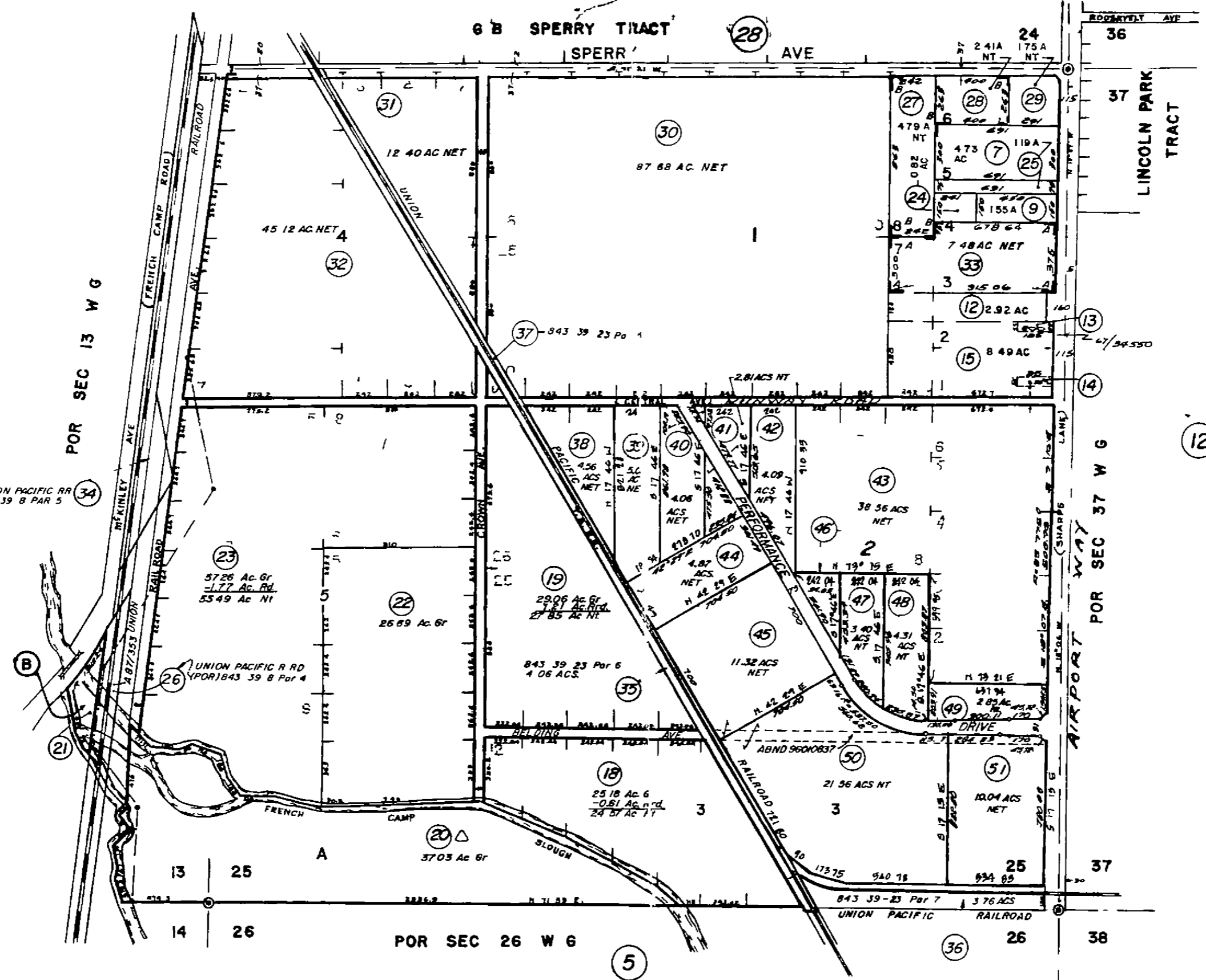
SIGNED

RETURN TO ORIGINATOR

Bk 193

UNION PACIFIC RR 843 39 B PAR 5

UNION PACIFIC R RD (VPOR) 843 39 B PAR 4



A - R S Bk 29 Pg 13
B - R S Bk 29 Pg 149

TERRA MATRE TRACT

SCALE 1"=600'
SAN JOAQUIN COUNTY
ASSESSOR'S MAPS

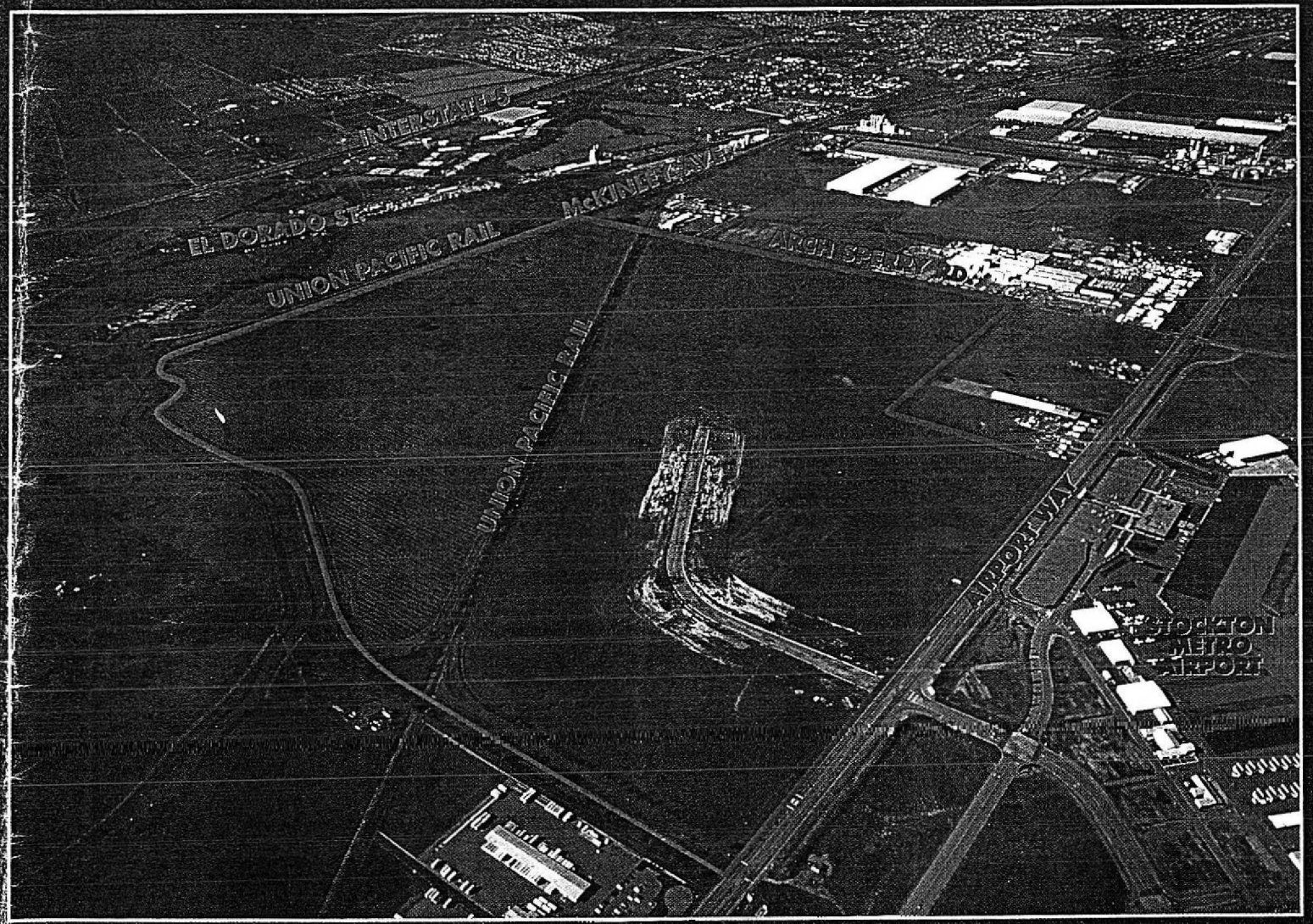
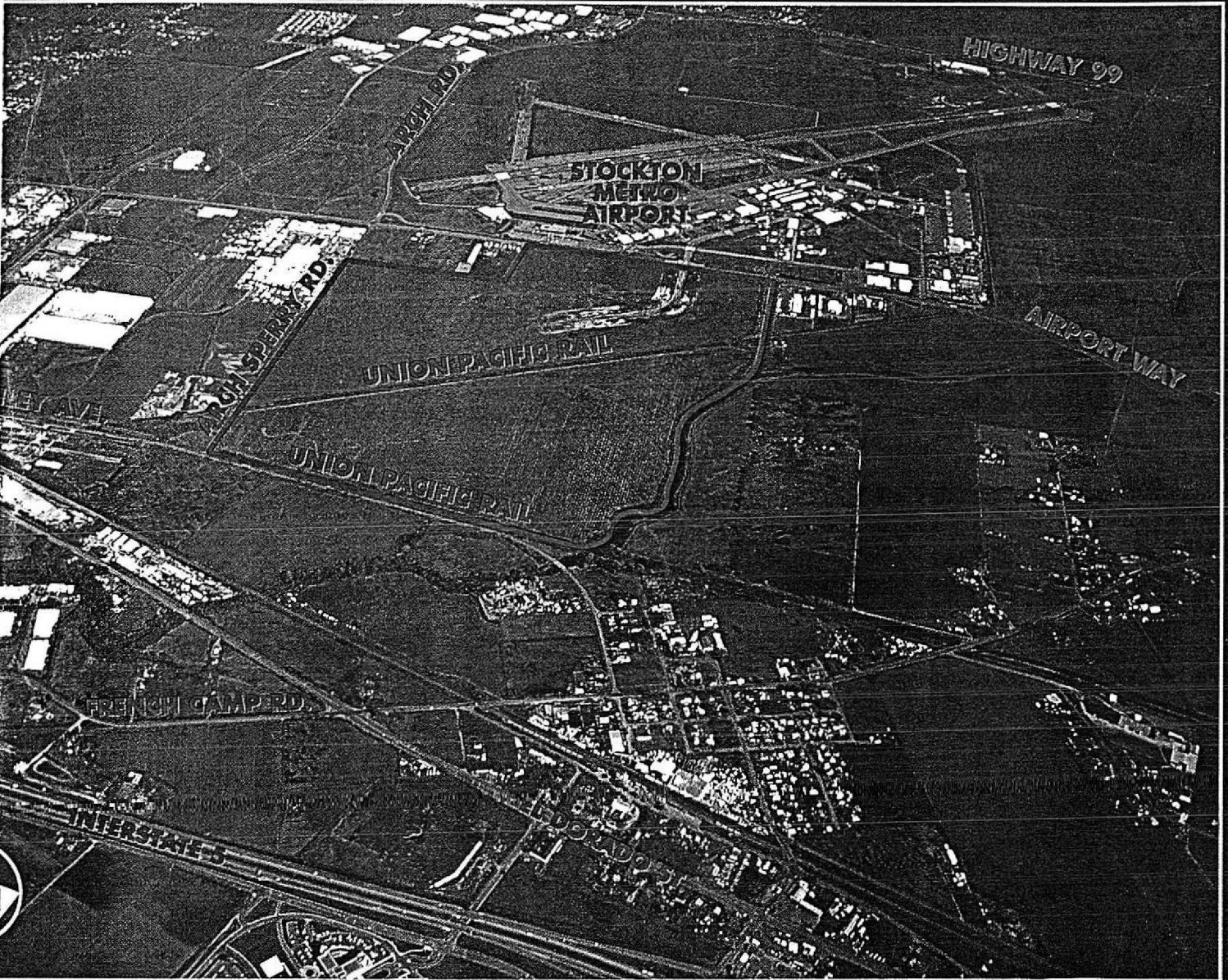
△-WILLIAMSON ACT PARCELS

TERRA MATRE TRACT

Airport Gateway Center is a 416 Acre Master Planned Business Park located in Stockton, California

AIRPORT GATEWAY CENTER

Stockton, California



*Industrial Sites Available
Buildings For Sale, Lease or Build to Suit*

Contact: Gregory O'Leary
Vice President

Rail service provided by:



Contact: Gregory O'Leary
Vice President

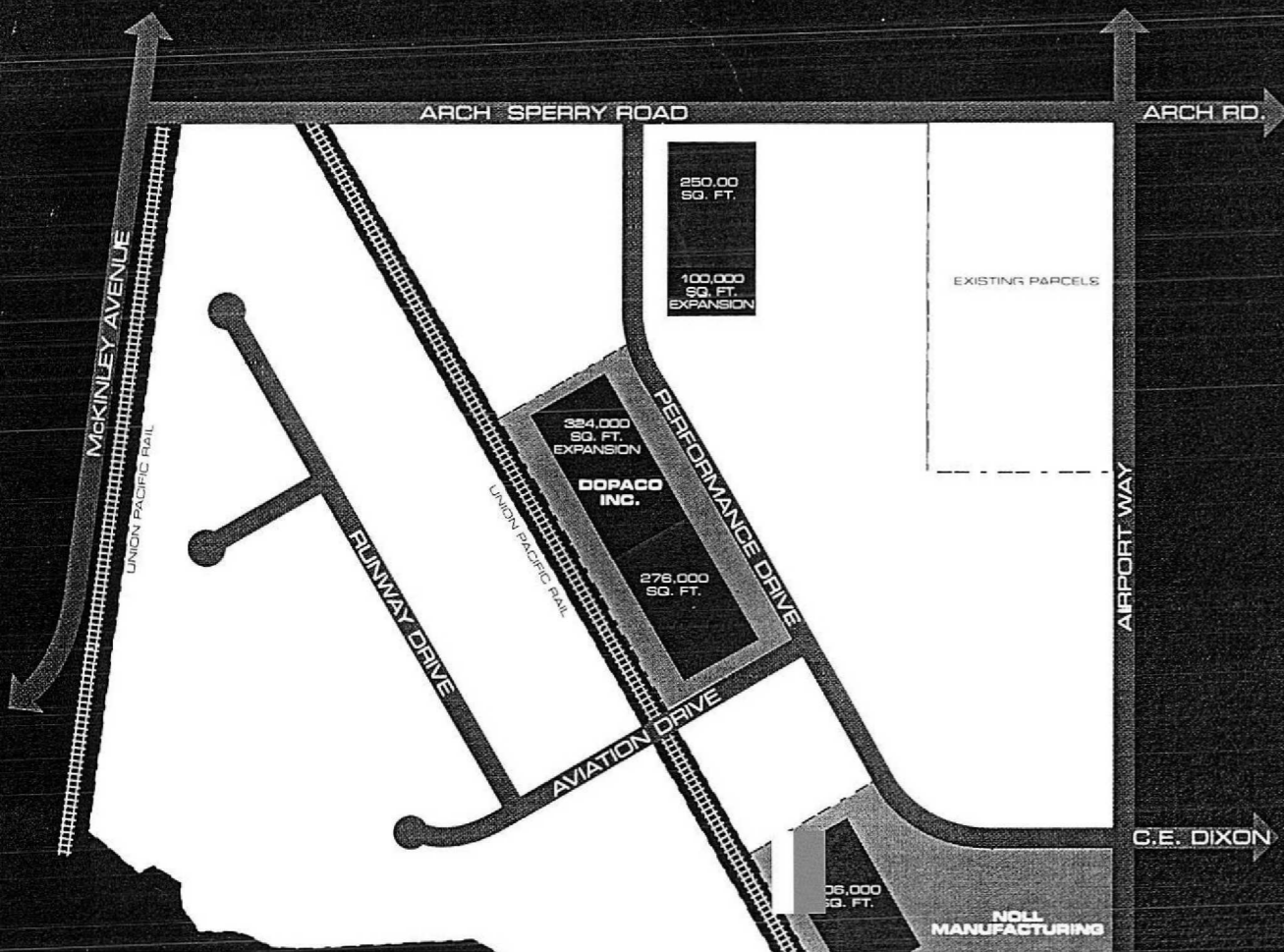


Another project by:

Airport Gateway Center is a 416-acre master-planned business park located directly west of the Stockton Metropolitan Airport. Panattoni-Gatlin Venture of Sacramento has developed Stockton's premier location for warehouse distribution and manufacturing operations. The Park provides excellent access to Interstate 5, Highway 99 and the Crosstown Freeway. San Joaquin County and the City of Stockton have been selected by numerous industries as the preferred business location serving the Bay Area, Northern California, and the West Coast.

Project Features

- Attractive business park environment.
- Experienced and responsive development team capable of providing design build-to-suit facilities for sale or lease.
- Warehouse distribution and manufacturing space available from 45,000 sq. ft. to 600,000 sq. ft. and greater.
- Fully improved sites ranging from 1 to 150 acres.
- Union Pacific Rail and non-rail served sites. Union Pacific Railroad's state-of-the-art intermodal facility is conveniently located 3 miles to the south.
- Zoned E-P (Enterprise Performance), City of Stockton.
- Excellent transportation systems for trucking, rail, air service and shipping (Port of Stockton).
- CC&R's to protect long-term integrity of the Business Park.
- State of California Enterprise Zone.



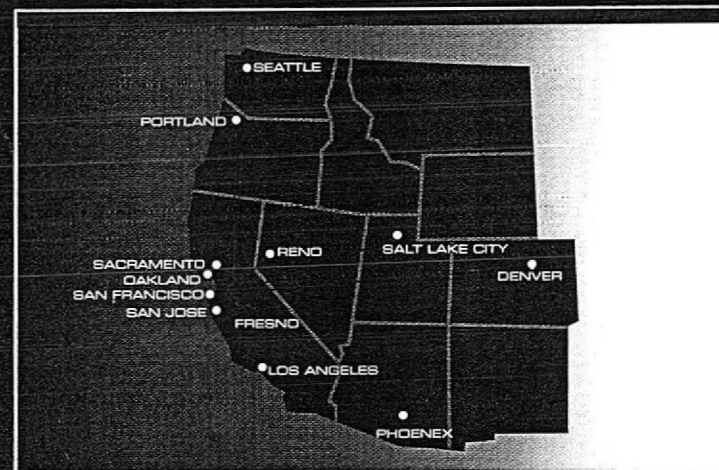
Services

Sewer	City of Stockton
Water	City of Stockton
Storm Drainage	City of Stockton
Gas & Electric	PG&E

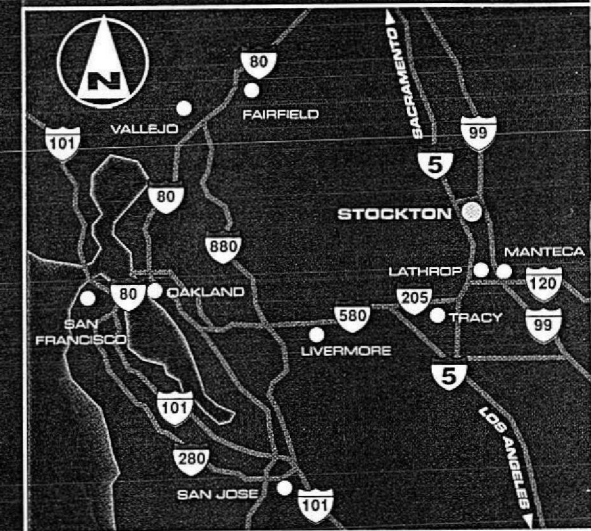
Mileage to Major Markets

NORTH		EAST	
Sacramento	49	Reno	177
Portland	632	Salt Lake City	697
Seattle	804	Denver	1,210

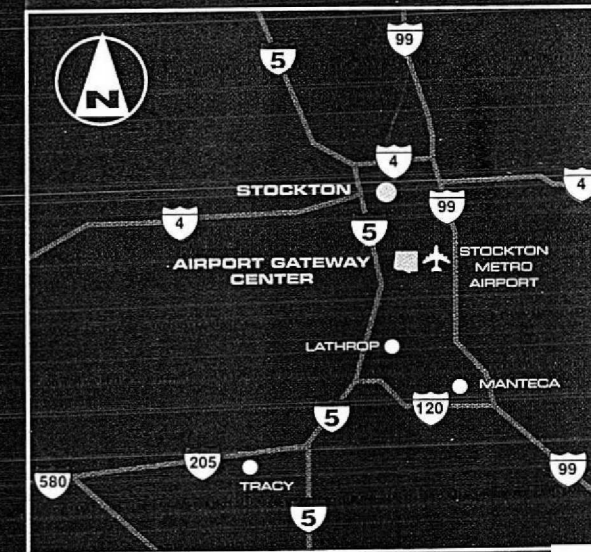
SOUTH		WEST	
Fresno	137	Oakland	73
Los Angeles	339	San Jose	76
Phoenix	709	San Francisco	83



Regional Map



Area Map



The information above has been obtained from sources believed reliable. While owner and broker do not doubt its accuracy, we have not verified and make no guarantee, warranty or representation about it. It is purchaser and or lessee's responsibility to independently confirm its accuracy and completeness. Any projections, opinions, assumptions or estimates used for example only, and do not represent the current or future performance of the property. The value of this transaction to you depends upon tax and other factors which should be evaluated by your tax, financial and legal advisors. You and your advisors should conduct a careful, independent investigation of the property to determine to your satisfaction the suitability of the property for your need.

GREGORY A. O'LEARY
VICE PRESIDENT
INDUSTRIAL PROPERTIES

209 476-2908
209 476-2960 FAX



- a Improvements to the following intersections including but not limited to traffic signals as identified in the Draft Supplemental Environmental Impact Report for this project

Downing Avenue/SB Interstate 5 ramps
Downing Avenue/NB Interstate 5 ramps
French Camp Road/SB Interstate 5 ramps
French Camp Road/NB Interstate 5 ramps
French Camp Road/Arch-Airport-Sperry (future)
Arch-Airport-Sperry/French Camp Road
Sperry Road/McKinley Avenue
French Camp Road/McKinley Avenue
French Camp Road/Airport Way
El Dorado Street/Downing Avenue (future)
Clayton Avenue/El Dorado Street/McKinley Avenue
Arch-Airport-Sperry/Performance Drive
Arch-Airport-Sperry/Alternate Access
Airport Way/Ralph Avenue
Airport Way/Industrial Drive
Airport Way/Sperry-Arch-Airport
Airport Way/C E Dixon Street
Arch-Airport-Sperry/SR 99 ramps
Arch-Airport Road/West SR 99 Frontage Road
Arch-Airport Road/East SR 99 Frontage Road
West SR 99 Frontage Road/SB SR 99 ramps
East SR 99 Frontage Road/NB SR 99 ramps

- b Widening of the following roadways

Airport Way north of C E Dixon Street to six lanes
El Dorado Street between Downing Avenue and Clayton Avenue to six lanes
El Dorado Street between Clayton Avenue to French Camp Road to four lanes
Arch-Sperry Road between Interstate 5 and State Route 99 to eight lanes

- 19 The owners, developers and/or successors-in-interest shall participate equitably in any area of benefit assessment district or other financing mechanism formed to design and/or construct the Arch-Sperry Specific Road Plan Corridor between Interstate 5 and Austin Road including but not limited to, grade separated railroad crossings waterway crossings traffic signals, realignment of French Camp Road and realignment of "old" Sperry Road

- 20 The owners developers and/or successors-in-interest shall pay a proportionate share based on traffic loadings of the cost of the Project Study Reports (PSRs) and necessary improvements described in the PSRs to the Arch Road/State Route 99 urban interchange the French Camp Road/Interstate 5 interchange and the State Route 99 and Interstate 5 mainline systems to provide levels-of-service that conform to California Urban Highway Standards
- 21 The owners, developers and/or successors-in-interest shall participate in any future bicycle facilities along the Arch-Sperry Corridor as described in the City's adopted Bicycle Facilities Master Plan
- 22 The owners developers and/or successors-in-interest shall dedicate all necessary rights-of-way and easements along French Camp Slough and comply with all other requirements of the agencies having jurisdiction over French Camp Slough
- 23 The owners developers and/or successors-in-interest shall provide all necessary rights-of-way and easements along the San Joaquin Valley Electric and Union Pacific railroads and comply with all requirements of the above railroad agencies as identified through the Environmental Impact Report and tentative map approval process
- 24 The owners developers and/or successors-in-interest shall obtain and dedicate easements for the outfall storm drain discharge lines to the existing slough
- 25 The storm drain detention basins shall be maintained privately by the property owners The owners developers and/or successors-in-interest shall prepare a maintenance agreement shall submit said agreement to the City for its approval and shall record such agreement If the private maintenance group defaults the City shall form a mandatory maintenance district to assume the responsibility for the maintenance of the detention basins
- 26 The location and design of the detention basins shall be approved by the Stockton Metropolitan Airport
- 27 The owners developers and/or successors-in-interest shall construct storm drain basins in accordance with the approved Interim Storm Drainage Plans Any subsequent Master Drainage Plans shall be approved by the City Engineer

Storm water shall be discharged to a tributary of the French Camp Slough located to the north of the project site. Discharge pumps shall be controlled by a metering device in the said tributary of the French Camp Slough so as to avoid discharges during peak flows in the slough.

28 The owners, developers and/or successors-in-interest shall obtain all applicable local, State and Federal permits for discharge of storm drainage into said tributary of the French Camp Slough.

Note Any of the above improvements specifically included in the identified improvements and needs assessment used to determine the Public Facilities Fees may be subject to reimbursement from the Public Facilities Fee as allowed by the City Council-Adopted Guidelines. Further, the City upon request of the developer, will attempt to form an Area of Benefit or other financial mechanism to recover the costs of improvements not included in the Public Facilities Fees which benefit other properties outside the limits of this project.

CITY OF STOCKTON
NOTICE OF DETERMINATION

TO County Clerk
San Joaquin County

NOTICE OF DETERMINATION
CITY OF STOCKTON
FROM

Lead Agency
City of Stockton
E/b Community Development Department
Planning Division
425 North El Dorado Street
Stockton CA 95202 1997
Contact Person Associate Planner David Stagnaro
Phone (209) 937 8266

Office of Planning and Research
1400 10th Street
Sacramento CA 95814

APR 27 AM 10

SAN JOAQUIN COUNTY
Patricia Paulsen
DEPUTY

SUBJECT NOTICE OF DETERMINATION PURSUANT TO PUBLIC RESOURCES CODE SECTION 21152 AND CAL. CODE OF REGULATIONS, TITLE 14, SECTIONS 15075, 15094, AND/OR 15096(I)

Project Title Airport Gateway Center Project
Initial Study File No IS8 97 EIR File No Supplemental EIR/Initial Study (SEIR3-82/IS8-97)
Discretionary Applications File Nos TM3 97 State Clearinghouse No 98022006
Project Applicant Reno West Investors, et al
Project Description/Location Tentative map to subdivide 190.87 ± acres into twenty five parcels varying in size from 1.75 to 29 acres located south of Sperry Road, west of Airport Way, north of French Camp Slough and east of McKinley Avenue

Determinations This is to advise that the City of Stockton approved the above described project on April 23, 1998 and has made the following determinations regarding the project

- 1 The project (will) (will not) have a significant effect on the environment
- 2 An environmental impact report was prepared & certified for this project pursuant to the provisions of CEQA
- 3 Mitigation measures (were) (were not) incorporated as part of the approval of the project
- 4 A Mitigation Monitoring and Reporting Program (was) (was not) adopted for this project
- 5 A Statement of Overriding Considerations (was) (was not) adopted for this project
- 6 Findings (were) (were not) made pursuant to the provisions of CEQA
- 7 Pursuant to Cal Code of Regulations Title 14 Sections 753 5(a) or 753 5(c)
 California Department of Fish and Game (CDFG) fees are required as applicable and will be filed with this Notice of Determination (NOD)

This is to certify that the Negative Declaration or Final EIR and any adopted mitigation measures findings statements of overriding consideration and record of project approval may be examined at the above noted Lead Agency address

JOHN CARLSON DIRECTOR
COMMUNITY DEVELOPMENT DEPARTMENT

By *David Stagnaro*
DAVID STAGNARO ASSOCIATE PLANNER

Date April 24, 1998

AFFIDAVIT OF FILING AND POSTING

I declare that on the date stamped above I received and posted this notice as required by California Public Resources Code Section 21152(c) Said notice will remain posted for 30 days from the filing date

Patricia Paulsen
Signature

Dept. Co Clerk
Title

5-30

NOTES

- THIS SUBDIVISION IS A RESUBDIVISION OF PORTIONS OF BLOCKS 1, 2, 3, 4 & 5 OF TERRA MATRE TRACT
- THIS SUBDIVISION CONTAINS 190.87 ACRES
- ELEVATIONS ARE REFERRED TO U.S.G.S DATUM
- PUBLIC IMPROVEMENTS WILL BE INSTALLED IN ACCORDANCE WITH THE CITY OF STOCKTON STANDARDS
- DOMESTIC WATER, STORM DRAIN AND SANITARY SEWER IS BY THE CITY OF STOCKTON WITHIN DEDICATED STREETS AND PUBLIC UTILITY EASEMENTS IN ACCORDANCE WITH AN APPROVED PUBLIC MASTER PLAN. UTILITY MASTER PLANS HAVE BEEN SUBMITTED FOR CITY REVIEW
- PUBLIC UTILITY EASEMENTS SHALL BE DEDICATED AS REQUIRED
- STREET WIDTH IS 75 WITH ADJACENT 15 P.U.E. UNLESS OTHERWISE NOTED
- THIS SUBDIVISION WILL BE DEVELOPED IN PHASES
- STREET NAMES WILL BE FINALIZED IN FINAL MAPS
- ////// DENOTES RESTRICTED ACCESS TO BE DEDICATED
- FLOOD ZONE AS SHOWN ON CURRENT FLOOD INSURANCE RATE MAP
 - ZONE AO FLOOD DEPTH OF 1 TO 3 FEET (USUALLY SHEET FLOW ON SLOPING TERRAIN) AVERAGE DEPTHS DETERMINED FOR AREAS OF ALLUVIAL FAN FLOODING VELOCITIES ALSO DETERMINED
 - ZONE X AREAS OF 100 YEAR FLOOD AREAS OF 100 YEAR FLOOD WITH AVERAGE DEPTHS OF LESS THAN 1 FOOT OR WITH DRAINAGE AREAS LESS THAN 1 SQUARE MILE, AND AREAS PROTECTED BY LEVEES FROM 100 YEAR FLOOD
- SUBDIVISION IMPROVEMENTS ARE SUBJECT TO DEFERRED IMPROVEMENT AGREEMENTS RECORDED BY L.N. 97071908 DATED 07/23/97
- EXISTING CROWN AVENUE, RUNWA ROAD, RAILROAD AVENUE AND BELDING AVENUE SHOWN ON MAP OF TERRA MATRE TRACT TO BE ABANDONED
- PROPOSED OAK TREE REMOVALS SHALL BE SUBJECT TO MITIGATION MEASURES AS DESCRIBED IN SER/158-97
- THE PROPERTY OWNERS, DEVELOPERS AND/OR SUCCESSORS-IN INTEREST SHALL COMPLY WITH THE PROVISIONS OF THE CALIFORNIA GENERAL CONSTRUCTION ACTIVITY STORM WATER PERMIT AND STATE WATER RESOURCES CONTROL BOARD ORDER NUMBER 92 DB DWD. COMPLIANCE IS MANDATORY PER THE CITY OF STOCKTON'S GRADING AND EROSION CONTROL ORDINANCE CHAPTER 13 PART V SECTIONS 13.500 THROUGH 13.513 OF THE STOCKTON MUNICIPAL CODE

LEGEND

EXISTING CONTOUR	---
EXISTING SANITARY SEWER	--- EX 12" SS ---
EXISTING STORM DRAIN	--- EX 18" D ---
EXISTING WATER MAIN	--- EX 16" W ---
PROPOSED SANITARY SEWER	--- 12" SS ---
PROPOSED STORM DRAIN	--- 15" D ---
PROPOSED WATER MAIN	--- 16" W ---

OWNER

RENO WEST INVESTORS
3620 FAIR OAKS BLVD SUITE 150
SACRAMENTO CALIFORNIA 95864
PHONE (916) 485 8900
ATTN: PAUL O'SULLIVAN

SUBDIVIDER

PARATTONI CATLIN VENTURE
3620 FAIR OAKS BLVD SUITE 150
SACRAMENTO CALIFORNIA 95864
PHONE (916) 485 8900
ATTN: PAUL O'SULLIVAN

FILED AS A TENTATIVE MAP THIS 5th DAY OF February 1998
FEE \$ 3,910

John Carlson, Secretary by ass
CITY OF STOCKTON PLANNING COMMISSION

APPROVED BY THE CITY OF STOCKTON PLANNING COMMISSION THIS 23rd DAY OF April 1998.

Robert Blanchard
CHAIRMAN

APPROVED BY THE CITY ENGINEER OF THE CITY OF STOCKTON THIS 17th DAY OF April 1998.

Patricia Simulbaugh
CITY ENGINEER

TENTATIVE MAP

TRACT NO 2778
SUBDIVISIONS OF SAN JOAQUIN COUNTY
AIRPORT GATEWAY CENTER

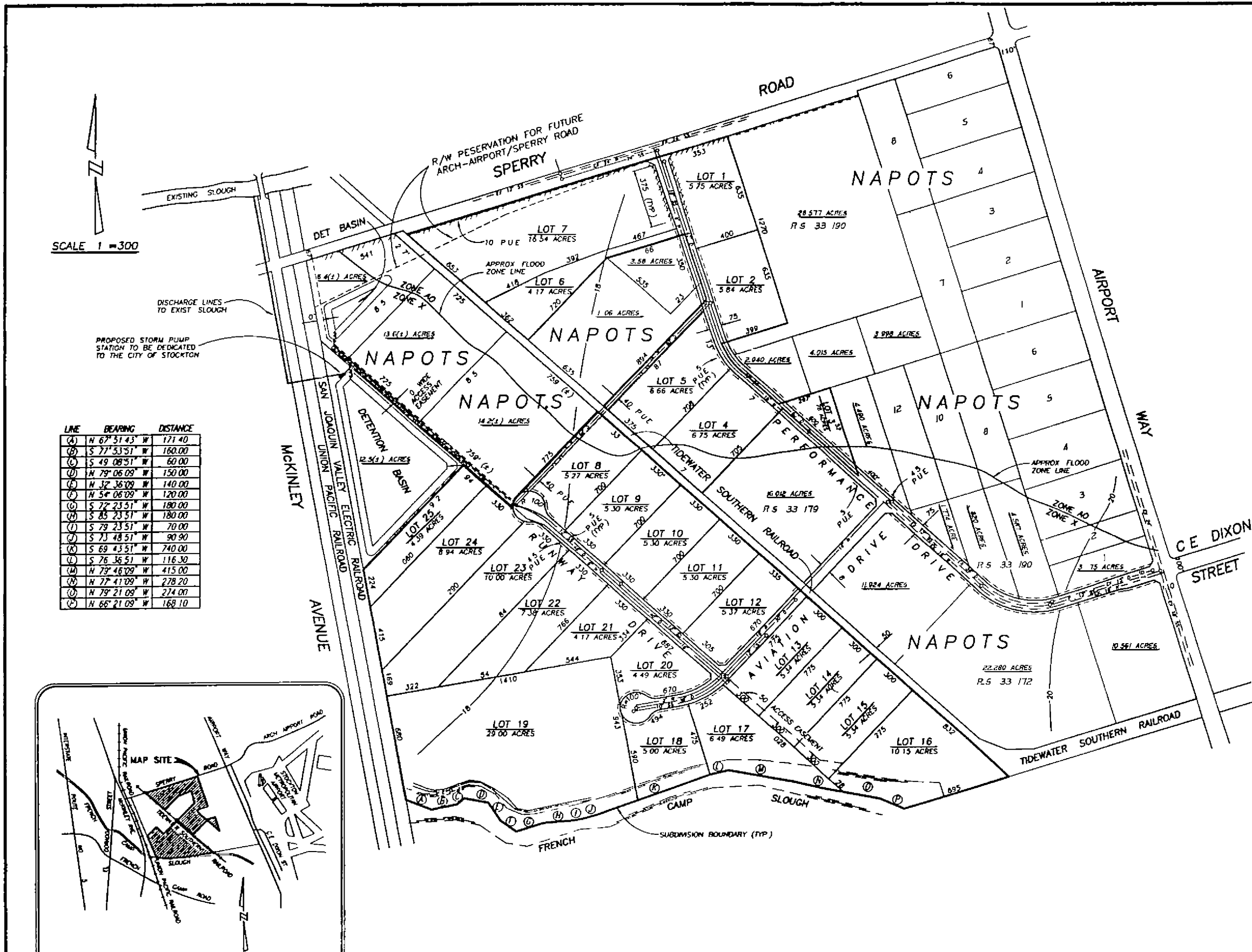
BEING A PORTION OF SECTION 25 C M WEBER GRANT
CITY OF STOCKTON SAN JOAQUIN COUNTY CALIFORNIA

OCTOBER 1997

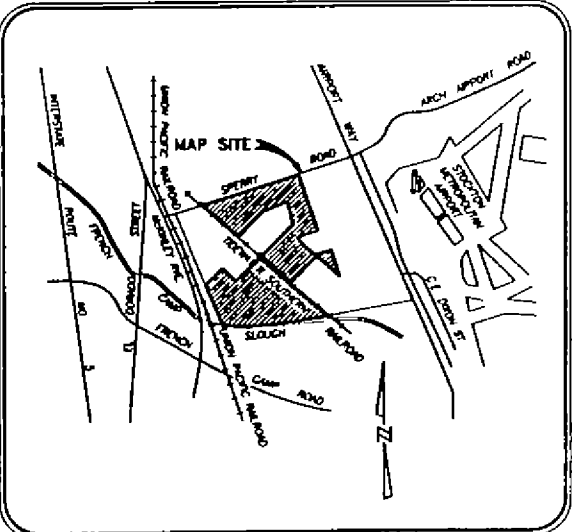
TENTATIVE MAP
AIRPORT GATEWAY CENTER
STOCKTON CALIFORNIA

SHEET
1
OF ONE

APPROVED 7773-97



LINE	BEARING	DISTANCE
(A)	N 67° 51' 43" W	171.40
(B)	S 77° 53' 51" W	160.00
(C)	S 49° 08' 51" W	60.00
(D)	N 79° 06' 09" W	150.00
(E)	N 32° 36' 09" W	140.00
(F)	N 54° 06' 09" W	120.00
(G)	S 72° 23' 51" W	180.00
(H)	S 85° 23' 51" W	180.00
(I)	S 79° 23' 51" W	70.00
(J)	S 73° 48' 51" W	90.90
(K)	S 69° 43' 51" W	740.00
(L)	S 76° 36' 51" W	116.30
(M)	N 79° 46' 09" W	415.00
(N)	N 77° 41' 09" W	278.20
(O)	N 79° 21' 09" W	274.00
(P)	N 66° 21' 09" W	168.10



NO.	REVISIONS	DATE	BY

SEIGFRIED ENGINEERING, Inc
Civil Engineering • Land Surveying • Structural Engineering • Planning
4043 Coronado Ave Stockton, CA 95204 1598 209 943-2021 AX 209 9 2-0214
Job No. 06389

SE+
JOB NO. 06389
DATE OCT 1997
F.B. NO. 860
DRAWN SQ/AL
DESIGN SD
CHECKED LAC

MOORE BIOLOGICAL CONSULTANTS
CIRCULATE & RETURN TO DJS

JAN 12

January 11 2000

Ms Terry Roscoe
California Department of Fish and Game
1701 Nimbus Road Ste A
Rancho Cordova CA 95670

Subject AIRPORT GATEWAY APPROACH TO SWAINSON'S HAWK TAKE AVOIDANCE

Dear Terry

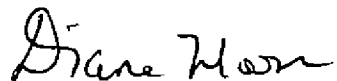
Thank you for taking the time last week to discuss several projects. You will recall that we monitored a Swainson's hawk nest last year during construction of the Dollar Tree warehouse. We anticipate that Swainson's hawks may nest in the same location again this year and construction will likely occur on at least one parcel in the vicinity of the nest tree. Consequently Pannatoni Development Company is interested in entering into an appropriate agreement with California Department of Fish and Game (CDFG) which would provide Pannatoni protection in the event of nest abandonment (i.e. "take") due to construction. You recommended that Pannatoni Development simply participate in the San Joaquin County Multi Species Habitat Conservation Plan (HCP) for the currently undeveloped parcels as the HCP is very near being fully adopted and this is one of the easiest and best ways to implement mitigation.

Following our discussion I spoke with Amy Augustine about the mechanics of participation in the HCP prior to it being fully adopted. She indicated that since the HCP is not fully adopted the Council of Governments (COG) is accepting contributions for terrestrial habitats at the "full rate" (\$2,600+/ per acre instead of \$1,500 per acre for row crop land). I should forward the project description information to Amy she will pass it on to the COG and the COG will generate the appropriate forms to execute the

ation agreement. When the HCP is formally adopted, the COG will refund the
rence between the full rate and the long term rate for participants. Amy also
nfirmated that Pannatoni will be responsible for implementing the standard Take
Avoidance measures for Swainson's hawks as described in the HCP (i.e. do not cut
down the nest tree).

Thanks again for your time and assistance on this project. We are currently working on a
map which shows which parcels are not yet developed and will provide this to you within
the next few weeks. Please call me at (209) 365-6828 with any questions.

Sincerely,



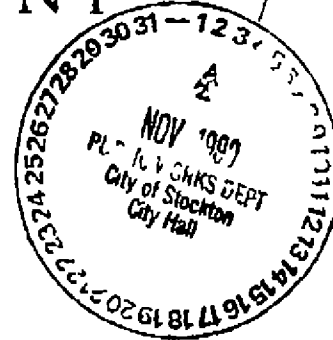
Diane S. Moore, M.S.
Principal Biologist

- c Mr. Dan Gifford (CDFG)
- Ms. Laura Mahoney (Pannatoni Development Company)
- Mr. Mike Hakeem, Esq. (Hakeem, Ellis, Simonelli & Marengo)
- Mr. Charlie Simpson (Insite Environmental)
- Mr. Mike Niblock (City of Stockton)

Airport Gateway Center



PANATTONI



October 29, 1999

Mr Paul Sensibaugh PE
Deputy Public Works Director
Public Works Department
City Hall
425 N El Dorado St
Stockton, CA 95202 1997

10 - 29 1999

Re Master Provisions and Timetable Exhibit D:
Airport Gateway Center

Mr Sensibaugh,

As you know, we are currently under construction on a significant amount of infrastructure surrounding the Airport Gateway Center. The completion date(s) for the various signals and roadwork were called out in Exhibit D of the Master Provisions and Timetable for both Dollar Tree and Cal Sheets. Pursuant to a discussion with the City of Stockton on 10/28/99, we are requesting your concurrence for updating the completion date(s) for the infrastructure work.

All of the following items are substantially complete. Although the Pump Station is not listed on Exhibit D, please note that the delivery of the pumps will determine the exact date of completion and ultimately the dedication to the City. The pumps have been ordered and the delivery date has been set for mid-December.

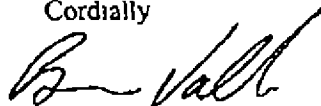
	Completion Date(s)
Sperry Road/Performance Dr Left Turn Lane	12/1/99
Sperry Road/Performance Dr Interim Improvements	12/1/99
Sperry Road/Performance Dr Traffic Signals	12/1/99
Airport Way/Performance Dr Interim Improvements	11/22/99
Airport Way/Performance Dr Interim Traffic Signals	11/22/99
*Sperry Road Improvements from Airport Way to Performance Dr	12/1/99

baugh

plans for the signal at Sperry and Airport have not been signed off by the City of Stockton Comments are being addressed for re submittal to the City

You are in agreement with the above please sign below where indicated

Cordially




Brian Vallis
Project Manager



Laura Mahoney
Project Manager

Concurred by

CITY OF STOCKTON


Paul Sensibaugh

10/29/99
Date

FILE - Planning



CITY OF STOCKTON

XAN/EE/Dis/file



MAN

OFFICE OF THE CITY MANAGER
CITY HALL
425 N EL DOBO-DO STREET
STOCKTON, CA 95202 367
209, 937-8213
FAX (209) 937-748

July 23 1999

Carl Panattoni
Panattoni Development Corporation
9806 Old Winery Place Suite One
Sacramento CA 95827

AIRPORT GATEWAY BUSINESS PARK

I've learned recently that you are relinquishing your role as a managing partner of the Airport Gateway Business Park here in Stockton. First of all, I would like to compliment you and your staff for doing an outstanding job of making this very difficult project a reality and to mention that the City team has enjoyed working with your staff. Laura Mahoney and Brian Valice have been especially valuable as was Dennis Raymond when he was associated with your firm.

We have just learned that Catlin Development Company will be the new managing partner of the development and I would like to make this transition as smooth as possible for all parties involved. As you know, we have held bi-weekly meetings for at least two years to discuss the status of the project, construction schedules, plan checking, etc. and to ensure that infrastructure permits were issued on time in order to serve your clients. To help the City in this time of transition, I am requesting that you assign someone from your staff to attend the next scheduled meeting with the new development team, Catlin Development. This will assist all of us so that we will understand the intricacies of the project, the schedules, the requirements that are remaining, and the concerns of any and all parties. If you would be so kind as to allow your representatives to attend this meeting, it would be greatly appreciated by our staff. If you have any questions or concerns regarding this matter, please call me at (209) 937-8530. I look forward to your answer.

DWANE MILNES
CITY MANAGER

ROBERT A SIVELL
ENTERPRISE ZONE MANAGER

RAS ja
CC Mike Hakeem
ODMA\GRPWISE\ICOS CM CM_Library\807.1

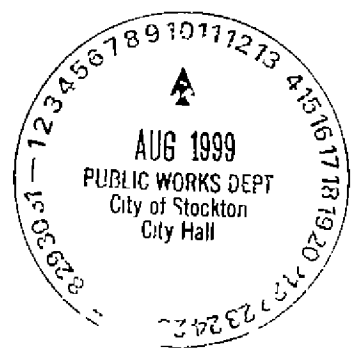
Plan 19
info/files

~~File~~

IMB-117?

 PANATTONI

RECEIVED
AUG 11 1999



CITY OF STOCKTON
COMMUNITY DEVELOPMENT DEPT

August 4 1999

Via Facsimile Transmission

AUG 11 1999

Mr Paul Sensibaugh
Deputy Public Works Director
CITY OF STOCKTON
425 North El Dorado Street
Stockton CA 95202

RE $\text{\$}$ Stockton Airport Gateway

Dear Paul

As you know, we are in the process of reviewing the allocation of properties among various partnerships. This process will continue to unfold on its own separate timetable. In the meantime, Laura Mahoney and Brian Vallis will continue to supervise the completion of the infrastructure improvements. This commitment will insure continuity for this project.

Sincerely

Reno West Investors

Carl D Panattoni

- cc Bob Sivell
- Benjamin S Catlin
- Paul O Sullivan
- Jay Heckenlively
- Marty Boersma
- Michael Hakeem
- Lex Corrales

bcc John Carlson

Sw

Planning
FILE

cc file



RECEIVED

AUG 10 1999

CITY OF STOCKTON
COMMUNICATIONS CENTER

August 4 1999

Via Facsimile Transmission

Mr Paul Sensibaugh
Deputy Public Works Director
CITY OF STOCKTON
425 North El Dorado Street
Stockton CA 95202

RE Stockton Airport Gateway

Dear Paul

As you know, we are in the process of reviewing the allocation of properties among various partnerships. This process will continue to unfold on its own separate timetable. In the meantime, Laura Mahoney and Brian Vallis will continue to supervise the completion of the infrastructure improvements. This commitment will insure continuity for this project.

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Reno West Investors

Carl D Panattoni

- cc Bob Sivell
- Benjamin S Catlin
- Paul O'Sullivan
- Jay Heckenlively
- Marty Boersma
- Michael Hakeem
- Lex Corrales



COMMUNITY DEVELOPMENT DEPARTMENT
CITY HALL
425 N. EL DORADO STREET
STOCKTON, CA 95202 1997

937-8266

Dave - do this
related to a
TM?

THE TM FOR THE
ENTIRE AREA = TM 3-47

1-12-1997
CATEWAY
E

July 16 1999

Panattoni Development Company
Attn: Laura Mahoney
9806 Old Winery Place Suite One
Sacramento, CA 95827

LANDSCAPING REQUIREMENT FOR DOPACO PROPERTY LOCATED AT 1110 PERFORMANCE DRIVE, STOCKTON, CALIFORNIA

This letter is to advise you of the landscaping requirements for your property located at the above-noted address. Section 16 059 9 3 of the Stockton Municipal Code states that the mandatory 20-foot front yard setback area be maintained with landscaping as required by the Community Development Director. This letter is being sent at this time so that any future owners, developers and/or successors-in-interest can be advised of the noted landscaping requirements. Please submit to this office within 30 days from the date of this letter completed landscaping plans for approval.

For your information SMC Section 16-059 9 is enclosed. If you have any questions regarding this matter please contact Associate Planner David Stagnaro at (209) 937-8266.

John Carlson

JOHN CARLSON DIRECTOR
COMMUNITY DEVELOPMENT DEPARTMENT

SM sl

Enclosure

G:\CDD\priv\Planning\LETTERS\LETSLIPANATTONI.doc



SHEPPARD MULLIN RICHTER & HAMPTON LLP

Mr John Carlson
November 12, 1999
Page 2

The current owner of the Parcel, however, has asked DPI to agree to a use covenant to run with the land that would restrict the uses of the Parcel to those permitted under the M-1, Light Industrial ("M-1") designation of the Code. It is my understanding that the Warehouse would also be a permitted use under the M-1 designation (although a use permit would be required if it actually were in an M-1 zone). However, the existence of a private covenant of that nature on the Parcel would not change the City's administration of the Code as it relates to the Parcel. Consequently, the City would continue to treat the Parcel pursuant to its E-P zoning and, therefore, no use permit would be required for the Warehouse.

If you concur with these interpretations, please complete the section below and return this letter to me, if possible by facsimile transmission to (415) 434-3947 with a copy by first class mail. If you need more information in order to respond, please contact me at (415) 774-2974.

Thank you very much for your assistance.

Yours sincerely,



Jim R. Karpak

for SHEPPARD, MULLIN, RICHTER & HAMPTON LLP

I concur that the City of Stockton would permit the Warehouse on the Parcel without a use permit.

DD
Initials/Signature
DAVID STAGNARD
Printed Name

11-19, 1999
Date
ASSOC PLANNER
Title

Enclosure

RE/DJS No 1450 P 4 cc Planning--handle DKS

SHEPPARD MULLIN RICHTER & HAMPTON LLP

LIMITED LIABILITY PARTNERSHIP FORM NO. 07/88 CAL CORP. NON

ATTORNEYS AT LAW

SEVENTEENTH FLOOR

FOUR EMBARCADERO CENTER

SAN FRANCISCO CALIFORNIA 94111-4106

TELEPHONE (415) 434-9100

FACSIMILE (415) 434-3847

RECEIVED

NOV 17 1999

CITY OF STOCKTON
COMMUNITY DEVELOPMENT DEPT
OUR FILE NUMBER

100-04292

WRITERS DIRECT LINE

(415) 774 2984

jkarpniak@smrh.com

November 12, 1999

FAXED
11/19

VIA FACSIMILE AND FIRST CLASS MAIL

Mr John Carlson
Director
Community Development Division
City of Stockton
425 N El Dorado Street
Stockton, CA 95202

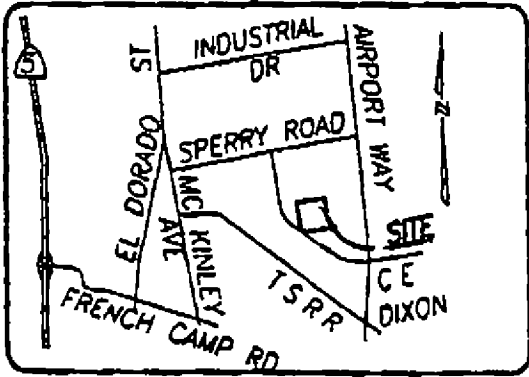
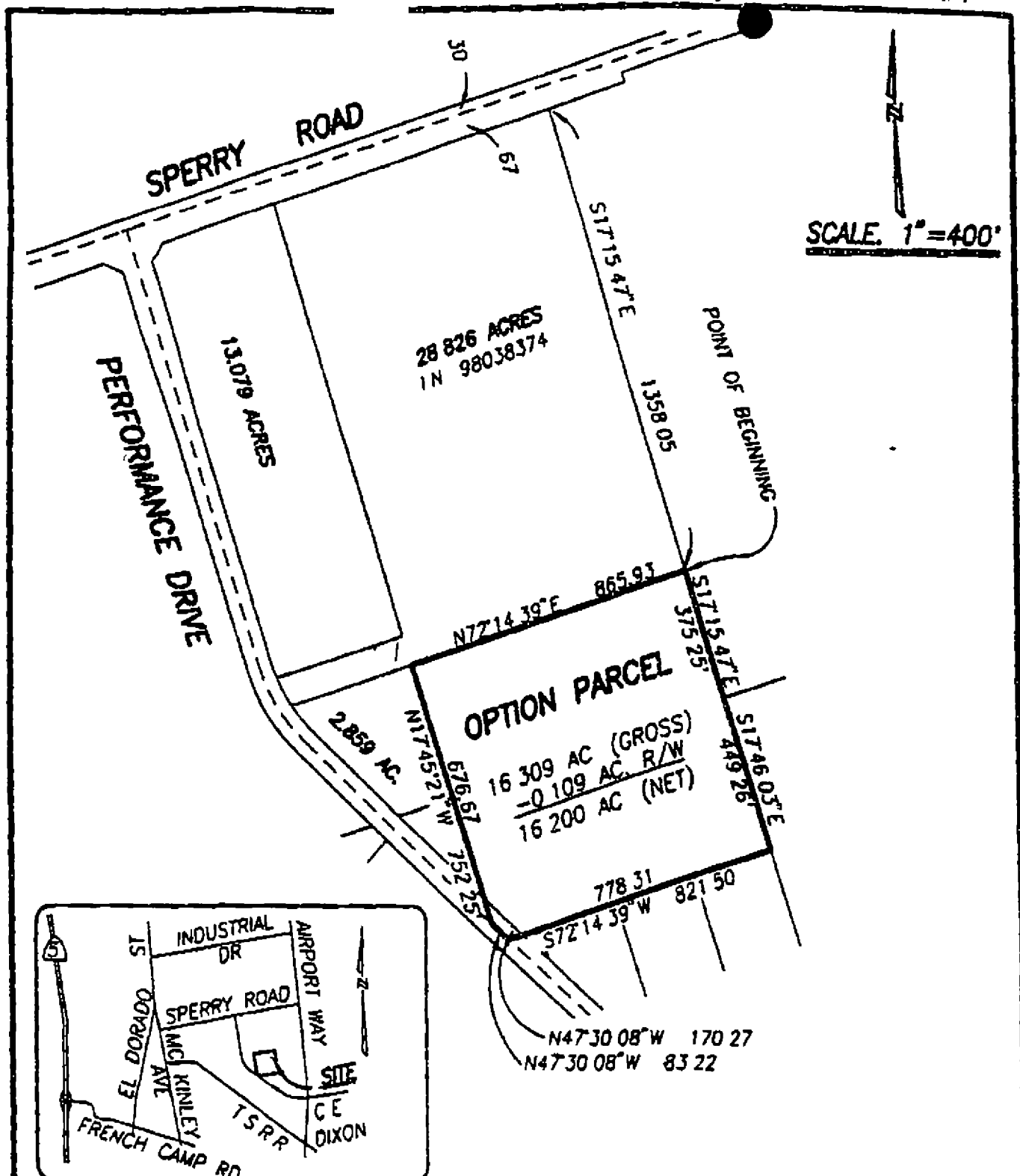
Re Request for Confirmation of Proposed Use
With Zoning Designation

Dear Mr Carlson

Sheppard, Mullin, Richter & Hampton LLP is serving as special California counsel for Distribution Properties, Inc , an Ohio corporation ("DPI") which is interested in purchasing certain property in Stockton and wishes to understand its rights to develop the property under the Planning and Zoning Code of the City of Stockton ("Code") Dan Stagnero of your staff suggested that I write to you for written confirmation that the proposed use described below is consistent with the Code

DPI holds the rights under an option agreement to purchase an approximately 16 acre parcel ("Parcel") in the Airport Gateway Center located on Performance Drive between Sperry Road and Airport Way (see attached drawing) The Parcel is currently zoned E-P, Enterprise Performance District ("E-P") DPI wishes to develop the Parcel as a warehouse/distribution facility ("Warehouse") generally similar to the warehouse on the 28 acre property immediately to the north of the Parcel It is my understanding that the Warehouse would be permitted as of right, with no use permit, provided that it meets the E-P Property Development Standards of the Code

SF-FILET\10061174240.1



VICINITY MAP
NOT TO SCALE

LEVER BROS. OPTION PARCEL
STOCKTON, CALIFORNIA

SIEGFRIED ENGINEERING, Inc.



Civil Engineering • Land Surveying • Structural Engineering • Planning
4043 Coronado Ave • Stockton CA 95204-2396 • 209 943-2021 • FAX 209 942-0214

SUBMITTED BY _____ SEI JOB NO. 96389 COMPUTER FILE NO. _____

96389

86/01/9 JMW A30211

SHEPPARD MULLIN, RICHTER & HAMPTON

cc *Planning--handle*
11/12/99

A LIMITED LIABILITY PARTNERSHIP INCLUDING PROFESSIONAL CORPORATIONS

ATTORNEYS AT LAW

FOUR EMBARCADERO CENTER, SUITE 1700

SAN FRANCISCO CALIFORNIA 94111

TELEPHONE (415) 434-9100

FACSIMILE (415) 434-3947

TELECOPIER COVER LETTER

100-04292

San Francisco Office

TELECOPY NUMBER

(415) 434 3947

TARGETED TIME

ATTORNEY/SECRETARY EXT

184/253

**** THIS TELECOPY TRANSMISSION WILL NOT BE MAILED ****

DATE November 12 1999

TO John Carlson, Director

FAX NO (209) 937 8893

FIRM Community Development Division,
City of Stockton

CONF NO (209) 937-8266

FROM Jim R. Karpiak

RECEIPT CONFIRMATION VIA PHONE REQUESTED YES X NO

FAX RECEIPT CONFIRMED BY _____

TOTAL NUMBER OF PAGES (INCLUDING THIS PAGE) 4

IF ALL PAGES NOT RECEIVED, PLEASE CALL (415) 434-9100

NAME OF SENDER _____ TIME STARTED _____

MESSAGE

NOTE: THIS MESSAGE IS INTENDED ONLY FOR THE USE OF THE INDIVIDUAL OR ENTITY TO WHICH IT IS ADDRESSED, AND MAY CONTAIN INFORMATION THAT IS PRIVILEGED, CONFIDENTIAL AND EXEMPT FROM DISCLOSURE UNDER APPLICABLE LAW. IF THE READER OF THIS MESSAGE IS NOT THE INTENDED RECIPIENT OR THE EMPLOYEE OR AGENT RESPONSIBLE FOR DELIVERING THE MESSAGE TO THE INTENDED RECIPIENT YOU ARE HEREBY NOTIFIED THAT ANY DISSEMINATION, DISTRIBUTION OR COPYING OF THIS COMMUNICATION IS STRICTLY PROHIBITED. IF YOU HAVE RECEIVED THIS COMMUNICATION IN ERROR PLEASE NOTIFY US IMMEDIATELY BY TELEPHONE AND RETURN THE ORIGINAL MESSAGE TO US AT THE ABOVE ADDRESS VIA THE U.S. POSTAL SERVICE. THANK YOU

Plans re

RECEIVED
5 1999

SHEPPARD MULLIN RICHTER & HAMPTON LLP

ATTORNEYS AT LAW
SEVENTEENTH FLOOR
FOUR EMBARCADERO CENTER
SAN FRANCISCO CALIFORNIA 94111-4106

CITY OF STOCKTON
COMMUNITY DEVELOPMENT DEPT

WRITER'S DIRECT LINE
(415) 774 2984
jkarpiak@smrh.com

TELEPHONE (415) 434 9100
FACSIMILE (415) 434-3947

OUR FILE NUMBER
100 04292

November 12 1999

VIA FACSIMILE AND FIRST CLASS MAIL

Mr John Carlson
Director
Community Development Division
City of Stockton
425 N El Dorado Street
Stockton, CA 95202

Re Request for Confirmation of Proposed Use
With Zoning Designation

Dear Mr Carlson

Sheppard, Mullin, Richter & Hampton LLP is serving as special California counsel for Distribution Properties, Inc, an Ohio corporation ("DPI") which is interested in purchasing certain property in Stockton and wishes to understand its rights to develop the property under the Planning and Zoning Code of the City of Stockton ("Code") Dan Stagnero of your staff suggested that I write to you for written confirmation that the proposed use described below is consistent with the Code

DPI holds the rights under an option agreement to purchase an approximately 16 acre parcel ("Parcel") in the Airport Gateway Center located on Performance Drive between Sperry Road and Airport Way (see attached drawing) The Parcel is currently zoned E-P, Enterprise Performance District ("E-P") DPI wishes to develop the Parcel as a warehouse/distribution facility ("Warehouse") generally similar to the warehouse on the 28 acre property immediately to the north of the Parcel It is my understanding that the Warehouse would be permitted as of right, with no use permit, provided that it meets the E-P Property Development Standards of the Code

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Mr John Carlson
November 12, 1999
Page 2

The current owner of the Parcel, however, has asked DPI to agree to a use covenant to run with the land that would restrict the uses of the Parcel to those permitted under the M-1, Light Industrial ("M-1") designation of the Code. It is my understanding that the Warehouse would also be a permitted use under the M-1 designation (although a use permit would be required if it actually were in an M-1 zone). However, the existence of a private covenant of that nature on the Parcel would not change the City's administration of the Code as it relates to the Parcel. Consequently, the City would continue to treat the Parcel pursuant to its E-P zoning and, therefore, no use permit would be required for the Warehouse.

If you concur with these interpretations, please complete the section below and return this letter to me, if possible by facsimile transmission to (415) 434-3947 with a copy by first class mail. If you need more information in order to respond, please contact me at (415) 774-2974.

Thank you very much for your assistance.

Yours sincerely,



Jim R. Karpak

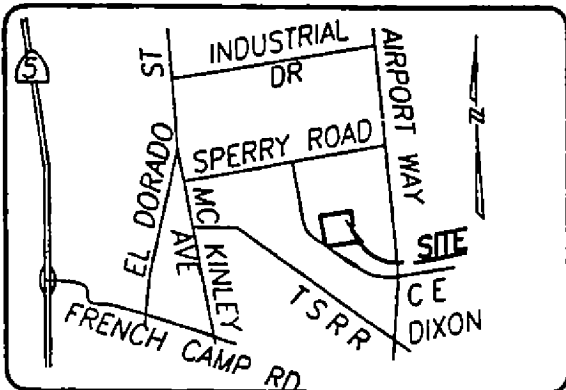
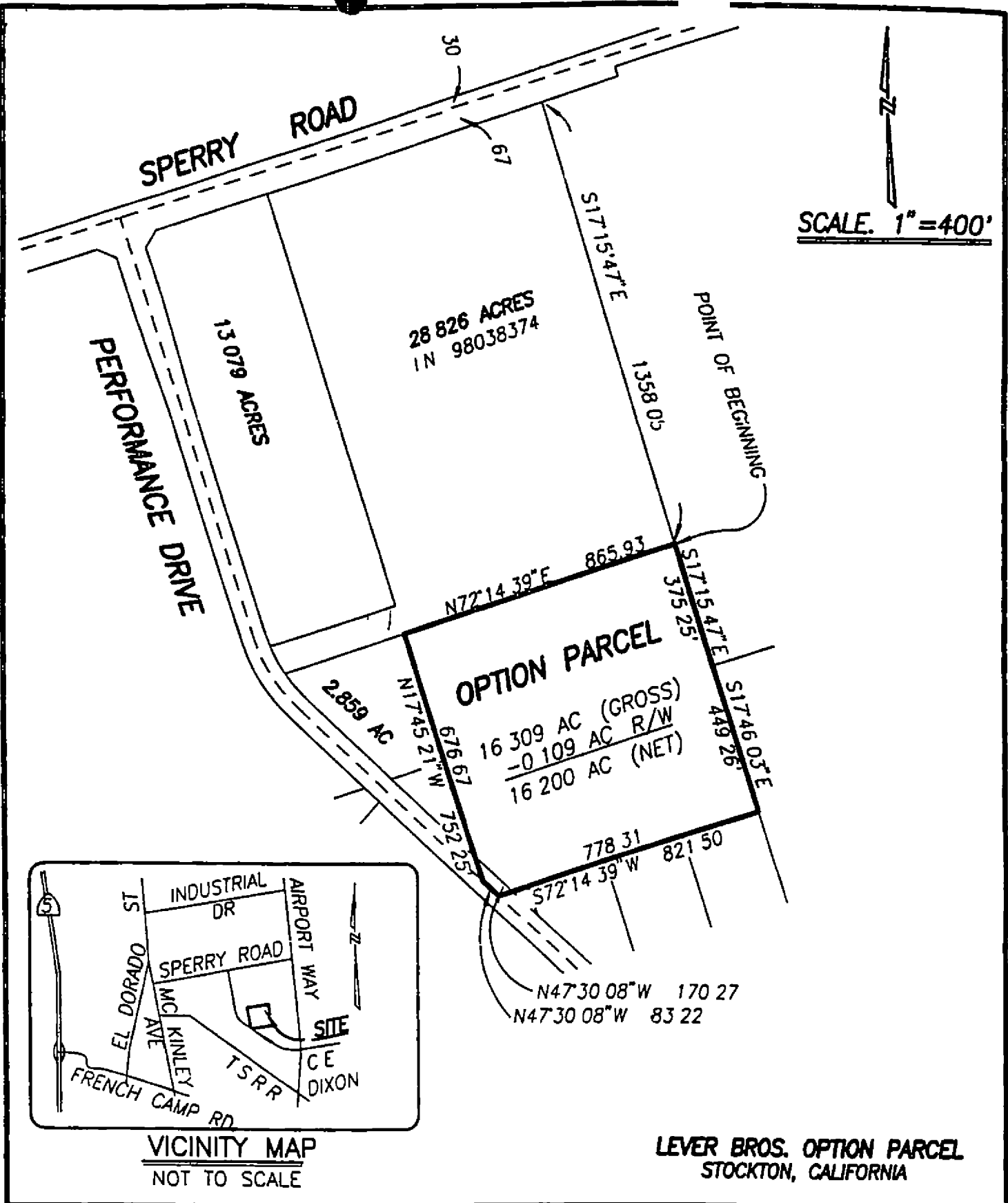
for SHEPPARD, MULLIN, RICHTER & HAMPTON LLP

I concur that the City of Stockton would permit the Warehouse on the Parcel without a use permit.

_____, 1999
Initials/Signature Date

Printed Name Title

Enclosure



LEVER BROS. OPTION PARCEL
STOCKTON, CALIFORNIA

SIEGFRIED ENGINEERING, Inc.



Civil Engineering • Land Surveying • Structural Engineering • Planning
4045 Coronado Ave • Stockton CA 95204-2396 • 209 943-2021 • FAX 209 942-0214

SUBMITTED BY _____ SEI JOB NO 96389 COMPUTER FILE NO _____

96389

LEVEREX DWG 6/19/98

FL
SHEPPARD MULLIN RICHTER & HAMPTON LLP
LIMITED LIABILITY PARTNERSHIP (INCLUSIVE OF PROFESSIONAL CORPORATION)

RECEIVED ✓

NOV 12 1999

ATTORNEYS AT LAW
SEVENTEENTH FLOOR
FOUR EMBARCADERO CENTER
SAN FRANCISCO CALIFORNIA 94111-4108

CITY OF STOCKTON
COMMUNITY DEVELOPMENT DEPT
OUR FILE NUMBER

WRITER'S DIRECT LINE
(415) 774 2984
jkarpniak@smrh.com

TELEPHONE (415) 434-9100
FACSIMILE (415) 434-3047

100-04292

November 12, 1999

VIA FACSIMILE AND FIRST CLASS MAIL

Mr John Carlson
Director
Community Development Division
City of Stockton
425 N El Dorado Street
Stockton, CA 95202

Re Request for Confirmation of Proposed Use
With Zoning Designation

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SF-FRLET10061174240.1

SHEPPARD MULLIN RICHTER & HAMPTON LLP

Mr John Carlson
November 12, 1999
Page 2

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Thank you very much for your assistance.

Yours sincerely,



Jim R. Karpiak

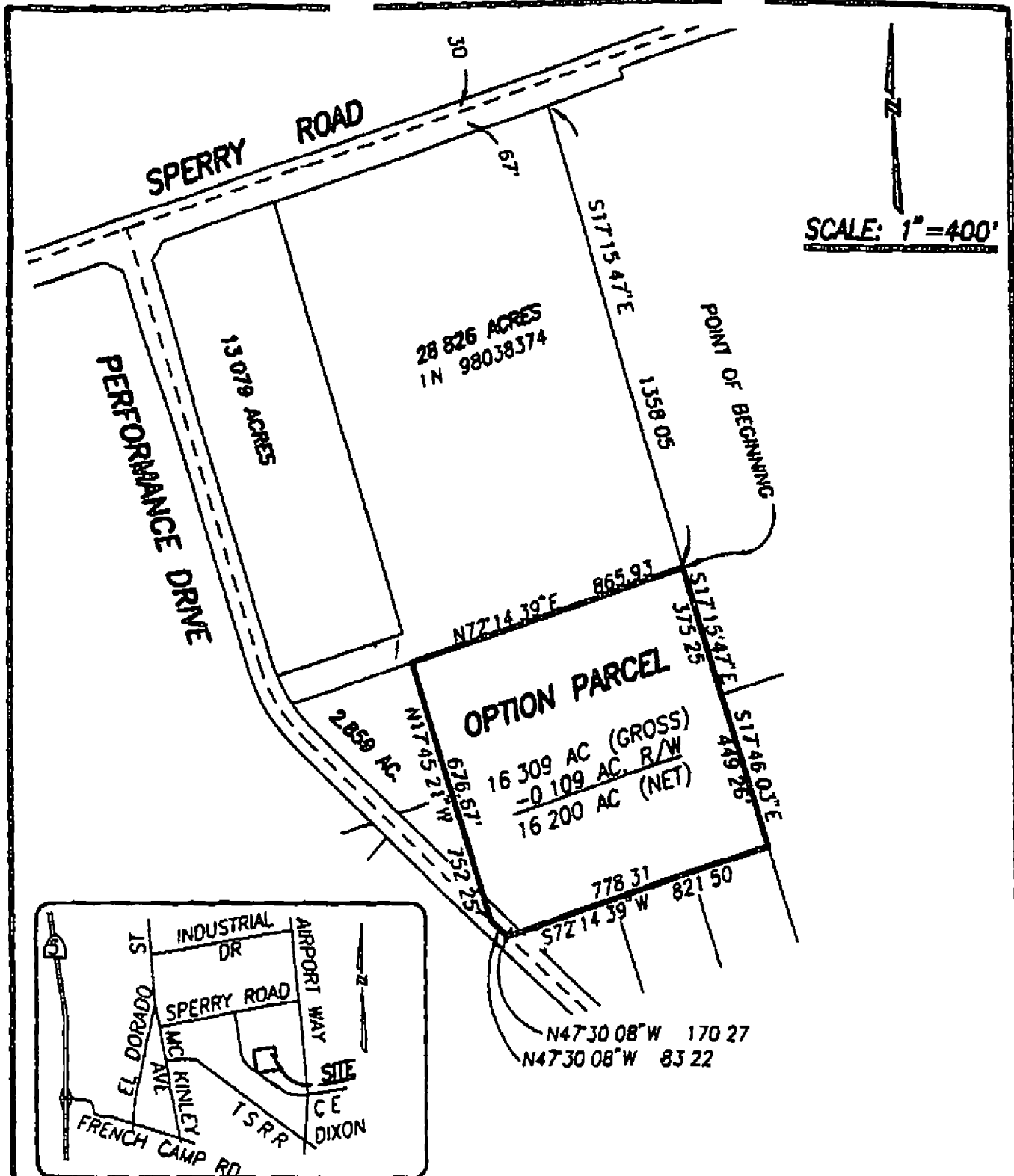
for SHEPPARD, MULLIN, RICHTER & HAMPTON LLP

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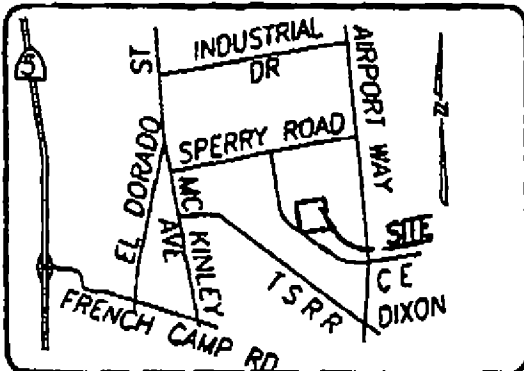
_____, 1999
Initials/Signature Date

Printed Name Title

Enclosure



SCALE: 1" = 400'



VICINITY MAP
NOT TO SCALE

LEVER BROS. OPTION PARCEL
STOCKTON, CALIFORNIA

SIEGFRIED ENGINEERING, Inc.



Civil Engineering • Land Surveying • Structural Engineering • Planning
4045 Coronado Ave. • Stockton, CA 95204-2396 • 209 943-2021 • FAX 209 942-0214

SUBMITTED BY _____ SEI JOB NO. 96389 COMPUTER FILE NO. _____

10/19/99 JMW ASB/ETI

SHEPPARD, MULLIN, RICHTER & HAMPTON, LLP

cc *Planning--handle*
11/12/99

A LIMITED LIABILITY PARTNERSHIP INCLUDING PROFESSIONAL CORPORATIONS

ATTORNEYS AT LAW

FOUR EMBARCADERO CENTER, SUITE 1700

SAN FRANCISCO, CALIFORNIA 94111

TELEPHONE (415) 434-9100

FACSIMILE (415) 434-3947

TELECOPIER COVER LETTER

100-04292

San Francisco Office

TELECOPY NUMBER

(415) 434-3947

TARGETED TIME

ATTORNEY/SECRETARY EXT

184/253

**** THIS TELECOPY TRANSMISSION WILL NOT BE MAILED ****

DATE November 12 1999

TO John Carlson, Director

FAX NO (209) 937-8893

FIRM Community Development Division,
City of Stockton

CONF NO (209) 937-8266

FROM Jim R. Karpak

RECEIPT CONFIRMATION VIA PHONE REQUESTED YES X NO

FAX RECEIPT CONFIRMED BY _____

TOTAL NUMBER OF PAGES (INCLUDING THIS PAGE) 4

IF ALL PAGES NOT RECEIVED, PLEASE CALL (415) 434-9100

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COMMUNITY DEVELOPMENT DEPARTMENT
CITY HALL
425 N. EL DORADO STREET
STOCKTON, CA 95202 1997

937-8266

June 9 1999

Reno West Investors, et al
c/o Laura Mahoney Project Manager
9806 Old Winery Place #1
Sacramento CA 95827

AIRPORT GATEWAY (REVISED TM3-97)

At its regular meeting of May 27 1999 the City Planning Commission approved your request to revise Condition No. 12 and delete Condition Nos. 13 and 14 for the Airport Gateway Center Project for property located south of Sperry Road and west of Airport Way

Approved Condition Changes and Deletions for TM3-97

Revised/added text is in bold and underlined

Revised Condition 3 The owners, developers and successors-in-interest shall comply with the mitigation measures identified in EIR3-82 and SEIR/IS8-97 **as amended by Addendum/Initial Study IS6-99**

Revised Condition 12 **Based on the selected land use alternative of a maximum 39% general light industrial and the remainder being high cube warehousing,** the owners, developers and/or successors-in-interest shall be responsible for 100% of the design and construction costs for the following improvements:

- a Left-turn lanes on the Sperry Road and Arch-Airport Road approaches to Airport Way
- b Widening of all intersection approaches including the at-grade Union Pacific Railroad crossing at Sperry Road/McKinley Avenue to provide auxiliary turn lanes and traffic signals
- c Widening of the southbound McKinley Avenue approach to provide a separate right turn lane; widening of both French Camp Road approaches to provide left turn lanes and traffic signals at the French Camp Road/McKinley Avenue intersection

- d Widening of Sperry Road between Performance Drive and McKinley Avenue to provide a minimum of four (4) through lanes

Note As it relates to the land use mix in Condition No. 12 the trip generation assumptions derived for the land use mix formed the basis for the traffic study conducted for the Airport Gateway DEIR and was used to identify the project impacts, mitigation measures and Conditions of Approval. Should this land use as identified in the condition be exceeded or projected to be exceeded, the developer may retain the services of a qualified traffic engineer as approved by the City, to reevaluate the traffic characteristics and traffic impacts of the land uses in Airport Gateway for comparison to the threshold identified in the original traffic study. Said new information may be used by the City to reevaluate the applicability of mitigation measures and the associated Conditions of Approval.

Deleted Condition 13 The owners, developers and/or successors-in-interest shall be responsible for 100% of the design and construction costs associated with the following improvements and shall provide said improvements prior to the issuance of any certificates of occupancy, if the land use intensity of the 368-acre Airport Gateway Center area exceeds the proposed project mix of 61% high cube warehouse and 39% general light industrial.

- a Widening of McKinley Avenue between Sperry Road and El Dorado Street to provide a minimum of 4 through lanes
- b Secondary access to Sperry Road from this area
- c Traffic signals at the Aviation Drive/Runway Drive intersection. These traffic signals shall be designed and installed when warranted or at the discretion of the Public Works Director.

Deleted Condition 14 The owners, developers and/or successors-in-interest shall also be responsible for 100% of the design and construction costs associated with the following improvements and shall provide said off-site improvements prior to the issuance of any certificates of occupancy if the land use intensity of the 368-acre Airport Gateway Center area exceeds the 61%/39% project mix and the City's standard light industrial category.

- a Completion of Downing Avenue as a minimum two-lane facility between Interstate 5 and El Dorado Street.

- b Construction of a secondary access to Airport Way from the Airport Gateway Center area
- c Widening of Ralph Avenue to provide dual eastbound left-turn lanes at the Airport Way/Ralph Avenue intersection
- d Widening of Sperry Road to provide dual eastbound left turn lanes and widen the Airport Way approaches to provide 6 through lanes at Airport Way/Sperry Road (Under cumulative conditions construction of an urban interchange at this intersection will be warranted)
- e Widening of the McKinley Avenue approach to provide a free westbound right-turn lane at the McKinley Avenue/Sperry Road intersection

If you have any questions please contact Associate Planner David Stagnaro at 937-8266



JOHN CARLSON SECRETARY
CITY PLANNING COMMISSION

JC rw

cc Stan Oshita Siegfried Engineering Inc
4045 Coronado Avenue
Stockton CA 95204 2396

Michael D Hakeem
Hakeem Ellis & Simonelli
2800 W March Lane Suite 200
Stockton CA 95219

Carl D Panattoni Benjamin S Catlin &
GR One Tenants in Common
c/o Benjamin S Catlin
3620 Fair Oaks Boulevard Suite 150
Sacramento CA 95864

Panattoni Catlin Joint Venture VII
A California Limited Partnership
3620 Fair Oaks Boulevard Suite 150
Sacramento CA 95864

Apollo Properties Inc
8395 Jackson Road Suite E
Sacramento CA 95826

Public Works

NOTICE To protest the imposition of any development fee dedication reservation or other exaction imposed on your project you must file written notice with the City Clerk's office within 90 days after approval of the project or imposition of the fees dedications reservations or other exactions stating that the required payment is tendered or will be tendered when due or that any conditions which have been imposed are provided for or satisfied under protest along with a statement of the factual elements of the dispute and the legal theory forming the basis for the protest



COMMUNITY DEVELOPMENT DEPARTMENT
CITY HALL
425 N. EL DORADO STREET
STOCKTON, CA 95202-1997

(209) 937-8266

May 5 1998

Reno West Investors et al
3620 Fair Oaks Boulevard Suite 150
Sacramento CA 95864

AIRPORT GATEWAY CENTER (TM3-97)

At its regular meeting of April 23 1998 the City Planning Commission approved your request to subdivide 190.87 acres into twenty-five parcels varying in size from 1.75 to 29 acres located south of Sperry Road west of Airport Way north of French Camp Slough and east of McKinley Avenue

This approval is subject to the following conditions

- 1 Comply with applicable Federal State County and City codes regulations and adopted standards and pay all applicable fees
- 2 This tentative map is subject to the provisions of the Deferred Improvement Agreement (Recorded on July 23 1997 - Instrument No. 97071908 affecting the subject property) Said agreement shall be binding on the owners developers and/or successors-in-interest
- 3 Based on the significant and/or potentially significant environmental effects identified in the Supplemental Environmental Impact Report/Initial Study for the Airport Gateway Center Project (SEIR3-82/IS8-97) and pursuant to Sections 15091 and 15093 of the State CEQA Guidelines and to the Public Resources Code Section 21081.6 this approval is based on and subject to the adopted findings mitigation measures and mitigation monitoring/reporting provisions as specified in the Findings, Statement of Overriding Considerations, and Mitigation Monitoring Program for the Airport Gateway Center Project
- 4 The owners developers and/or successors-in-interest shall dedicate and improve Sperry Road to ultimately provide a minimum half-section of 67 feet along the project frontage as illustrated in TM29-83. In addition the owners developers and/or successors-in-interest shall dedicate the necessary slope easements for the future Arch-Sperry Corridor grade separated crossings adjacent to this tentative map
- 5 The owners developers and/or successors-in-interest shall dedicate access rights to the City of Stockton at the following locations



- a Aviation Drive north side entire length between Runway Drive and the Tidewater Southern Railroad
 - b Aviation Drive south side entire length between Runway Drive and the Tidewater Southern Railroad, except at the 50-foot wide non-exclusive access easement
 - c Aviation Drive both sides, west of Runway Drive within 400 feet
 - d Runway Drive both sides within 400 feet of Aviation Drive
- 6 Access to Lot 12 shall be provided via a non-exclusive access easement through Lot 11
- 7 Access to Lot 25 shall be provided via a non-exclusive access easement through Lot 24
- 8 Access to all lots shall be limited to one driveway per tentative map parcel. A common or shared access shall be counted as 0.5 access.
- 9 Prior to the recordation of any final maps for this tentative map area, the owners, developers and/or successors-in-interest shall be responsible for 100% of the design and construction costs for approach geometrics, as described below, and traffic signals at the following intersections:
- Sperry Road/Performance Drive-provide dual left-turn lanes for westbound and northbound approaches and a free right-turn lane on the eastbound approach. The northbound approach shall consist of a minimum of dual left-turn lanes, one through lane and one right-turn lane.
- Airport Way/C E Dixon Street-provide a southbound right-turn lane, a lengthened northbound left-turn lane, and dual left-turn lanes, one through lane and a right-turn lane for the eastbound approach.
- 10 Prior to the recordation of any final maps for this tentative map area, the owners, developers and/or successors-in-interest shall be responsible for 100% of the design and construction cost to provide four (4) through lanes on Sperry Road between Airport Way and Performance Drive.
- 11 The owners, developers and/or successors-in-interest shall be responsible for 100% of the design and construction costs for traffic signals at the Aviation Drive/Performance Drive intersection. These traffic signals shall be designed as part of the subdivision improvements and shall be installed when warranted or at the discretion of the Public Works Director.

- 12 Prior to recordation of any final maps for this tentative map area the owners developers and/or successors-in-interest shall be responsible for 100% of the design and construction costs for the following improvements
 - a Left-turn lanes on the Sperry Road and Arch-Airport Road approaches to Airport Way
 - b Widening of all intersection approaches including the at-grade Union Pacific Railroad crossing at Sperry Road/McKinley Avenue to provide auxiliary turn lanes and traffic signals
 - c Widening of the southbound McKinley Avenue approach to provide a separate right turn lane widening of both French Camp Road approaches to provide left turn lanes and traffic signals at the French Camp Road/McKinley Avenue intersection
 - d Widening of Sperry Road between Performance Drive and McKinley Avenue to provide a minimum of four (4) through lanes

- 13 The owners developers and/or successors-in-interest shall be responsible for 100% of the design and construction costs associated with the following improvements and shall provide said improvements prior to the issuance of any certificates of occupancy if the land use intensity of the 368-acre Airport Gateway Center area exceeds the proposed project mix of 61% high cube warehouse and 39% general light industrial
 - a Widening of McKinley Avenue between Sperry Road and El Dorado Street to provide a minimum of 4 through lanes
 - b Secondary access to Sperry Road from this area
 - c Traffic signals at the Aviation Drive/Runway Drive intersection These traffic signals shall be designed and installed when warranted or at the discretion of the Public Works Director

- 14 The owners developers and/or successors-in-interest shall also be responsible for 100% of the design and construction costs associated with the following improvements and shall provide said off site improvements prior to the issuance of any certificates of occupancy if the land use intensity of the 368-acre Airport Gateway Center area exceeds the 61%/39% project mix and the City's standard light industrial category
 - a Completion of Downing Avenue as a minimum two lane facility between Interstate 5 and El Dorado Street
 - b Construction of a secondary access to Airport Way from the Airport Gateway Center area

- c Widening of Ralph Avenue to provide dual eastbound left-turn lanes at the Airport Way/Ralph Avenue intersection
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- e Widening of the McKinley Avenue approach to provide a free westbound right-turn lane at the McKinley Avenue/Sperry Road intersection

Note As it relates to the land use mix in Condition Nos 12 13 and 14 the trip generation assumptions derived for the various land use mixes formed the basis for the traffic study conducted for the Airport Gateway DEIR and were used to identify project impacts mitigation measures and Conditions of Approval Should the land use thresholds identified in these conditions be exceeded or projected to be exceeded the developer may retain the services of a qualified traffic engineer as approved by the City to reevaluate the traffic characteristics and traffic impacts of land uses in Airport Gateway for comparison to the thresholds identified in the original traffic study Said new information may be used by the City to reevaluate the applicability of mitigation measures and associated on Conditions of Approval

- 15 The owners developers and/or successors-in-interest shall obtain all necessary permits and shall construct the Aviation Drive at-grade crossing of the Tidewater Southern Railroad Access to this crossing shall not be permitted until all required safety equipment is operational and the crossing is open to the general public
- 16 The owners developers and/or successors-in-interest shall dedicate and improve a 40-foot wide emergency access only road extending north from the north end of Runway Drive to the east side of the San Joaquin Valley Electric Railroad right-of-way then north to Sperry Road
- 17 The owners developers and/or successors-in-interest shall be granted short term temporary access to Runway Drive from Sperry Road for a period of 18 months from the time of application to the Public Utilities Commission for the Aviation Drive at-grade crossing of Tidewater Southern Railroad The owners, developers and/or successors-in-interest shall be responsible for monitoring the traffic operations at the intersection of this access and Sperry Road Specifically as additional lots are developed the owners, developers and/or successors-in-interest shall provide a traffic analysis to the Public Works Department to determine if the access configuration can adequately accommodate the additional trip generation Said traffic analysis shall identify the improvements needed to maintain the City's minimum level-of-service standard as well as provide for safe access The owners developers and/or successors-in-interest shall be responsible for 100% of these identified improvements

- 18 The owners developers and/or successors-in-interest shall be responsible for their proportionate share based on traffic loadings of the costs for the following improvements to reduce the significance of the cumulative impacts
- a Improvements to the following intersections including but not limited to traffic signals as identified in the Draft Supplemental Environmental Impact Report for this project
- Downing Avenue/SB Interstate 5 ramps
 - Downing Avenue/NB Interstate 5 ramps
 - French Camp Road/SB Interstate 5 ramps
 - French Camp Road/NB Interstate 5 ramps
 - French Camp Road/Arch-Airport-Sperry (future)
 - Arch-Airport-Sperry/French Camp Road
 - Sperry Road/McKinley Avenue
 - French Camp Road/McKinley Avenue
 - French Camp Road/Airport Way
 - El Dorado Street/Downing Avenue (future)
 - Clayton Avenue/El Dorado Street/McKinley Avenue
 - Arch-Airport-Sperry/Performance Drive
 - Arch-Airport-Sperry/Alternate Access
 - Airport Way/Ralph Avenue
 - Airport Way/Industrial Drive
 - Airport Way/Sperry-Arch-Airport
 - Airport Way/C E Dixon Street
 - Arch-Airport-Sperry/SR 99 ramps
 - Arch-Airport Road/West SR 99 Frontage Road
 - Arch-Airport Road/East SR 99 Frontage Road
 - West SR 99 Frontage Road/SB SR 99 ramps
 - East SR 99 Frontage Road/NB SR 99 ramps
- b Widening of the following roadways
- Airport Way north of C E Dixon Street to six lanes
 - El Dorado Street between Downing Avenue and Clayton Avenue to six lanes
 - El Dorado Street between Clayton Avenue to French Camp Road to four lanes
 - Arch-Sperry Road between Interstate 5 and State Route 99 to eight lanes
- 19 The owners developers and/or successors in-interest shall participate equitably in any area of benefit assessment district or other financing mechanism formed to design and/or construct the Arch-Sperry Specific Road Plan Corridor between Interstate 5 and

Austin Road including but not limited to grade separated railroad crossings, waterway crossings traffic signals realignment of French Camp Road and realignment of "old" Sperry Road

- 20 The owners developers and/or successors-in-interest shall pay a proportionate share based on traffic loadings, of the cost of the Project Study Reports (PSRs) and necessary improvements described in the PSRs to the Arch Road/State Route 99 urban interchange, the French Camp Road/Interstate 5 interchange and the State Route 99 and Interstate 5 mainline systems to provide levels-of-service that conform to California Urban Highway Standards
- 21 The owners, developers and/or successors-in-interest shall participate in any future bicycle facilities along the Arch-Sperry Corridor as described in the City's adopted Bicycle Facilities Master Plan
- 22 The owners developers and/or successors-in-interest shall dedicate all necessary rights-of-way and easements along French Camp Slough and comply with all other requirements of the agencies having jurisdiction over French Camp Slough
- 23 The owners developers and/or successors-in-interest shall provide all necessary rights-of-way and easements along the San Joaquin Valley Electric and Union Pacific railroads and comply with all requirements of the above railroad agencies as identified through the Environmental Impact Report and tentative map approval process
- 24 The owners developers and/or successors-in-interest shall obtain and dedicate easements for the outfall storm drain discharge lines to the existing slough
- 25 The storm drain detention basins shall be maintained privately by the property owners. The owners developers and/or successors-in-interest shall prepare a maintenance agreement shall submit said agreement to the City for its approval and shall record such agreement. If the private maintenance group defaults the City shall form a mandatory maintenance district to assume the responsibility for the maintenance of the detention basins
- 26 The location and design of the detention basins shall be approved by the Stockton Metropolitan Airport
- 27 The owners developers and/or successors-in-interest shall construct storm drain basins in accordance with the approved Interim Storm Drainage Plans. Any subsequent Master Drainage Plans shall be approved by the City Engineer. Storm water shall be discharged to a tributary of the French Camp Slough located to the north of the project site. Discharge pumps shall be controlled by a metering device in the said tributary of the French Camp Slough so as to avoid discharges during peak flows in the slough

Reno West Investors, et .

May 5, 1998

Page 7

28 The owners developers and/or successors-in-interest shall obtain all applicable local State and Federal permits for discharge of storm drainage into said tributary of the French Camp Slough

Note Any of the above improvements specifically included in the identified improvements and needs assessment used to determine the Public Facilities Fees may be subject to reimbursement from the Public Facilities Fee as allowed by the City Council-Adopted Guidelines Further, the City upon request of the developer will attempt to form an Area of Benefit or other financial mechanism to recover the costs of improvements not included in the Public Facilities Fees which benefit other properties outside the limits of this project

Enclosed is a copy of the approved tentative map which will expire on April 23 2000 Any request for an extension shall be made in writing filed with this office and accompanied by the appropriate fees prior to expiration of the tentative map and clearly stating the reasons for requesting the extension In granting an extension new conditions may be imposed and existing conditions may be revised

If you have any questions please contact Associate Planner David Stagnaro at 937-8266

John Carlson
JOHN CARLSON SECRETARY
CITY PLANNING COMMISSION

JC rw

Enclosure

cc Stan Oshita w/enclosure
Siegfried Engineering Inc
4045 Coronado Avenue
Stockton CA 95204 2396

Panattoni Catlin Joint Venture VII A
California Limited Partnership
3620 Fair Oaks Boulevard Suite 150
Sacramento CA 95864

Michael D Hakeem
Hakeem Ellis & Simonelli
2800 West March Lane Suite 200
Stockton CA 95219

Apollo Properties Inc
8395 Jackson Road Suite E
Sacramento CA 95826

Carl D Panattoni Benjamin S Catlin and
GR One tenants in common
c/o Benjamin S Catlin
3620 Fair Oaks Boulevard Suite 150
Sacramento CA 95864

Dwp
Panattoni Catlin Venture
3620 Fair Oaks Boulevard Suite 150
Sacramento CA 95864

Public Works w/sepia

NOTICE To protest the imposition of any development fee dedication reservation or other exaction imposed on your project you must file written notice with the City Clerk's office within 90 days after approval of the project or imposition of the fees dedications reservations or other exactions stating that the required payment is tendered or will be tendered when due or that any conditions which have been imposed are provided for or satisfied under protest along with a statement of the factual elements of the dispute and the legal theory forming the basis for the protest



COMMUNITY DEVELOPMENT DEPARTMENT
CITY HALL
425 N. EL DORADO STREET
STOCKTON, CA 95202 1987

937-8266

May 5 1998

Reno West Investors et al
3620 Fair Oaks Boulevard Suite 150
Sacramento CA 95864

AIRPORT GATEWAY CENTER (TM3-97)

The following is prepared as additional information related to the City Planning Commission approval of your tentative subdivision map on April 23 1998

- 1 Show the location of Heritage Oak Trees including trunk diameter
- 2 Show the location of other oak trees which can be depicted in groupings if applicable
- 3 The engineer shall prepare the final map for this tentative map area using CCS-83 datum
- 4 The engineer shall be reminded that direct taps are not permitted in 18-inch sewer lines
- 5 Any on-site wells and septic tanks shall be abandoned and destroyed as required by the San Joaquin County Department of Environmental Health prior to the issuance of building permits
- 6 A minimum of a 10-foot wide public utility easement shall be provided along all street frontages within the subdivision
- 7 A soils report shall be completed prior to issuance of building permits
- 8 Street names shall be subject to approval by the Community Development Department
- 9 Show all right-of-way widths and typical sections of all streets including Sperry Road and Airport Way



Reno West Investors, et a

May 5, 1998

Page 2

- 10 Show the width location and identity of all existing easements
- 11 The Record of Survey number labeled on the map across the 3 820 acre and 4 587 acre lots on the north side of Performance Drive approximately 800 feet west of Airport Way is incorrect
- 12 Some of the utility line sizes are illegible All sanitary sewer water and storm drain lines shall conform with the applicable approved interim plans
- 13 This tentative map was drawn at a scale of 1" = 300' The Stockton Municipal Code requires that tentative maps be drawn at a minimum scale of 1" = 100'
- 14 Any existing overhead utility lines on the site shall be placed underground

If you have any questions please contact Associate Planner David Stagnaro at 937-8266



JOHN CARLSON SECRETARY
CITY PLANNING COMMISSION

JC rw

cc Stan Oshita w/enclosure
Siegfried Engineering Inc
4045 Coronado Avenue
Stockton CA 95204 2396

Michael D Hakeem
Hakeem Ellis & Simonelli
2800 West March Lane Suite 200
Stockton CA 95219

Carl D Panattoni Benjamin S Catlin and
GR One tenants in common
c/o Benjamin S Catlin
3620 Fair Oaks Boulevard Suite 150
Sacramento CA 95864

Panattoni-Catlin Joint Venture VII A
California Limited Partnership
3620 Fair Oaks Boulevard Suite 150
Sacramento CA 95864


Apollo Properties Inc
8395 Jackson Road Suite E
Sacramento CA 95826

Panattoni Catlin Venture
3620 Fair Oaks Boulevard Suite 150
Sacramento CA 95864


Public Works w/sepra
Building Division

NOTICE To protest the imposition of any development fee dedication reservation or other exaction imposed on your project you must file written notice with the City Clerk's office within 90 days after approval of the project or imposition of the fees dedications reservations or other exactions stating that the required payment is tendered or will be tendered when due or that any conditions which have been imposed are provided for or satisfied under protest along with a statement of the factual elements of the dispute and the legal theory forming the basis for the protest

TM 3-97

 PANATTONI

January 13, 1999

 4 1999

To (see distribution list)
CITY OF STOCKTON

Re STOCKTON AIRPORT GATEWAY
CHANGE OF ADDRESS

Please effect the following address change to your computer database and/or Rolodex

All correspondence for either RENO WEST INVESTORS, PANATTONI CATLIN JOINT VENTURE VII OR PANATTONI DEVELOPMENT COMPANY should be addressed as follows

Reno West Investors c/o Laura Mahoney Project Manager 9806 Old Winery Place #1 Sacramento CA 95827	Panattoni Catlin Joint Venture VII c/o Laura Mahoney Project Manager 9806 Old Winery Place #1 Sacramento CA 95827	Panattoni Development Company c/o Laura Mahoney Project Manager 9806 Old Winery Place #1 Sacramento CA 95827
--	---	--

Recently it came to our attention that some departments are sending mail to the old address of 3620 Fair Oaks Blvd Suite 150, Sacramento Your assistance in implementing this change will be greatly appreciated and will ensure a quick response to letters and requests for information

Thank you If you have any questions or I may be of assistance please do not hesitate to call

Cordially

L. Mahoney

Laura Mahoney
Project Manager

DISTRIBUTION LIST

CITY ATTORNEY

Guy D Petzold

ECONOMIC DEVELOPMENT

Bob Sivell

Wanetta Conroy

COMMUNITY DEVELOPMENT

John Carlson

Ed O'Reilly

P Nelson Fox

Sam Mah

Mike Niblock

David Stagnaro

HOUSING & REDEVELOPMENT

Mike Wallace

MUNICIPAL UTILITIES

Ed Formosa

Mark Madison

Glen Birdzell

PARKS & RECREATION

Victor Machado

PUBLIC WORKS

Paul Sensibaugh

Reed Hogan

David Hillberry

Gregg Halladay

Dave Olson

Kathy Tomura

Gregg Meissner

FIRE DEPARTMENT

Ken Uehling

14/11/98 file info by

Em

SIEGFRIED ENGINEERING, Inc.

Civil Engineering ■ Land Surveying ■ Structural Engineering ■ Planning



Robert W. Siegfried
Founder

Wayne M. West
Lex A. Corrales
Stephen R. Thumlert
Anthony J. Lopes

August 4, 1998

Ms. Laura Mahoney
Panattoni - Catlin Venture
3620 Fair Oaks Boulevard
Suite 150
Sacramento, CA 95864

**Subject GATX Lot Line Adjustment
 Airport Gateway Center
 Stockton, California**

Dear Laura

I am writing to inform you of some consequences of the subject lot line adjustment process disclosed March 26th in a meeting with City Engineer Paul Sensibaugh. Also present at the meeting were Sam Ma, Deputy Director of Planning and Sharon Parker of the City Manager's Office.

With the imminent City adoption of the project Environmental Impact Report (EIR) and filing of the Tentative Map (TM) the City Engineer is concerned that the parcels formed by the subject lot line adjustment process are in substantial conformance with the TM. Therefore, to form the GATX site, we must revise the TM to show the one large lot for GATX. The new TM will be routed through the City departments as an administrative memorandum which will not effect the EIR and can be approved by the Planning Commission.

The GATX site will have to remain on the TM as one lot. Catellus will have to sign the TM application and the final map agreeing to the requirements of the Conditions of Approval and the EIR.

For the other four parcels formed by the lot line adjustment process the City Engineer cannot make findings for substantial conformance. The formation of these parcels facilitate the formation of the GATX site. They are intermediate parcels which will be ultimately reconfigured by the filing of the final map to conform to the TM.

Page 2

Ms Laura Mahoney

Subject GATX Lot Line Adjustment, AGC, Stockton, California

There was much discussion regarding the building of the intermediate parcels. At the meetings conclusion my understanding was that once the EIR is adopted, and the TM is in effect, a building permit could not be issued for the any parcels which are not in substantial conformance with the TM until the Planning Commission approved a revised TM with a revised EIR.

Please call if you wish to discuss this matter further

Very truly yours

Siegfried Engineering, Inc


Jim A. Billigmeier P E

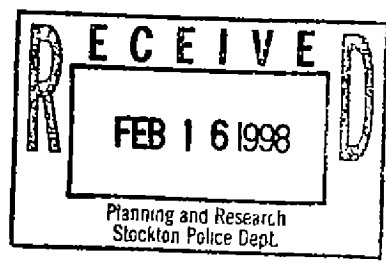
jlm

cc Paul Sensibaugh
Sam Ma
Reed Hogan
Sharon Parker

RC/DJS

Sm

C
 COMMUNITY DEVELOPMENT DEPARTMENT
 PLANNING DIVISION
 CITY PERMIT CENTER
 425 NORTH EL DORADO STREET
 STOCKTON, CALIFORNIA 95202 1997
 (209) 937 8266



February 17 1998

CITY OF STOCKTON

SAN JOAQUIN COUNTY

STATE

___ City Manager
Glen Birdzell w/map

___ Planning
w/map

___ CalTrans
w/map

MAR 2

___ Comm Dev /Planning
Ramirez w/map

___ Public Works
w/map

OTHER AGENCIES

___ Comm Dev./Building
Himes w/sm

___ Public/ Env Health Serv
w/sm

___ P G & E - Stockton
w/map

___ P G & E - Fresno
w/sm

___ Housing & Redev Dept
Pinkerton w/sm

___ Flood Control
w/map

___ Pacific Bell
w/map

___ PW/Eng
Sensibaugh w/map
___ PW/Meissner w/map

___ Sheriff's Dept.
w/sm

___ MediaOne Cable
w/sm

___ PW/O & M
Escobar w/map

SCHOOL DISTRICTS

___ SMART
w/map

___ PW/Solid Waste
Chen w/map

___ Stockton Unifled
w/map

___ U S Post Office
w/map

___ Parks & Rec
Machado w/map

___ Lincoln Unifled
w/map

___ LAFCO
w/sm

___ Fire Department
Gillis w/map

___ Lodi Unifled
w/map

___ COG/ALUC
w/sm

~~Police Department~~
~~Marconi w/map~~

___ Manteca Unifled
w/map

___ Cal Water
w/map

___ Municipal Utilities Dept
Madison w/map

___ Other

___ MIS
Edgecomb w/map

___ File w/map

Enclosed is a tentative map or proposal of PANATTONI CATLIN VENTURE (AIRPORT-GATEWAY CENTER) (TM3 97) which will be considered by the City of Stockton Planning Commission. The Development Review Committee will formulate a staff position and recommendation on this proposal on March 24, 1998. We would appreciate your reviewing this proposal and submitting any comments to this office by March 10, 1998.

Please answer and return the questionnaire on the back of this page as well as the map

If you have any questions regarding this proposal, please contact Associate Planner David Stagnaro at (209) 937-8266

To Community Development Department/Planning Division

From ^{1 2 3} ~~1 937~~ Stockton Police Department/Planning & Research

Subject RESPONSE TO TENTATIVE MAP OR PROPOSAL OF

TM3-97

1 We have reviewed the tentative map or proposal and our comments are as follows

a The map is acceptable xxx

b The map is not acceptable _____ for reasons itemized below

c Other comments

2 Recommendation

3 Person reviewing the map Bob Marconi Phone 937-8651
Bob Marconi, Police Planning Analyst

As it relates to the land use mix in Conditions 12, 13 and 14, the trip generation assumptions, derived for the various land use mixes, formed the basis for the traffic study conducted for the Airport Gateway DEIR and were used to identify project impacts, mitigation measures and Conditions of Approval. Should the land use thresholds identified in these conditions be exceeded or projected to be exceeded, the developer may retain the services of a qualified traffic engineer, as approved by the City, to reevaluate the traffic characteristics and traffic impacts of land uses in Airport Gateway for comparison to the thresholds identified in the original traffic study. Said new information may be used by the City to reevaluate the applicability of mitigation measures and associated Conditions of Approval.

Panattoni Development-Catlin Group

VIA FEDERAL EXPRESS

MEMORANDUM

DATE 4/22/98
TO MICHAEL HAKEEM
COMPANY HAKEEM ELLIS & SIMONELLI
FROM LAURA MAHONEY
RE TENTATIVE SUBDIVISION MAP STATEMENT

Enclosed for your use are (5) "Tentative Subdivision Map Statements" They are as follows

- 1 Reno West Investors, ±180 06 acres,
- 2 Panattoni-Catlin Joint Venture VII ±11 924 acres,
- 3 Apollo Properties Inc ±6 603 acres and
- 4 Carl D Panattoni Benjamin S Catlin, and GR One tenants in common, 16 012 acres

If you require additional documentation please do not hesitate to call

/lam

Enclosures

Y
Date _____

TM No _____

Public Hearing Date _____

Case Planner _____

TENTATIVE SUBDIVISION MAP STATEMENT

1 Name of owner(s) Reno West Investors, a California General Partnership

Address(es) and phone no(s) 3620 Fair Oaks Blvd , Ste 150, Sacramento,
California, 95864 (916) 485-8900

2 Name of subdivider(s) same as above

Address(es) and phone no(s) _____

3 Contact person(s) regarding the map Michael Hakeem, Esq

Address(es) and phone no(s) HAKEEM, ELLIS & SIMONELLI, 2800 W March Lane,
Ste 200, Stockton, CA 95219 (209) 474-2800

4 The attached subdivision map shows proposed right of way, lot pattern, and other information as required by the City Subdivision Ordinance. The undersigned fully understands the provisions of the Subdivision Ordinance and will comply with all the requirements contained therein.

5 Specific Data

a Name of subdivision Airport Gateway Center

b Total acreage in subdivision ±180.06 acres

c General location of subdivision Please see attached description

d Estimated number of lots 25

e Water service provided by City of Stockton

f If any portion of the map is impacted by noise as defined by the noise contour information contained in the Noise Element of the General Plan, Ldn contours in excess of 60dB must be plotted on the map.

g School District Manteca Unified
If the map is within a school district in which conditions of overcrowding have been designated by the City Council, the following must be answered:

(over)

6 In accordance with the provisions of the "School Facilities Dedication Ordinance" as outlined in the Stockton Municipal Code, Chapter 8, Part VII, and in compliance with said ordinance, the undersigned prefers to

_____ a dedicate land for interim school facilities, or

_____ b pay a fee in lieu of dedication as established by the City Council, or

_____ c a combination of both

The Planning Commission may approve the map for residential development without requiring the payment of fees or the dedication of land if the Planning Commission finds that there exists specific overriding fiscal, economic, social or environmental factors which would benefit the community as a whole

7 Remarks (Specify purpose of land division)

Include the following as indicated when application is returned

1 Two (2) copies of completed application form

2 Application fee _____ plus _____ per lot

3 Initial Study and fee _____

4 A L U C fee _____

5 One copy of reverse sepla

6 _____ copies of full size print

7 One copy of 8-1/2" by 11" reduction


8 Other _____


Signature(s)  _____
Benjamin S Catlin

Title Managing General Partner

Date 4/22/98

DISCLOSURE OF CAMPAIGN CONTRIBUTIONS

I Benjamin S Catlin  owner for a
(name)

I Benjamin S Catlin  applicant for a
(name)

(type and number of permit)

on oath declare that

(Please check appropriate statement and complete)

Owner Applicant

 I have not made any campaign contributions in the amount of \$250 or more to any member of the Planning Commission within the past 12 months

 I have made a campaign contribution for (local) (State) (federal) election of \$ _____ to _____ within the last 12 months (name of Commissioner)

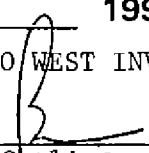
I understand that Government Code Section 84308 requires this disclosure and I make this disclosure in compliance therewith. Further, I hereby declare that I shall not make any campaign contributions of \$250 or more to any member of the Planning Commission while my application is pending or within 3 months after a decision thereon by the Planning Commission.

I am informed and believe that to do so would be a violation of State law

I declare under penalty of perjury that the foregoing is true and correct

Executed in Stockton California this 22 day of April 1998

RENO WEST INVESTORS

Benjamin S Catlin  **Owner** Managing General Partner

Benjamin S Catlin  **Applicant**

Date _____

TM No _____

Public Hearing Date _____

Case Planner _____

TENTATIVE SUBDIVISION MAP STATEMENT

1 Name of owner(s) Panattoni-Catlin Joint Venture VII, a California General Partnership

Address(es) and phone no(s) 3620 Fair Oaks Blvd , Ste 150, Sacramento, California, 95864 (916) 485-8900

2 Name of subdivider(s) same as above

Address(es) and phone no(s) _____

3 Contact person(s) regarding the map Michael Hakeem, Esq

Address(es) and phone no(s) HAKHEEM, ELLIS & SIMONELLI, 2800 W March Lane Ste 200, Stockton, CA 95219 (209) 474-2800

4 The attached subdivision map shows proposed right of way, lot pattern, and other information as required by the City Subdivision Ordinance. The undersigned fully understands the provisions of the Subdivision Ordinance and will comply with all the requirements contained therein.

5 Specific Data

a Name of subdivision Airport Gateway Center

b Total acreage in subdivision ±11.924 acres

c General location of subdivision Please see attached description

d Estimated number of lots 25

e Water service provided by City of Stockton

f If any portion of the map is impacted by noise as defined by the noise contour information contained in the Noise Element of the General Plan, Ldn contours in excess of 60dB must be plotted on the map.

g School District Manteca Unified
If the map is within a school district in which conditions of overcrowding have been designated by the City Council, the following must be answered:

(over)

6 In accordance with the provisions of the "School Facilities Dedication Ordinance" as outlined in the Stockton Municipal Code, Chapter 8, Part VII, and in compliance with said ordinance, the undersigned prefers to

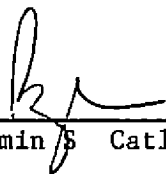
- a dedicate land for interim school facilities, or
- b pay a fee in lieu of dedication as established by the City Council, or
- c a combination of both

The Planning Commission may approve the map for residential development without requiring the payment of fees or the dedication of land if the Planning Commission finds that there exists specific overriding fiscal, economic, social or environmental factors which would benefit the community as a whole

7 Remarks (Specify purpose of land division)

Include the following as indicated when application is returned


- 1 Two (2) copies of completed application form
- 2 Application fee _____ plus _____ per lot
- 3 Initial Study and fee _____
- 4 A L U C fee _____
- 5 One copy of reverse sepiia
- 6 _____ copies of full size print
- 7 One copy of 8-1/2" by 11" reduction
- 8 Other _____

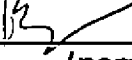
Signature(s) 
Benjamin S Catlin

Title Managing General Partner

Date 4/22/98

DISCLOSURE OF CAMPAIGN CONTRIBUTIONS

I Benjamin S Catlin  owner for a
Benjamin S Catlin (name)

I Benjamin S Catlin , applicant for a
Benjamin S Catlin (name)

(type and number of permit)

on oath declare that

(Please check appropriate statement and complete)

Owner Applicant

by to I have not made any campaign contributions in the amount of \$250 or more to any member of the Planning Commission within the past 12 months

_____ I have made a campaign contribution for (local) (State) (federal) election of \$ _____ to _____ within the last 12 months (name of Commissioner)

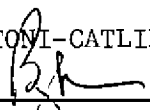
I understand that Government Code Section 84308 requires this disclosure and I make this disclosure in compliance therewith Further I hereby declare that I shall not make any campaign contributions of \$250 or more to any member of the Planning Commission while my application is pending or within 3 months after a decision thereon by the Planning Commission

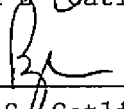
I am informed and believe that to do so would be a violation of State law

I declare under penalty of perjury that the foregoing is true and correct

Executed in Stockton, California this 22 day of April, 1998

PANATTONI-CATLIN JOINT VENTURE VII

Benjamin S Catlin 
Benjamin S Catlin **Owner** Managing General Partner

Benjamin S Catlin 
Benjamin S Catlin **Applicant**

Date _____

TM No _____

Public Hearing Date _____

Case Planner _____

TENTATIVE SUBDIVISION MAP STATEMENT

1 Name of owner(s) Apollo Properties, Inc

Address(es) and phone no(s) 8395 Jackson Road, Ste E, Sacramento, CA
95826 (916) 381-6509

2 Name of subdivider(s) Same as above

Address(es) and phone no(s) _____

3 Contact person(s) regarding the map Michael Hakeem, Esq

Address(es) and phone no(s) HAKHEEM, ELLIS & SIMONELLI, 2800 W March Lane,
Ste 200, Stockton, CA 95219 (209) 474-2800

4 The attached subdivision map shows proposed right of way, lot pattern, and other information as required by the City Subdivision Ordinance. The undersigned fully understands the provisions of the Subdivision Ordinance and will comply with all the requirements contained therein.

5 Specific Data

a Name of subdivision Airport Gateway Center

b Total acreage in subdivision ±6.003 acres

c General location of subdivision Please see attached description

d Estimated number of lots 25

e Water service provided by City of Stockton

f If any portion of the map is impacted by noise as defined by the noise contour information contained in the Noise Element of the General Plan, Ldn contours in excess of 60dB must be plotted on the map.

g School District Manteca Unified
If the map is within a school district in which conditions of overcrowding have been designated by the City Council, the following must be answered:

(over)

6 In accordance with the provisions of the "School Facilities Dedication Ordinance" as outlined in the Stockton Municipal Code, Chapter 8, Part VII, and in compliance with said ordinance, the undersigned prefers to

- _____ a dedicate land for interim school facilities, or
- _____ b pay a fee in lieu of dedication as established by the City Council, or
- _____ c a combination of both

The Planning Commission may approve the map for residential development without requiring the payment of fees or the dedication of land if the Planning Commission finds that there exists specific overriding fiscal, economic, social or environmental factors which would benefit the community as a whole

7 Remarks (Specify purpose of land division)

Include the following as indicated when application is returned

- 1 Two (2) copies of completed application form
- 2 Application fee _____ plus _____ per lot
- 3 Initial Study and fee _____
- 4 A L U C fee _____
- 5 One copy of reverse sepiia
- 6 _____ copies of full size print
- 7 One copy of 8-1/2" by 11" reduction
- 8 Other _____

Signature(s) *Linda Stanley*
Linda Stanley

Title President

Date 7/22/98

DISCLOSURE OF CAMPAIGN CONTRIBUTIONS

I, Linda Stanley (name), owner for a

I, Linda Stanley (name) applicant for a

(type and number of permit)

on oath declare that

(Please check appropriate statement and complete)

Owner Applicant

I have not made any campaign contributions in the amount of \$250 or more to any member of the Planning Commission within the past 12 months

I have made a campaign contribution for (local) (State) (federal) election of \$ _____ to _____ within the last 12 months (name of Commissioner)

I understand that Government Code Section 84308 requires this disclosure and I make this disclosure in compliance therewith. Further I hereby declare that I shall not make any campaign contributions of \$250 or more to any member of the Planning Commission while my application is pending or within 3 months after a decision thereon by the Planning Commission

I am informed and believe that to do so would be a violation of State law

I declare under penalty of perjury that the foregoing is true and correct

Executed in Stockton California, this 22 day of April, 1998

APOLLO PROPERTIES, INC

Linda Stanley
Linda Stanley **Owner** President

Linda Stanley
Linda Stanley **Applicant**

Date _____

TM Nc _____

Public Hearing Date _____

Case Planner _____

TENTATIVE SUBDIVISION MAP STATEMENT

1 Name of owner(s) Carl D Panattoni, Benjamin S Catlin and GR One a Calif
Limited Partnership, tenancy in common

Address(es) and phone no(s) c/o Benjamin S Catlin, 3620 Fair Oaks Blvd ,
Ste 150, Sacramento CA 95864 (916) 485-8900

2 Name of subdivider(s) same as above

Address(es) and phone no(s) _____

3 Contact person(s) regarding the map Michael Hakeem, Esq

Address(es) and phone no(s) HAKHEEM, ELLIS & SIMONELLI, 2800 W March Lane,
Ste 200, Stockton CA 95219 (209) 474-2800

4 The attached subdivision map shows proposed right of way, lot pattern, and other information as required by the City Subdivision Ordinance. The undersigned fully understands the provisions of the Subdivision Ordinance and will comply with all the requirements contained therein.

5 Specific Data

a Name of subdivision Airport Gateway Center

b Total acreage in subdivision ±16.012 acres

c General location of subdivision Please see attached description

d Estimated number of lots 25

e Water service provided by City of Stockton

f If any portion of the map is impacted by noise as defined by the noise contour information contained in the Noise Element of the General Plan, Ldn contours in excess of 60dB must be plotted on the map.

g School District Manteca Unified
If the map is within a school district in which conditions of overcrowding have been designated by the City Council, the following must be answered:

6 In accordance with the provisions of the "School Facilities Dedication Ordinance" as outlined in the Stockton Municipal Code, Chapter 8, Part VII, and in compliance with said ordinance, the undersigned prefers to

_____ a dedicate land for interim school facilities, or

_____ b pay a fee in lieu of dedication as established by the City Council, or

_____ c a combination of both

The Planning Commission may approve the map for residential development without requiring the payment of fees or the dedication of land if the Planning Commission finds that there exists specific overriding fiscal, economic, social or environmental factors which would benefit the community as a whole

7 Remarks (Specify purpose of land division)

Include the following as indicated when application is returned

1 Two (2) copies of completed application form

2 Application fee _____ plus _____ per lot

3 Initial Study and fee _____



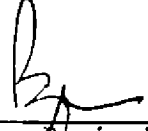
4 A L U C fee _____

5 One copy of reverse sepla

6 _____ copies of full size print

7 One copy of 8-1/2" by 11" reduction

8 Other _____

Signature(s)  Benjamin S. Catlin  Carl D. Panattoni  GR One, Benjamin S. Catlin
Managing General Partner

Title _____

Date 4/22/98

DISCLOSURE OF CAMPAIGN CONTRIBUTIONS

Benjamin S. Catlin

GR One, Benjamin S. Catlin, Managing General Partner
owner for a

Carl D. Panattoni (name)

GR One, Benjamin S. Catlin, Managing General Partner
applicant for a

Carl D. Panattoni (name)

Benjamin S. Catlin

(type and number of permit)

on oath declare that

(Please check appropriate statement and complete)

Owner Applicant

Owner

I have not made any campaign contributions in the amount of \$250 or more to any member of the Planning Commission within the past 12 months

Applicant

I have made a campaign contribution for (local) (State) (federal) election of \$ _____ to _____ within the last 12 months (name of Commissioner)

I understand that Government Code Section 84308 requires this disclosure and I make this disclosure in compliance therewith. Further I hereby declare that I shall not make any campaign contributions of \$250 or more to any member of the Planning Commission while my application is pending or within 3 months after a decision thereon by the Planning Commission

I am informed and believe that to do so would be a violation of State law

I declare under penalty of perjury that the foregoing is true and correct

Executed in Stockton, California, this 22 day of April 1998

Carl D. Panattoni

Benjamin S. Catlin

Benjamin S. Catlin
GR One, **Owner**
Benjamin S. Catlin, Managing General Partner

Benjamin S. Catlin
GR One, **Applicant**
Benjamin S. Catlin, Managing General Partner

Carl D. Panattoni

Benjamin S. Catlin

HOP24-98/IS13-97/EI 90/GPA2-97
Z-3-97/TM12-97/SPA-31/UP72-97
SEIR3-82/IS8-97/TM3-97/UP9-98/UP10-98
UP28-98/UP30-98/Z-4-98/UP57-97/UP47-97

AFFIDAVIT OF MAILING

STATE OF CALIFORNIA
COUNTY OF SAN JOAQUIN

Georgia M. Polk, being first duly sworn deposes and says

That she is a citizen of the United States over the age of twenty one (21) years, and at all times herein mentioned as an employee of the City of Stockton County of San Joaquin State of California

That on the 17th day of April 1998, she deposited in the United States mail facilities in the City of Stockton, County of San Joaquin State of California a copy of the agenda and staff report for the Planning Commission meeting of April 23, 1998 with the postage thereon prepaid addressed to each of the following named persons to wit

(SEE ATTACHED LIST)

That said persons on the attached list are the owners, applicants subdividers and/or engineers or representatives of the property and that this material was sent pursuant to State Code and/or City policy

Dated April 17, 1998

I certify (or declare) under penalty or perjury that the foregoing is true and correct



Arnaiz Watanabe Ltd II
P O Box 8596
Stockton CA 95208
(IS13-97/EIR1-90/GPA2-97/Z-3-97/TM12-97
SPA2-97/UP72-97)

Chevron/McDonalds Joint Venture
1750 Howe Avenue Suite 550
Sacramento CA 95825-3370
(IS13-97/EIR1-90/GPA2-97/Z-3-97/TM12-97
SPA2-97/UP72-97)

Tom Terpstra
Herum Crabtree Dyer Zolezzi, & Terpstra LLP
2291 W March Lane Suite B100
Stockton CA 95207
(IS13-97/EIR1-90/GPA2-97/Z-3-97/TM12-97
SPA2-97/UP72-97)

Daniel Correa
2006 Angelico Circle
Stockton CA 95207
(UP9-98)

Delores B Peterson
1227 Lencoe Drive
Stockton CA 95210
(HOP24-98)

Jeff Sanguinetti
A R Sanguinetti & Assoc
1150 W Robinhood Drive Suite 1C
Stockton CA 95207
(Z-4-98)

Peter Troger
9050 Bainbridge Place
Stockton CA 95209
(Z-4-98)

Thompson-Hysell Inc
1016 12th Street
Modesto CA 95354
(IS13-97/EIR1-90/GPA2-97/Z-3-97/TM12-97
SPA2-97/UP72-97)

Insite Environmental
2155 West March Lane Suite 1-C
Stockton CA 95207
(IS13-97/EIR1-90/GPA2-97/Z-3-97/TM12-97
SPA2-97/UP72-97)

Takashi T Watanabe Trust
3203 North Alpine Road
Stockton CA 95215
(IS13-97/EIR1-90/GPA2-97/Z-3-97/TM12-97
SPA2-97/UP72-97)

Nicanor Bernardino
2354 Canal Drive
Stockton CA 95204
(UP9-98)

Dorryl Ziege
1400 River Park Boulevard
Napa CA 94559
(HOP24-98)

Jay Allen
Stone Bros
1024 West Robinhood Drive
Stockton CA 95207
(Z-4-98)

Stan Oshita
Siegfried Engineering Inc
4045 Coronado Avenue
Stockton CA 95204-2396
(TM3-97/SEIR3-82/IS8-97)

Panattoni-Catlin Venture
3620 Fair Oaks Boulevard Suite 150
Sacramento CA 95864
ATTN Time Gagnier
(TM3-97/SEIR3-82/IS8-97)

Michael D Hakeem
Hakeem Ellis & Simonelli
2800 West March Lane Suite 200
Stockton CA 95219
(TM3-97/SEIR3-82/IS8-97)

David Koon Yet
P O Box 162
Ripon CA 95366
(UP28-98)

Downtown Stockton Alliance
Attn Tim Viall
P O Box 1165
Stockton CA 95201-1165
(UP30-98)

Tina De La Rosa
Orchids Restaurant
126 E Market Street
Stockton CA 95202
(UP30-98)

Gene & Josephine Celli
905 W Magnolia Street
Stockton, CA 95206
(UP10-98)

RHL Design Group
1390 Willow Pass Road Suite 420
Concord CA 94520
Attn Blythe Wilson
(UP10-98)

Reno West Investors
3620 Fair Oaks Boulevard Suite 150
Sacramento CA 95864
(TM3-97/SEIR3-82/IS8-97)

The FHK Stockton Partners
640 Watt Avenue Suite 100
Sacramento CA 95864
(UP28-98)

F & F Investment Co Ptp et al
P O Box 8600
Palm Springs CA 92264
(UP28-98)

Lung Kong Family Association
134 East Market Stret
Stockton CA 95202
(UP30-98)

Sun Chor & Betty Gong
214 W Clay Street
Stockton CA 95206
(UP10-98)

Andrew Magnasco
3200 Snyder Lane
Stockton CA 95215
(UP10-98)

Ralph L White
2305 South El Dorado Street
Stockton CA 95206
(UP57-97)

St George s Neighborhood Betterment
P O Box 6644
Stockton CA 95206
(UP57-97)

Stockton Flea Market Inc
Attn Tae W Oh
P O Box 6938
Stockton CA 95206
(UP47-97)

Kuysean Kang
2150 Fontana Avenue #11
Stockton, CA 95204
(UP47-97)

CC COMMUNITY DEVELOPMENT DEPARTMENT
PLANNING DIVISION
CITY PERMIT CENTER
425 NORTH EL DORADO STREET
STOCKTON CALIFORNIA 95202 1997
(209) 937 8266

February 17 1998

CITY OF STOCKTON

City Manager
Glen Birdzell w/map

Comm Dev /Planning
Ramirez w/map

Comm Dev /Building
Hlmes w/sm

Housing & Redev Dept
Pinkerton w/sm

PW/Eng
Sensibaugh w/map

PW/Meissner w/map

PW/O & M
Escobar w/map

PW/Solid Waste
Chen w/map

Parks & Rec
Machado w/map

Fire Department
Gills w/map

Police Department
Marconi w/map

Municipal Utilities Dept
Madison w/map

MIS
Edgecomb w/map

File w/map

SAN JOAQUIN COUNTY

Planning
w/map

Public Works
w/map

Public/ Env Health Serv
w/sm

Flood Control
w/map

Sheriff's Dept
w/sm

SCHOOL DISTRICTS

Stockton Unified
w/map

Lincoln Unified
w/map

Lodi Unified
w/map

Manteca Unified
w/map

STATE

CalTrans
w/map

OTHER AGENCIES

P G & E Stockton
w/map

P G & E Fresno
w/sm

Pacific Bell
w/map

MediaOne Cable
w/sm

SMART
w/map

U S Post Office
w/map

LAFCO
w/sm

COG/ALUC
w/sm

Cal Water
w/map

Other

Enclosed is a tentative map or proposal of: PANATTONI CATLIN VENTURE (AIRPORT GATEWAY CENTER) (TM3 97) which will be considered by the City of Stockton Planning Commission. The Development Review Committee will formulate a staff position and recommendation on this proposal on March 24, 1998. We would appreciate your reviewing this proposal and submitting any comments to this office by March 10, 1998.

Please answer and return the questionnaire on the back of this page as well as the map.

If you have any questions regarding this proposal please contact Associate Planner David Stagnaro at (209) 937 8266.

20/DSS

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MEMORANDUM

APR 7 1998

April 7 1998

TO John Carlson Community Development Director
 Attention David Stagnaro, Associate Planner

FROM Gregg S Meissner Senior Transportation Planner

SUBJECT **TENTATIVE MAP FOR PANATTONI-CATLIN VENTURE (TM3-97)**
AIRPORT GATEWAY CENTER

Public Works staff reviewed the subject tentative map. Based on their review, standard Public Works' conditions and conformance to the adopted Standard Plans and Specifications are recommended.

Further, it is also recommended approval of the tentative map include the following conditions:

1. Comply with all applicable conditions and all mitigation measures adopted in EIR3-82 and IS8-97.
2. The owners, developers and/or successors-in-interest shall dedicate and improve Sperry Road to provide a minimum half-section of 67 feet. In addition, the owners, developers and/or successors-in-interest shall dedicate the necessary slope easements for the future Arch-Sperry Corridor grade separated crossings adjacent to this tentative map.
3. The owners, developers and/or successors-in-interest shall dedicate access rights to the City of Stockton at the following locations:
 - a. Aviation Drive, north side, entire length, between Runway Drive and the Tidewater Southern Railroad.
 - b. Aviation Drive, south side, entire length, between Runway Drive and the Tidewater Southern Railroad, except at the 50-foot wide non-exclusive access easement.
 - c. Aviation Drive, both sides, west of Runway Drive within 400 feet.
 - d. Runway Drive, both sides, within 400 feet of Aviation Drive.

TENTATIVE MAP FOR PANATTONI-CATLIN VENTURE (TM3-97) AIRPORT GATEWAY CENTER

- 4 Access to Lot 12 shall be provided via a non-exclusive access easement through Lot 11
- 5 Access to Lot 25 shall be provided via a non-exclusive access easement through Lot 24
- 6 Access to all lots shall be limited to one driveway per parcel. A common or shared access shall be counted as 0.5 access
- 7 Prior to the recordation of any final maps for this tentative map area, the owners, developers and/or successors-in-interest shall be responsible for 100% the design and construction costs for approach geometrics, as described below, and traffic signals at the following intersections

Sperry Road/Performance Drive - provide dual left turn lanes for westbound and northbound approaches and a free right turn lane on the eastbound approach. The northbound approach shall consist of a minimum of dual left turn lanes, one through lane and one right turn lane

Airport Way/C E Dixon Street - provide a southbound right turn lane, a northbound left turn lane, and dual left turn lanes, one through lane and a right turn lane for the eastbound approach
- 8 Prior to the recordation of any final maps for this tentative map area, the owners, developers and/or successors-in-interest, shall be responsible for 100% of the design and construction cost to provide 4 through lanes on Sperry Road between Airport Way and Performance Drive
- 9 The owners, developers and/or successors-in-interest shall be responsible for 100% of the design and construction costs for traffic signals at the Aviation Drive/Performance Drive intersection. These traffic signals shall be designed as part of the subdivision improvements and shall be installed when warranted or at the discretion of the Public Works Director
- 10 Prior to recordation of any final maps for this tentative map area, the owners, developers and/or successors-in-interest shall be responsible for 100% of the design and construction costs for the following improvements

TENTATIVE MAP FOR PANATTONI-CATLIN VENTURE (TM3-97) AIRPORT GATEWAY CENTER

- a Left turn lanes on the Sperry Road and Arch-Airport Road approaches to Airport Way
 - b Widening of all intersection approaches including the at-grade Union Pacific Railroad crossing at Sperry Road/McKinley Avenue to provide auxiliary turn lanes and traffic signals
 - c Widening of the southbound McKinley Avenue approach to provide a separate right turn lane widening of both French Camp Road approaches to provide left turn lanes and traffic signals at the French Camp Road/McKinley Avenue intersection
 - d Widening of Sperry Road between Performance Drive and McKinley Avenue to provide a minimum of 4 through lanes
- 11 The owners, developers and/or successors-in-interest shall be responsible for 100% of the design and construction costs associated with the following improvements and shall provide said improvements prior to the issuance of any certificates of occupancy if the land use intensity of the 368-acre Airport Gateway Center area exceeds the proposed project mix of 61% high cube warehouse and 39% general light industrial
- a Widening of McKinley Avenue between Sperry Road and El Dorado Street to provide a minimum of 4 through lanes
 - b Secondary access to Sperry Road from this area
 - c Traffic signals at the Aviation Drive/Runway Drive intersection These traffic signals shall be designed and installed when warranted or at the discretion of the Public Works Director
- 12 The owners, developers and/or successors-in-interest shall be also responsible for 100% of the design and construction costs associated with the following improvements and shall provide said off-site improvements prior to the issuance of any certificates of occupancy, if the land use intensity of the 368-acre Airport Gateway Center area exceeds the 61%/39% project mix and the City's standard light industrial category

TENTATIVE MAP FOR PANATTONI-CATLIN VENTURE (TM3-97) AIRPORT GATEWAY CENTER

- a Completion of Downing Avenue as a minimum 2-lane facility between Interstate 5 and El Dorado Street
 - b Construction of a secondary access to Airport Way from the Airport Gateway Center area
 - c Widening of Ralph Avenue to provide dual eastbound left turn lanes at the Airport Way/Ralph Avenue intersection
 - d Widening of Sperry Road to provide dual eastbound left turn lanes and widen the Airport Way approaches to provide 6 through lanes at Airport Way/Sperry Road
 - e Widening of the McKinley Avenue approach to provide a free westbound right turn lane at the McKinley Avenue/Sperry Road intersection
 - f Construction of an urban interchange at the Airport Way/Sperry Road
- 13 The owners developers and/or successors-in-interest shall obtain all necessary permits to construct the Aviation Drive at-grade crossing of the Tidewater Southern Railroad Access to this crossing shall not be permitted until all required safety equipment is operational and the crossing is open to the general public
- 14 The owners developers and/or successors-in-interest shall dedicate and improve a 40-foot wide emergency access only road, extending north from the north end of Runway Drive to the east side of the San Joaquin Valley Electric Railroad right-of-way, then north to Sperry Road
- 15 The owners developers and/or successors-in-interest shall be granted short-term temporary access to Runway Drive from Sperry Road for a period of 18 months from the time of application to the Public Utilities Commission for the Aviation Drive at-grade crossing of Tidewater Southern Railroad The owners developers and/or successors-in-interest shall be responsible for monitoring the traffic operations at the intersection of this access and Sperry Road Specifically as additional lots are developed the owners developers and/or successors-in-interest shall provide a traffic analysis to the Public Works Department to determine if the access configuration can adequately accommodate the additional trip generation Said

TENTATIVE MAP FOR PANATTONI-CATLIN VENTURE (TM3-97) AIRPORT GATEWAY CENTER

traffic analysis shall identify the improvements needed to maintain the City's minimum level-of-service standard as well as provide for safe access. The owners, developers and/or successors-in-interest shall be responsible for 100% of these identified improvements.

- 16 The owners, developers and/or successor-in-interest shall be responsible for their proportionate share, based on traffic loadings, of the costs for the following improvements to reduce the significance of the cumulative impacts:

- a Improvements to the following intersections including but not limited to traffic signals as identified in the Draft Supplemental Environmental Impact Report for this project:

Downing Avenue/SB Interstate 5 ramps
Downing Avenue/NB Interstate 5 ramps
French Camp Road/SB Interstate 5 ramps
French Camp Road/NB Interstate 5 ramps
French Camp Road/Arch-Airport-Sperry (future)
Arch-Airport-Sperry/French Camp Road
Sperry Road/McKinley Avenue
French Camp Road/McKinley Avenue
French Camp Road/Airport Way
El Dorado Street/Downing Avenue (future)
Clayton Avenue/El Dorado Street/McKinley Avenue
Arch-Airport-Sperry/Performance Drive
Arch-Airport-Sperry/Alternate Access
Airport Way/Ralph Avenue
Airport Way/Industrial Drive
Airport Way/Sperry-Arch-Airport
Airport Way/C E Dixon Street
Arch-Airport-Sperry/SR 99 ramps
Arch-Airport Road/West SR 99 Frontage Road
Arch-Airport Road/East SR 99 Frontage Road
West SR 99 Frontage Road/SB SR 99 ramps
East SR 99 Frontage Road/NB SR 99 ramps

TENTATIVE MAP FOR PANATTONI-CATLIN VENTURE (TM3-97) AIRPORT GATEWAY CENTER

- b Widening of the following roadways
- Airport Way north of C E Dixon Street to 6 lanes
 - El Dorado Street between Downing Avenue and Clayton Avenue to 6 lanes
 - El Dorado Street between Clayton Avenue to French Camp Road to 4 lanes
 - Arch-Sperry Road between Interstate 5 and State Route 99 to 8 lanes
- 17 The owners, developers and/or successors-in-interest shall participate equitably in any area of benefit assessment district or other financing mechanism formed to design and/or construct the Arch-Sperry Specific Road Plan Corridor between Interstate 5 and Austin Road including, but not limited to, grade separated railroad crossings, waterway crossings, traffic signals, re-alignment of French Camp Road and re-alignment of "old" Sperry Road.
- 18 The owners, developers and/or successors-in-interest shall pay a proportionate share based on traffic loadings of the cost of the Project Study Reports (PSRs) and necessary improvements described in the PSRs to the Arch Road/State Route 99 urban interchange, the French Camp Road/Interstate 5 interchange, and the State Route 99 and Interstate 5 mainline systems to provide levels-of-service that conform to California Urban Highway Standards.
- 19 The owners, developers and/or successors-in-interest shall participate in any future bicycle facilities along the Arch-Sperry corridor as described in the City's adopted Bicycle Facilities Master Plan.
- 20 The owners, developers and/or successors-in-interest shall dedicate all necessary rights-of-way and easements along French Camp Slough and comply with all other requirements of the agencies having jurisdiction over French Camp Slough.
- 21 The owners, developers and/or successors-in-interest shall dedicate all necessary rights-of-way and easements along the San Joaquin Valley Electric and Union Pacific railroads and comply with all requirements of the above railroad agencies.
- 22 The owners, developers and/or successors-in-interest shall obtain and dedicate easements for the outfall storm drain discharge lines to the existing slough.
- 23 The storm drain detention basins shall be maintained privately by the property

TENTATIVE MAP FOR PANATTONI-CATLIN VENTURE (TM3-97) AIRPORT GATEWAY CENTER

owners The owners, developers and/or successors-in-interest shall prepare a maintenance agreement shall submit said agreement to the City for its approval and shall record such agreement If the private maintenance group defaults the City shall form a mandatory maintenance district to assume the responsibility for the maintenance of the detention basins

- 24 The location and size of the detention basins shall be approved by the Stockton Metropolitan Airport
- 25 The owners, developers and/or successors-in-interest shall construct storm drain basins in accordance with the approved Interim Storm Drainage Plans Any subsequent Master Drainage Plans shall be approved by the City Engineer Storm water shall be discharged to French Camp Slough Discharge pumps shall be controlled by a float in French Camp Slough so as to avoid discharges during peak flows in the slough
- 26 The owners, developers and/or successors-in-interest shall obtain all applicable local State and Federal permits for discharge of storm drainage to French Camp Slough

Further the following information should be forwarded to the owners developers and/or successors-in-interest

- 1 Show all right-of-way widths and typical sections of all streets including Sperry Road and Airport Way
- 2 All existing site wells or septic tanks shall be abandoned in accordance with all San Joaquin County Health Department standards
- 3 Show the width location and identity of all existing easements
- 4 The Record of Survey number labeled on the map across the 3 820 acre and 4 587 acre lots on the north side of Performance Drive approximately 800 feet west of Airport Way is incorrect
- 5 Some of the utility line sizes are illegible All sanitary sewer water and storm drain lines shall conform with the applicable approved interim plans

John Carlson
April 7 1998
Page 8

**TENTATIVE MAP FOR PANATTONI-CATLIN VENTURE (TM3-97) AIRPORT GATEWAY
CENTER**

- 6 This tentative map was drawn at a scale of 1" = 300' The Stockton Municipal Code requires that tentative maps be drawn at a minimum scale of 1" = 100'
- 7 Any existing overhead utility lines on the site shall be placed underground

JAMES B GIOTTONINI
PUBLIC WORKS DIRECTOR



GREGG S MEISSNER
SENIOR TRANSPORTATION PLANNER

GSM KT/RH

cc PW files

STAFF REPORT

Item E-3(b) PUBLIC HEARING - Tentative Map
Case No TM3-97 Reno West Investors et al

Data Reno West Investors et al as property owner is requesting a tentative map to subdivide a 190.87-acre parcel into twenty-five (25) lots for industrial development. The site is zoned E-P Enterprise Performance District. The site is bounded to

- the north across Sperry Road by manufacturing uses and vacant land in the County zoned I-G
- the east by vacant land and warehousing uses under construction zoned E-P Enterprise Performance District
- the south across the French Camp Slough and Tidewater Southern Railroad tracks (owned by Union Pacific Railroad) by a truck terminal and vacant land in the county zoned I-L and AU-20 respectively and
- the west across the Union Pacific Railroad tracks and McKinley Avenue by vacant land in the county zoned AU-20

The tentative map site is presently vacant except for the Tidewater Southern Railroad tracks (Zoning Map page 71). See attached exhibits.

General Plan The City of Stockton General Plan diagram designates this site for Industrial uses.

Environmental Clearance Staff has prepared, circulated, and is recommending certification of Supplemental Environmental Impact Report/Initial Study (SEIR3-82/IS8-97) for the proposed project [see Item E-3(a) on this same agenda]. In addition, pursuant to the California Environmental Quality Act (CEQA), the State CEQA Guidelines, and the Public Resources Code, staff has prepared and is recommending adoption of the related CEQA Findings, Statement of Overriding Considerations, and a Mitigation Monitoring Program for the Airport Gateway Center Project, which are required for approval of the proposed tentative map and for the previously approved Deferred Improvement Agreement.

Development Review Committee The Development Review Committee recommended approval of this tentative map subject to the proposed conditions in this staff report

Discussion As background the Airport Gateway Center Project site of approximately 416 acres was annexed into the City in 1984 and was designated zoned and proposed to be subdivided (TM29-83) in order to permit industrial development. A final map was not filed prior to the expiration date of Tentative Map 29-83 which voided the approved tentative planned parcelization of the area. However prior to annexation into the City the project site had been legally subdivided into five-acre parcels by the recorded Terra Matre Tract map.

A portion of the site primarily east of the Tidewater Southern Railroad is being developed outside the tentative map process through the adjustment of existing legal parcel boundaries via lot line adjustments and parcel mergers and subject to existing Stockton Municipal Code (SMC) zoning regulations.

The current owners of the Airport Gateway Center Project area and the City of Stockton have entered into a Deferred Improvement Agreement (DIA) that requires the owners, developers and/or successors-in-interest (ODS) to comply with all the conditions of Tentative Map 29-83 and the mitigation measures of the previously-certified Environmental Impact Report (EIR3-82) unless they are superseded by a more current tentative map and/or EIR document (see related agenda item E-3(a)). The DIA requires the owners, developers and/or successors-in-interest to comply with the tentative map conditions regardless of whether all or part of the tentative map area is completed via a final map. Similarly the mitigation measures identified in SEIR3-82/IS8-97 will be binding upon the owners, developers and successors-in-interest.

Although the subject tentative map (TM3-97) covering 190.87 acres is the discretionary application that necessitates the need for the additional environmental documentation under the California Environmental Quality Act (CEQA) the DIA commits the ODS to comply with the mitigation measures (SEIR3-82/IS8-97) for the Airport Gateway Center Project area in its entirety (416 acres) with specified exceptions.

The applicant's proposal is to subdivide the remaining 190.87-acre area within the Airport Gateway Center Project into 25 parcels ranging in size from 1.75 to 29 acres. Access to and egress from the proposed parcels will be

provided via Performance Drive (existing) Aviation Drive (proposed) and Runway Drive (proposed) A short-term temporary access to Runway Drive from Sperry Road will be granted in accordance with proposed Condition No 17

Recommendation Approval based on the following findings

- 1 The map complies with applicable State, County and City codes regulations and adopted standards
- 2 None of the grounds for denial as stipulated in Section 66474 of the State of California Subdivision Map Act are applicable on this tentative map
- 3 The proposed project is consistent with the City's existing General Plan for the location of industrial uses
- 4 The City of Stockton has reviewed the Final Supplemental Environmental Impact Report/Initial Study (SEIR3-82/IS8-97) for the proposed project and has certified that it has been completed in compliance with the California Environmental Quality Act (CEQA) and that the final decision-making body for this and/or any related discretionary approvals for the project has considered the information contained in the Final SEIR/IS prior to approving the project
- 5 Pursuant to Sections 15091 and 15093 of the State CEQA Guidelines and to Public Resources Code Section 21081.6 this approval is based on, and subject to the adopted findings mitigation measures and mitigation monitoring/reporting provisions as specified in the Findings, Statement of Overriding Considerations, and Mitigation Monitoring Program for the Airport Gateway Center Project (SEIR3-82/IS8-97 April 1998)

Proposed Conditions TM3-97

- 1 Comply with applicable Federal State County and City codes regulations and adopted standards and pay all applicable fees
- 2 This tentative map is subject to the provisions of the Deferred Improvement Agreement (Recorded on July 23 1997 - Instrument No 97071908 affecting the subject property) Said agreement shall be binding on the owners developers and/or successors-in-interest

- 3 Based on the significant and/or potentially significant environmental effects identified in the Supplemental Environmental Impact Report/Initial Study for the Airport Gateway Center Project (SEIR3-82/IS8-97) and pursuant to Sections 15091 and 15093 of the State CEQA Guidelines and to the Public Resources Code Section 21081.6 this approval is based on and subject to, the adopted findings mitigation measures and mitigation monitoring/reporting provisions, as specified in the Findings, Statement of Overriding Considerations, and Mitigation Monitoring Program for the Airport Gateway Center Project.
- 4 The owners developers and/or successors-in-interest shall dedicate and improve Sperry Road to ultimately provide a minimum half-section of 67 feet along the project frontage as illustrated in TM29-83. In addition, the owners developers and/or successors-in-interest shall dedicate the necessary slope easements for the future Arch-Sperry Corridor grade separated crossings adjacent to this tentative map.
- 5 The owners developers and/or successors-in-interest shall dedicate access rights to the City of Stockton at the following locations:
 - a Aviation Drive north side entire length between Runway Drive and the Tidewater Southern Railroad
 - b Aviation Drive south side entire length between Runway Drive and the Tidewater Southern Railroad except at the 50-foot wide non-exclusive access easement
 - c Aviation Drive both sides west of Runway Drive within 400 feet
 - d Runway Drive both sides within 400 feet of Aviation Drive
- 6 Access to Lot 12 shall be provided via a non-exclusive access easement through Lot 11.
- 7 Access to Lot 25 shall be provided via a non-exclusive access easement through Lot 24.
- 8 Access to all lots shall be limited to one driveway per tentative map parcel. A common or shared access shall be counted as 0.5 access.

- 9 Prior to the recordation of any final maps for this tentative map area the owners developers and/or successors in-interest shall be responsible for 100% of the design and construction costs for approach geometrics as described below and traffic signals at the following intersections

Sperry Road/Performance Drive-provide dual left-turn lanes for westbound and northbound approaches and a free right-turn lane on the eastbound approach The northbound approach shall consist of a minimum of dual left-turn lanes one through lane and one right-turn lane

Airport Way/C E Dixon Street-provide a southbound right-turn lane a lengthened northbound left-turn lane and dual left-turn lanes one through lane and a right-turn lane for the eastbound approach

- 10 Prior to the recordation of any final maps for this tentative map area the owners developers and/or successors-in-interest shall be responsible for 100% of the design and construction cost to provide four (4) through lanes on Sperry Road between Airport Way and Performance Drive
- 11 The owners developers and/or successors-in-interest shall be responsible for 100% of the design and construction costs for traffic signals at the Aviation Drive/Performance Drive intersection These traffic signals shall be designed as part of the subdivision improvements and shall be installed when warranted or at the discretion of the Public Works Director
- 12 Prior to recordation of any final maps for this tentative map area the owners developers and/or successors-in-interest shall be responsible for 100% of the design and construction costs for the following improvements
- a Left-turn lanes on the Sperry Road and Arch-Airport Road approaches to Airport Way
 - b Widening of all intersection approaches, including the at-grade Union Pacific Railroad crossing at Sperry Road/McKinley Avenue to provide auxiliary turn lanes and traffic signals

- c Widening of the southbound McKinley Avenue approach to provide a separate right turn lane widening of both French Camp Road approaches to provide left turn lanes and traffic signals at the French Camp Road/McKinley Avenue intersection
 - d Widening of Sperry Road between Performance Drive and McKinley Avenue to provide a minimum of four (4) through lanes
- 13 The owners developers and/or successors-in-interest shall be responsible for 100% of the design and construction costs associated with the following improvements and shall provide said improvements prior to the issuance of any certificates of occupancy if the land use intensity of the 368-acre Airport Gateway Center area exceeds the proposed project mix of 61% high cube warehouse and 39% general light industrial
- a Widening of McKinley Avenue between Sperry Road and El Dorado Street to provide a minimum of 4 through lanes
 - b Secondary access to Sperry Road from this area
 - c Traffic signals at the Aviation Drive/Runway Drive intersection These traffic signals shall be designed and installed when warranted or at the discretion of the Public Works Director
- 14 The owners developers and/or successors-in-interest shall also be responsible for 100% of the design and construction costs associated with the following improvements and shall provide said off-site improvements prior to the issuance of any certificates of occupancy if the land use intensity of the 368-acre Airport Gateway Center area exceeds the 61%/39% project mix and the City's standard light industrial category
- a Completion of Downing Avenue as a minimum two-lane facility between Interstate 5 and El Dorado Street
 - b Construction of a secondary access to Airport Way from the Airport Gateway Center area
 - c Widening of Ralph Avenue to provide dual eastbound left-turn lanes at the Airport Way/Ralph Avenue intersection

- d Widening of Sperry Road to provide dual eastbound left turn lanes and widen the Airport Way approaches to provide 6 through lanes at Airport Way/Sperry Road (Under cumulative conditions construction of an urban interchange at this intersection will be warranted)
 - e Widening of the McKinley Avenue approach to provide a free westbound right-turn lane at the McKinley Avenue/Sperry Road intersection
- 15 The owners developers and/or successors-in-interest shall obtain all necessary permits and shall construct the Aviation Drive at-grade crossing of the Tidewater Southern Railroad Access to this crossing shall not be permitted until all required safety equipment is operational and the crossing is open to the general public
- 16 The owners developers and/or successors-in-interest shall dedicate and improve a 40-foot wide emergency access only road extending north from the north end of Runway Drive to the east side of the San Joaquin Valley Electric Railroad right-of-way then north to Sperry Road
- 17 The owners developers and/or successors-in-interest shall be granted short-term temporary access to Runway Drive from Sperry Road for a period of 18 months from the time of application to the Public Utilities Commission for the Aviation Drive at-grade crossing of Tidewater Southern Railroad The owners developers and/or successors-in-interest shall be responsible for monitoring the traffic operations at the intersection of this access and Sperry Road Specifically as additional lots are developed the owners developers and/or successors-in-interest shall provide a traffic analysis to the Public Works Department to determine if the access configuration can adequately accommodate the additional trip generation Said traffic analysis shall identify the improvements needed to maintain the City's minimum level-of-service standard as well as provide for safe access The owners developers and/or successors-in-interest shall be responsible for 100% of these identified improvements
- 18 The owners developers and/or successors-in-interest shall be responsible for their proportionate share based on traffic loadings of the costs for the following improvements to reduce the significance of the cumulative impacts

- a Improvements to the following intersections including but not limited to traffic signals, as identified in the Draft Supplemental Environmental Impact Report for this project

Downing Avenue/SB Interstate 5 ramps
Downing Avenue/NB Interstate 5 ramps
French Camp Road/SB Interstate 5 ramps
French Camp Road/NB Interstate 5 ramps
French Camp Road/Arch-Airport-Sperry (future)
Arch-Airport-Sperry/French Camp Road
Sperry Road/McKinley Avenue
French Camp Road/McKinley Avenue
French Camp Road/Airport Way
El Dorado Street/Downing Avenue (future)
Clayton Avenue/El Dorado Street/McKinley Avenue
Arch-Airport-Sperry/Performance Drive
Arch-Airport-Sperry/Alternate Access
Airport Way/Ralph Avenue
Airport Way/Industrial Drive
Airport Way/Sperry-Arch-Airport
Airport Way/C E Dixon Street
Arch-Airport-Sperry/SR 99 ramps
Arch-Airport Road/West SR 99 Frontage Road
Arch-Airport Road/East SR 99 Frontage Road
West SR 99 Frontage Road/SB SR 99 ramps
East SR 99 Frontage Road/NB SR 99 ramps

- b Widening of the following roadways

Airport Way north of C E Dixon Street to six lanes
El Dorado Street between Downing Avenue and
Clayton Avenue to six lanes
El Dorado Street between Clayton Avenue to French
Camp Road to four lanes
Arch-Sperry Road between Interstate 5 and State Route 99 to
eight lanes

- 19 The owners, developers and/or successors-in-interest shall participate equitably in any area of benefit assessment district or other financing mechanism formed to design and/or construct the Arch-Sperry Specific Road Plan Corridor between Interstate 5 and Austin Road including but not limited to grade separated railroad crossings, waterway crossings, traffic signals, realignment of French Camp Road and realignment of "old" Sperry Road

- 20 The owners developers and/or successors-in-interest shall pay a proportionate share based on traffic loadings of the cost of the Project Study Reports (PSRs) and necessary improvements described in the PSRs to the Arch Road/State Route 99 urban interchange the French Camp Road/Interstate 5 interchange and the State Route 99 and Interstate 5 mainline systems to provide levels-of-service that conform to California Urban Highway Standards
- 21 The owners developers and/or successors-in-interest shall participate in any future bicycle facilities along the Arch-Sperry Corridor as described in the City's adopted Bicycle Facilities Master Plan
- 22 The owners developers and/or successors-in-interest shall dedicate all necessary rights-of-way and easements along French Camp Slough and comply with all other requirements of the agencies having jurisdiction over French Camp Slough
- 23 The owners developers and/or successors-in-interest shall provide all necessary rights-of-way and easements along the San Joaquin Valley Electric and Union Pacific railroads and comply with all requirements of the above railroad agencies as identified through the Environmental Impact Report and tentative map approval process
- 24 The owners developers and/or successors-in-interest shall obtain and dedicate easements for the outfall storm drain discharge lines to the existing slough
- 25 The storm drain detention basins shall be maintained privately by the property owners. The owners developers and/or successors-in-interest shall prepare a maintenance agreement shall submit said agreement to the City for its approval and shall record such agreement. If the private maintenance group defaults the City shall form a mandatory maintenance district to assume the responsibility for the maintenance of the detention basins
- 26 The location and design of the detention basins shall be approved by the Stockton Metropolitan Airport
- 27 The owners developers and/or successors-in-interest shall construct storm drain basins in accordance with the approved Interim Storm Drainage Plans. Any subsequent Master Drainage Plans shall be approved by the City Engineer. Storm water shall be discharged to

a tributary of the French Camp Slough located to the north of the project site. Discharge pumps shall be controlled by a metering device in the said tributary of the French Camp Slough so as to avoid discharges during peak flows in the slough.

- 28 The owners, developers and/or successors-in-interest shall obtain all applicable local, State and Federal permits for discharge of storm drainage into said tributary of the French Camp Slough.

Note Any of the above improvements specifically included in the identified improvements and needs assessment used to determine the Public Facilities Fees may be subject to reimbursement from the Public Facilities Fee as allowed by the City Council-Adopted Guidelines. Further, the City upon request of the developer will attempt to form an Area of Benefit or other financial mechanism to recover the costs of improvements not included in the Public Facilities Fees which benefit other properties outside the limits of this project.

April 16, 1998

Note Staff reports are prepared well in advance of the Planning Commission consideration of the proposal and reflect the staff's view based on the best available information at the time the report was formulated. Evidence submitted during the course of the public hearing may require a re-evaluation of the staff's position.

MEMORANDUM

APR 23 1998

April 23, 1998

TO Stockton Planning Commission
FROM Bob Sivell Economic Development/Enterprise Zone Manager
SUBJECT AIRPORT GATEWAY BUSINESS CENTER

I would like to voice my support of the Airport Gateway Business Center Project. The project will enhance Stockton's ability to attract national and international businesses to invest in the community and in turn provide jobs for its residents. Several businesses currently conducting site searches require relatively large rail-served sites for their projects. This site demand criteria will be offered at Airport Gateway Business Center.

Further, the addition of this project to Stockton's inventory of industrial land will display to the State of California's Trade & Commerce Agency our continuing commitment to the success of the Stockton/San Joaquin Enterprise Zone.



for ROBERT A. SIVELL
ECONOMIC DEVELOPMENT/ENTERPRISE ZONE MANAGER
ECONOMIC DEVELOPMENT DIVISION

RAS ja

cc Community Development Department

T:\MANAGER\ZONE\LETTERS\GATESUP.WPD

PH 4-23-98

Copies to PC 4/23/98 sl

4-9-98

- CORRUGATED SYNERGIES
NOT YET RECORDED

- DIANE MOORE - T/C w/ US FISH & WILDLIFE
IN ONE WEEK.

- M HAKEEM - NO EFFORT TO FURTHER ADDRESS
CONCERNS

- SEND LIST OF T/m CONDITIONS & PROP OWNERS
TO M HAKEEM FOR SIGNATURES

- ADDITIONAL NOTES FOR T/m

- LETTER OF APRIL 2, AS ADDENDUM
TO DEFERRED IMPROVEMENT AGREEMENT
INCLUDE IN STAFF REPORT?

- FISH & GAME FEE?

ECONOMIC REVIEW COMMITTEE MEETING

PROJECT AIRPORT GATEWAY

DATE APRIL 9, 1998

PLEASE SIGN IN

NAME	COMPANY/DEPARTMENT	PHONE NUMBER
SAM MAH	CD/PLANNING	937-8266
Greg Messner	Public Works	937-8270
Kathy Tomura	Public Works	937-8690
Sally Break	Housing & Redevelopment	937-2322
Ron Cook	H.R.D.	937-8063
MIKE MALBOM	DOT	474-2800
Bob Sivell	COS	937-8530
REED HOGAN	COS PW	937-8868
LAURA MAHONEY	PANATTONI	916-485-8900
BRIAN VALUS	PANATTONI / AP & THOMAS	916-485-8200
DAVID STAGNARO	COS	937-8266
Sharon Parker	Econ Dev Div.	937-8862
Lex Corrales	Siegfried Eng. Inc.	943-2021
Jim Billigmeier	" "	" "

CURRENT NUMBER OF EMPLOYEES NA

PROJECTED ADDITIONAL EMPLOYEES NA

† manager@zone@ercsign in (9/97-ai)

13
4

Recorded on 7/23/97
Inst # 97071908

After Recording Return To
City of Stockton
Public Works Dept
425 N El Dorado Street
Stockton CA 95202

DEFERRED IMPROVEMENT AGREEMENT

OWNERS

Panattoni-Catlin Venture

Reno West Investors

Panattoni-Catlin Joint Venture VII
A California General Partnership

Apollo Properties Inc

ADDRESS

3620 Fair Oaks Blvd , Suite 150
Sacramento, CA 95864

THIS AGREEMENT is made and entered into at Stockton California this 24th day of JUNE 1997, by and between

Panattoni-Catlin Venture, Reno West Investors, Panattoni Catlin Joint Venture VII, A California General Partnership, and Apollo Properties, Inc hereinafter referred to as "Owners" and the CITY OF STOCKTON a Municipal Corporation, located in the County of San Joaquin, State of California hereinafter referred to as "City"

WHEREAS Owners own real property in the City of Stockton, County of San Joaquin, State of California and more particularly described as follows

That certain real property situated in the City of Stockton, County of San Joaquin, State of California described as follows

All that certain real property known as Terra Matre Tract as shown on the map filed for record in Book of Maps and Plats Volume 6 Page 26 San Joaquin Records

WHEREAS Owners are desirous of deferring construction of said improvements associated with the above described parcel

WHEREAS said improvements to be deferred are the following

SEE EXHIBIT "A"

CONSISTING OF BUT NOT LIMITED TO The design and installation of facilities listed in Exhibit A

WHEREAS it is in the interest of the City to defer completion of said frontage improvements at this time pursuant to the authority contained in Stockton Municipal Code Section 16-174.1 because of the lack of frontage improvements on adjacent parcels

NOW THEREFORE IT IS MUTUALLY STIPULATED AND AGREED

1 That said construction is deferred until required by the City pursuant to Exhibit "A" Such determination shall be made by the City Engineer based upon the totality of circumstances associated with the parcel such as adjacent development changed conditions of the adjacent area etc

2 That upon notification by the City, the Owners or Owners successors or assigns will engage a civil engineer licensed in the State of California to design and prepare construction drawings for the required improvements to be approved by the City Engineer Said drawings shall be submitted within 90 days after notification and construction shall commence within 30 days after approval of the construction drawings

3 That the Owners or Owners successors or assigns shall pay to the City all plan checking, processing and inspection fees pertaining to the improvements associated with this specific agreement

4 That construction of said improvements shall be in accordance with Chapter 16 Division 4 of the City of Stockton Municipal Code

5 That in event of default by Owners, or Owners' successors or assigns the City is hereby authorized to cause said construction to be done and charge the entire cost and expense to Owners, or Owners' successors or assigns including interest thereon at the maximum legal rate from the date of notice of said cost and expense until paid Such costs or expenses shall constitute a lien upon the property

6 That this agreement shall be recorded in the office of the Recorder of San Joaquin County, California, at the expense of Owners and shall constitute notice to all successors

and assigns of the title to said real property of the obligation herein set forth and also a lien in such amount as will fully reimburse the City including interest as hereinabove set forth subject to foreclosure in event of default in payment

7 That in event of litigation occasioned by any default of Owners or Owners' successors or assigns Owners or Owners' successors or assigns agree to pay all costs involved, including reasonable attorney's fees, and that the same shall become a part of the lien against said real property

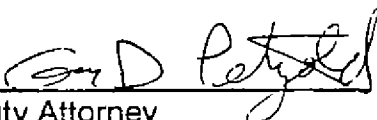
8 That the term "Owners" shall include not only the present Owners but also heirs successors executors administrators and assigns it being the intent of the parties hereto that the obligations herein undertaken shall run with said real property and constitute a lien there against


9 The obligations herein undertaken shall be binding upon the heirs, successors executors administrators and assigns of the parties hereto

IN WITNESS WHEREOF, the parties hereto have affixed their signatures as of the date first written above

APPROVED AS TO FORM

CITY OF STOCKTON


Deputy City Attorney

BY 
PAUL M. SENSIBAUGH
DEPUTY PUBLIC WORKS
DIRECTOR/CITY ENGINEER

OWNER

Be SC
Panattoni-Catlin Venture

OWNER ACKNOWLEDGMENT

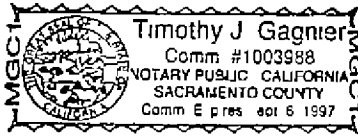
STATE OF CALIFORNIA)
) SS
COUNTY OF SAN JOAQUIN)

On July 23 1997 before me Timothy J Gagner
(Name Title of Officer)

personally appeared Benjamin S. Catlin (X) personally known to me or () proved to me on the basis of satisfactory evidence to be the person(s) whose name(e) is/are subscribed to the within instrument and acknowledged to me that he/she/they executed the same in his/her/their authorized capacity(ies) and that by his/her/their signature(s) on the instrument the person(s) or the entity upon behalf of which the person(s) acted executed the instrument

WITNESS my hand and official seal

Timothy J Gagner
Signature of Notary



OWNER

Be SC
Reno West Investors

OWNER ACKNOWLEDGMENT

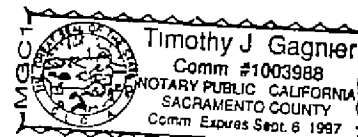
STATE OF CALIFORNIA)
) SS
COUNTY OF SAN JOAQUIN)

On July 23 1997 before me Timothy J Gagner
(Name Title of Officer)

personally appeared Benjamin S. Catlin (X) personally known to me or () proved to me on the basis of satisfactory evidence to be the person(s) whose name(e) is/are subscribed to the within instrument and acknowledged to me that he/she/they executed the same in his/her/their authorized capacity(ies) and that by his/her/their signature(s) on the instrument the person(s) or the entity upon behalf of which the person(s) acted executed the instrument

WITNESS my hand and official seal

Timothy J Gagner
Signature of Notary



OWNER

Be SC
Panattoni-Catlin Joint Venture VII, A California General Partnership

OWNER ACKNOWLEDGMENT

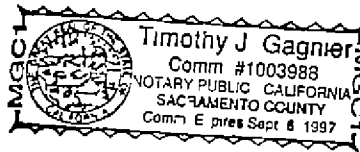
STATE OF CALIFORNIA)
) SS
COUNTY OF SAN JOAQUIN)

On July 23, 1997 before me Timothy J Gagner
(Name Title of Officer)

personally appeared Benjamin S Catlin () personally known to me or () proved to me on the basis of satisfactory evidence to be the person(s) whose name(e) is/are subscribed to the within instrument and acknowledged to me that he/she/they executed the same in his/her/their authorized capacity(ies) and that by his/her/their signature(s) on the instrument the person(s) or the entity upon behalf of which the person(s) acted executed the instrument

WITNESS my hand and official seal

Timothy J Gagner
Signature of Notary



OWNER

Linda M Stanley
Apollo Properties Inc

OWNER ACKNOWLEDGMENT

STATE OF CALIFORNIA)
) SS
COUNTY OF Sacramento) SS
COUNTY OF SAN JOAQUIN)

On July 21, 1997 before me Cathleen A Gipe, Notary Public
(Name Title of Officer)

personally appeared Linda M Stanley () personally known to me or () proved to me on the basis of satisfactory evidence to be the person(s) whose name(e) is/are subscribed to the within instrument and acknowledged to me that he/she/they executed the same in his/her/their authorized capacity(ies) and that by his/her/their signature(s) on the instrument the person(s) or the entity upon behalf of which the person(s) acted executed the instrument

WITNESS my hand and official seal

Cathleen A Gipe
Signature of Notary



CALIFORNIA ALL-PURPOSE ACKNOWLEDGMENT

No 5907

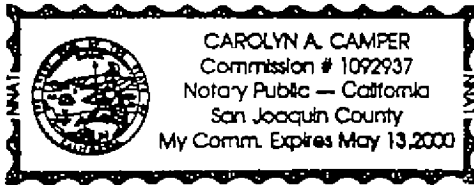
State of California

County of San Joaquin

On July 22, 1997 before me, Carolyn A Camper, Notary Public
DATE NAME TITLE OF OFFICER E.G. JANE DOE NOTARY PUBLIC

personally appeared Paul M Sensibaugh
NAME(S) OF SIGNER(S)

personally known to me - ~~OR~~ proved to me on the basis of satisfactory evidence to be the person(s) whose name(s) (s) (is) are subscribed to the within instrument and acknowledged to me that he/she/they executed the same in (his/hers/their) authorized capacity (yes) and that by (his/hers/their) signature(s) on the instrument the person(s) or the entity upon behalf of which the person(s) acted executed the instrument



WITNESS my hand and official seal

Carolyn A Camper
SIGNATURE OF NOTARY

OPTIONAL

Though the data below is not required by law it may prove valuable to persons relying on the document and could prevent fraudulent reattachment of this form

CAPACITY CLAIMED BY SIGNER

- INDIVIDUAL
- CORPORATE OFFICER

TITLE(S)

- PARTNER(S) LIMITED
- GENERAL
- ATTORNEY IN FACT
- TRUSTEE(S)
- GUARDIAN/CONSERVATOR
- OTHER _____

SIGNER IS REPRESENTING
NAME OF PERSON(S) OR ENTITY(IES)

DESCRIPTION OF ATTACHED DOCUMENT

TITLE OR TYPE OF DOCUMENT

NUMBER OF PAGES

DATE OF DOCUMENT

SIGNER(S) OTHER THAN NAMED ABOVE

EXHIBIT "A"

	PHASE A	PHASE B	PHASE C	PHASE D **
THRESHOLD TRIGGERS	Less than 65 acres and/or maximum of three sites (whichever is smaller)	65 acres or greater than 3 sites	200 acres or greater	Land use intensification as defined in note #4
DRAINAGE	Ditch storage (if calculations show) plus 135 000 square feet at 8 feet deep with ditch/basins	Addition 1 pump and redundant and extra storage	Full build out per master plans 2 pumps and redundant	
SANITARY SEWER *	Construct master line to Sperry Road		Full build out per master plans	
WATER *	Water line southeast of second temporary cul-de-sac			
STREETS	<p>Performance Drive connect to Sperry</p> <p>Ultimate Geometrics at Sperry Rd/ Performance Dr Intersection and Airport Way/Performance Dr Intersection</p> <p>Left turn west bound on Sperry to south-bound on Performance</p> <p>Underground signal conduits on Performance</p> <p>Signal at Airport Way/Performance Dr Intersection</p> <p>Railroad crossing *</p>	<p>Signal at Sperry Road/Performance Drive intersection</p> <p>Widen Sperry for 2 lanes in each direction plus turn lanes and transitions plus widening onto Arch Airport</p> <p>Railroad Crossings *</p>	<p>Full Frontage Improvements on Sperry Rd And Airport Way plus all internal streets</p> <p>Railroad Crossings *</p>	<p>Secondary Public Street access to Sperry Road east of Performance Drive and a minimum of 900' away from Airport Way and 900' away from Performance Drive</p>

Additional Deferred Improvement Agreement Requirements

- 1) The Owners Developer and/or Successors In Interest shall dedicate necessary right of way on Sperry Road and Airport Way to accommodate all identified improvements and mitigation measures as well as to conform to proposed road alignments and adopted Specific Plans - Phase A
- 2) Any deviation from the three initial agreed upon site locations shall necessitate moving to stage B
- 3) * Depending on site location Railroad crossings may be required in Phase A Phase B or Phase C
- 4) The total Non High Cube warehouse uses as defined by the City's Public Facilities Fee Program shall not exceed 37.5% of the 368 acres
** Intensification above this level assumed in the Cumulative Access Evaluation for Airport Gateway Business Park Dated June 14 1997 shall cause the Owners Developer and/or Successors-In Interest to construct the specified second access
- 5) All mitigation measures of COS EIR 3 82 and all conditions of TM 29 83 apply unless superseded by a more current EIR and/or TM document
- 6) ** Sewer water and storm drainage improvements are as defined in Interim Utility Plans prepared by Siegfried Engineering and dated _____

12 Please re file - main *AK/VA/4* SECRET 12

MEMORANDUM *FYI Dave*

FEB 6 1998

February 6 1998

TO Sam Mah Deputy Planning Director Community Development
Department/Planning Division
Attn Mike Niblock Senior Planner

FROM Ron L Cook Real Property Agent Housing and Redevelopment
Department

SUBJECT **ABANDONMENT - PORTION OF CROWN AVENUE AND RUNWAY
ROAD - MAP OF TERRA MATRE TRACT**

Please provide the appropriate environmental review for the above-referenced project as described below

Project Title Street abandonment

Applicant Reno West Investors-Stockton

Description/Location All that certain portion of 40 foot wide Runway Road between the Union Pacific Railroad and Crown Avenue(paper street) and all that portion of 40 foot wide Crown Avenue between Tidewater Southern Railroad and Runway Road (see attached map)

Also please make the required determination as to conformity to the General Plan in accordance with Section 65402 of the Government Code If you have questions regarding this request please contact me at 8063

STEVEN J PINKERTON, DIRECTOR

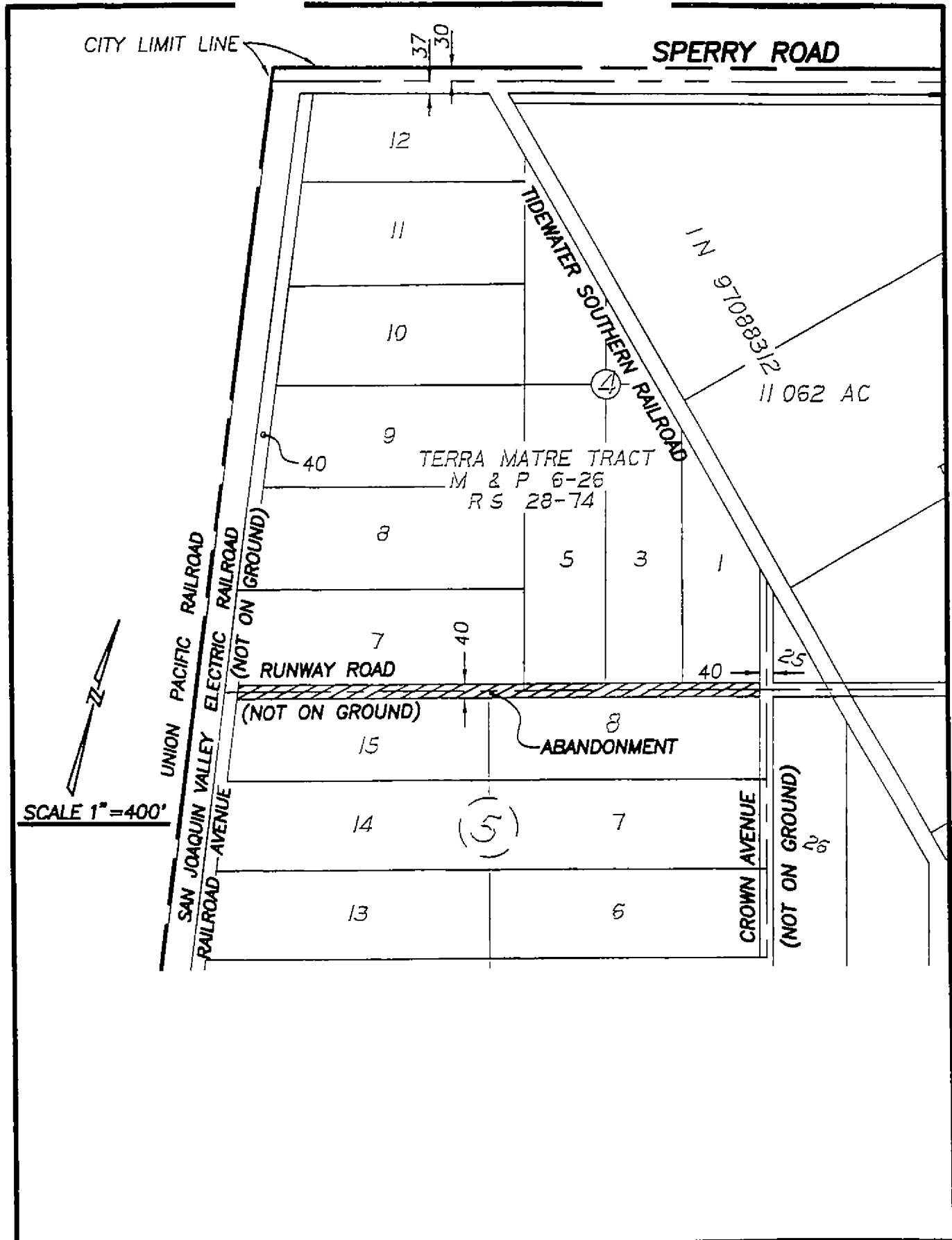


RON L COOK
REAL PROPERTY AGENT

RC/rc

Attachment

t \mikew\misc\cdenv mem



No	Revised	By
Drawn by		SV
Checked by		SRT
Scale		AS SHOWN

**AIRPORT GATEWAY CENTER
ABANDONMENT OF RUNWAY ROAD**

**CITY OF STOCKTON
DEPARTMENT OF PUBLIC WORKS**

Approved by	
	CITY ENGINEER
Date	
Drawing No	C-2583E

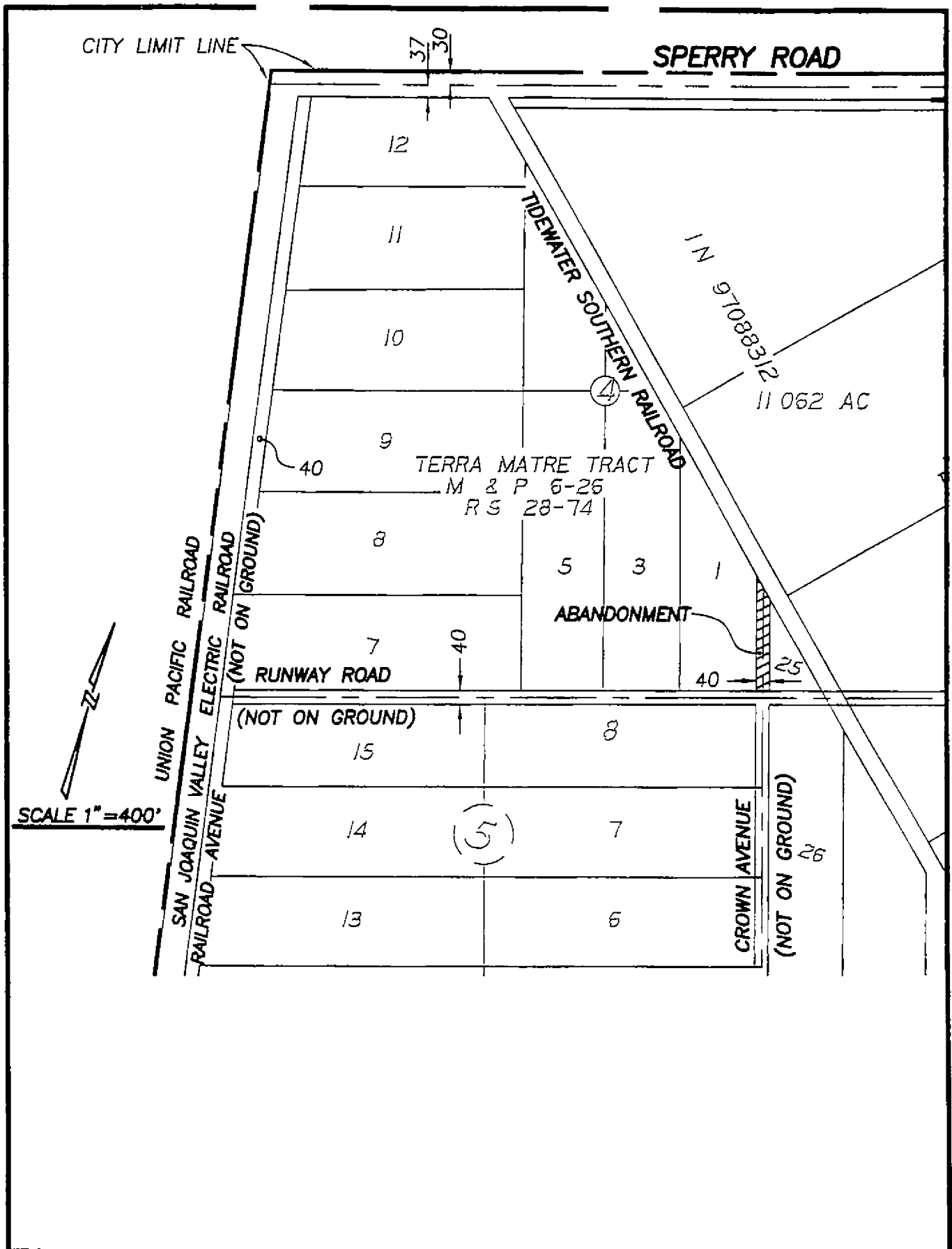
DESCRIPTION

ABANDONMENT OF RUNWAY ROAD

All that certain portion of Runway Road as shown on the Map of Terra Matre Tract filed for record in Books of Maps and Plats, Volume 6, Page 26, San Joaquin County Records, City of Stockton County of San Joaquin lying between the easterly right-of way line and it's projection of Railroad Avenue, 40 feet in width, as shown on said Map of Terra Matre Tract and the westerly right-of way line and it's projection of Crown Avenue, 40 feet in width, as shown on said Map of Terra Matre Tract

JAB
#97332
11/03/97

Stephen R. Thumlert, P L S #4334



No	Revised	By
Drawn by	SV	
Checked by	SRT	
Scale	AS SHOWN	

**AIRPORT GATEWAY CENTER
ABANDONMENT OF CROWN AVENUE**

**CITY OF STOCKTON
DEPARTMENT OF PUBLIC WORKS**

Approved by	
	CITY ENGINEER
Date	
Drawing No	C-2583F

DESCRIPTION

ABANDONMENT OF CROWN AVENUE

All that certain portion of Crown Avenue as shown on the Map of Terra Matre Tract filed for record in Books of Maps and Plats, Volume 6, Page 26 San Joaquin County Records, City of Stockton County of San Joaquin, lying between the southwesterly boundary of Tidewater Southern Railroad Right of-Way as shown on the map filed in Book of Surveys, Volume 28 Page 74, San Joaquin County Records and the northerly right-of-way line and its projection of Runway Road, 40 feet in width, as shown on said Map of Terra Matre Tract

JAB
#97332
10/27/97

Stephen R. Thumlert P L S #4334

This will be added as a note in the conditions following Condition 14

As it relates to the land use mix in Conditions 12 13 and 14 the trip generation assumptions derived for the various land use mixes formed the basis for the traffic study conducted for the Airport Gateway DEIR and were used to identify project impacts, mitigation measures and Conditions of Approval Should the land use thresholds identified in these conditions be exceeded or projected to be exceeded, the developer may retain the services of a qualified traffic engineer, as approved by the City to reevaluate the traffic characteristics and traffic impacts of land uses in Airport Gateway for comparison to the thresholds identified in the original traffic study Said new information may be used by the City to reevaluate the applicability of mitigation measures and associated on Conditions of Approval

Harrison will provide TRANSPARENCY

ECONOMIC REVIEW COMMITTEE MEETING

PROJECT AIRPORT GATEWAY

DATE APRIL 9, 1998

PLEASE SIGN IN

NAME	COMPANY/DEPARTMENT	PHONE NUMBER
SAM MAH	CD/PLANNING	937-8266
Gregg Messner	Public Works	937-8270
Kathy Tomura	Public Works	937-8690
Sally Break	Housing & Redevel	937-8132
Ron Cook	H.R.D.	937-8063
MIKE MALBOM	DET	474-2800
Bob Sivell	COS	937-8530
REED HOGAN	COS PW	937-8868
LAURA MALBOM	PANATTONI	911-485-8900
BRIAN VALUS	PANATTONI / APPLS	916-485-8900
DAVID STAGNARA	COS	937-8266
Sharon Parker	Econ Dev Div	937-8862
Lex Corrales	Siegfried Eng. Inc.	943-2021
Jim Billigmeier	" "	" "

CURRENT NUMBER OF EMPLOYEES NA

PROJECTED ADDITIONAL EMPLOYEES NA

1
2
3
4
5
6
7
8
9
10

RE/DK
300RN
O RL

Recorded on 07/23/97

Inst # 97071908

Smt

After Recording Return To
City of Stockton
Public Works Dept.
425 N El Dorado Street
Stockton, CA 95202

DEFERRED IMPROVEMENT AGREEMENT

OWNERS

Panattoni-Catlin Venture

Reno West Investors

Panattoni-Catlin Joint Venture VII
A California General Partnership,

Apollo Properties, Inc

ADDRESS

3620 Fair Oaks Blvd , Suite 150
Sacramento, CA 95864

THIS AGREEMENT is made and entered into at Stockton, California, this 24th day of JUNE 1997, by and between

Panattoni-Catlin Venture, Reno West Investors, Panattoni Catlin Joint Venture VII, A California General Partnership, and Apollo Properties, Inc, hereinafter referred to as "Owners", and the CITY OF STOCKTON, a Municipal Corporation, located in the County of San Joaquin State of California hereinafter referred to as "City "

WHEREAS Owners own real property in the City of Stockton, County of San Joaquin State of California, and more particularly described as follows

That certain real property situated in the City of Stockton, County of San Joaquin, State of California, described as follows

All that certain real property known as Terra Matre Tract as shown on the map filed for record in Book of Maps and Plats, Volume 6 Page 26, San Joaquin Records

WHEREAS Owners are desirous of deferring construction of said improvements associated with the above described parcel

WHEREAS said improvements to be deferred are the following

SEE EXHIBIT "A"

CONSISTING OF BUT NOT LIMITED TO The design and installation of facilities listed in Exhibit "A"

WHEREAS it is in the interest of the City to defer completion of said frontage improvements at this time, pursuant to the authority contained in Stockton Municipal Code Section 16-174 1, because of the lack of frontage improvements on adjacent parcels

NOW THEREFORE, IT IS MUTUALLY STIPULATED AND AGREED

1 That said construction is deferred until required by the City pursuant to Exhibit "A" Such determination shall be made by the City Engineer based upon the totality of circumstances associated with the parcel such as adjacent development changed conditions of the adjacent area, etc

2 That, upon notification by the City, the Owners, or Owners' successors or assigns will engage a civil engineer licensed in the State of California to design and prepare construction drawings for the required improvements to be approved by the City Engineer Said drawings shall be submitted within 90 days after notification and construction shall commence within 30 days after approval of the construction drawings

3 That the Owners, or Owners' successors or assigns shall pay to the City all plan checking, processing and inspection fees pertaining to the improvements associated with this specific agreement.

4 That construction of said improvements shall be in accordance with Chapter 16, Division 4 of the City of Stockton, Municipal Code

5 That in event of default by Owners, or Owners' successors or assigns the City is hereby authorized to cause said construction to be done and charge the entire cost and expense to Owners, or Owners' successors or assigns including interest thereon at the maximum legal rate from the date of notice of said cost and expense until paid Such costs or expenses shall constitute a lien upon the property

6 That this agreement shall be recorded in the office of the Recorder of San Joaquin County, California, at the expense of Owners and shall constitute notice to all successors

and assigns of the title to said real property of the obligation herein set forth, and also a lien in such amount as will fully reimburse the City, including interest as hereinabove set forth, subject to foreclosure in event of default in payment.

7 That in event of litigation occasioned by any default of Owners, or Owners' successors or assigns Owners, or Owners' successors or assigns agree to pay all costs involved, including reasonable attorney's fees, and that the same shall become a part of the lien against said real property

8 That the term "Owners" shall include not only the present Owners but also heirs, successors, executors, administrators and assigns, it being the intent of the parties hereto that the obligations herein undertaken shall run with said real property and constitute a lien there against.


9 The obligations herein undertaken shall be binding upon the heirs, successors, executors, administrators and assigns of the parties hereto

IN WITNESS WHEREOF, the parties hereto have affixed their signatures as of the date first written above

APPROVED AS TO FORM

CITY OF STOCKTON


Deputy City Attorney

BY 
PAUL M SENSIBAUGH
DEPUTY PUBLIC WORKS
DIRECTOR/CITY ENGINEER

OWNER.

BSC
Panattoni-Catlin Venture

OWNER ACKNOWLEDGMENT

STATE OF CALIFORNIA)
) SS
COUNTY OF SAN JOAQUIN)

On July 23 1997 before me Timothy J. Gagnier
(Name Title of Officer)

personally appeared Benjamin S. Catlin, (X) personally known to me or () proved to me on the basis of satisfactory evidence to be the person(s) whose name(e) is/are subscribed to the within instrument and acknowledged to me that he/she/they executed the same in his/her/their authorized capacity(ies) and that by his/her/their signature(s) on the instrument the person(s) or the entity upon behalf of which the person(s) acted executed the instrument.

WITNESS my hand and official seal

Timothy J. Gagnier
Signature of Notary



OWNER

BSC
Reno West Investors

OWNER ACKNOWLEDGMENT

STATE OF CALIFORNIA)
) SS
COUNTY OF SAN JOAQUIN)

On July 23 1997 before me Timothy J. Gagnier
(Name Title of Officer)

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WITNESS my hand and official seal

Timothy J. Gagnier
Signature of Notary



OWNER

Be 80
Panatoni-Catlin Joint Venture VII, A California General Partnership

OWNER ACKNOWLEDGMENT

STATE OF CALIFORNIA)
) SS
COUNTY OF SAN JOAQUIN)

On July 23 1997 before me, Timothy J Gagnier,
(Name, Title of Officer)

personally appeared Benjamin S Catlin, () personally known to me or () proved to me on the basis of satisfactory evidence to be the person(s) whose name(e) is/are subscribed to the within instrument and acknowledged to me that he/she/they executed the same in his/her/their authorized capacity(ies), and that by his/her/their signature(s) on the instrument the person(s), or the entity upon behalf of which the person(s) acted executed the instrument.

WITNESS my hand and official seal

Timothy J Gagnier
Signature of Notary



OWNER

Linda M Straley
Apollo Properties, Inc

OWNER ACKNOWLEDGMENT

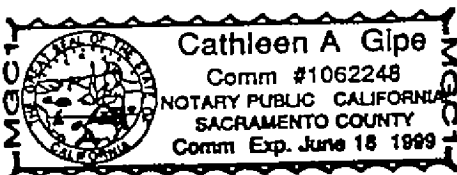
STATE OF CALIFORNIA)
) SS
COUNTY OF Sacramento SAN JOAQUIN)

On July 24 1997 before me Cathleen A Gipe, Notary Public
(Name Title of Officer)

personally appeared Linda M Straley, () personally known to me or () proved to me on the basis of satisfactory evidence to be the person(s) whose name(e) is/are subscribed to the within instrument and acknowledged to me that he/she/they executed the same in his/her/their authorized capacity(ies) and that by his/her/their signature(s) on the instrument the person(s) or the entity upon behalf of which the person(s) acted executed the instrument.

WITNESS my hand and official seal

Cathleen A Gipe
Signature of Notary



CALIFORNIA ALL-PURPOSE ACKNOWLEDGMENT

No. 5907

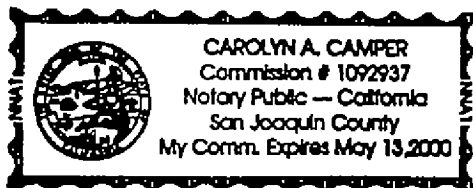
State of California

County of San Joaquin

On July 22, 1997 before me, Carolyn A. Camper, Notary Public
DATE NAME, TITLE OF OFFICER E.G., JANE DOE, NOTARY PUBLIC

personally appeared Paul M. Sensibaugh
NAME(S) OF SIGNER(S)

personally known to me - ~~OR~~ approved to me on the basis of satisfactory evidence to be the person(s) whose name(s) (s) is/are subscribed to the within instrument and acknowledged to me that he/she/they executed the same in (his/hers/their) authorized capacity(ies), and that by (his/hers/their) signature(s) on the instrument the person(s), or the entity upon behalf of which the person(s) acted, executed the instrument



WITNESS my hand and official seal

Carolyn A. Camper
SIGNATURE OF NOTARY

OPTIONAL

Though the data below is not required by law it may prove valuable to persons relying on the document and could prevent fraudulent reattachment of this form

CAPACITY CLAIMED BY SIGNER

- INDIVIDUAL
- CORPORATE OFFICER

TITLE(S)

- PARTNER(S) LIMITED GENERAL
- ATTORNEY-IN-FACT
- TRUSTEE(S)
- GUARDIAN/CONSERVATOR
- OTHER _____

DESCRIPTION OF ATTACHED DOCUMENT

TITLE OR TYPE OF DOCUMENT

NUMBER OF PAGES

DATE OF DOCUMENT

SIGNER IS REPRESENTING
NAME OF PERSON(S) OR ENTITY(IES)

SIGNER(S) OTHER THAN NAMED ABOVE

EXHIBIT A"

	PHASE A	PHASE B	PHASE C	PHASE D **
THRESHOLD TRIGGERS	Less than 65 acres and/or maximum of three sites (whichever is smaller)	65 acres or greater than 3 sites	200 acres or greater	Land use intensification as defined in note #4
DRAINAGE *	Ditch storage (if calculations show) plus 135,000 square feet at 8 feet deep with ditch/basins	Addition 1 pump and redundant and extra storage	Full build out per master plans 2 pumps and redundant	
SANITARY SEWER* *	Construct master line to Sperry Road		Full build out per master plans	
WATER	Water line southeast of second temporary cul-de-sac			
STREETS	Performance Drive connect to Sperry Ultimate Geometrics at Sperry Rd/ Performance Dr intersection and Airport Way/Performance Dr intersection Left turn west-bound on Sperry to south-bound on Performance Underground signal conduits on Performance Signal at Airport Way/Performance Dr intersection Railroad crossing *	Signal at Sperry Road/Performance Drive Intersection Widen Sperry for 2 lanes in each direction plus turn lanes and transitions plus widening onto Arch Airport Railroad Crossings *	Full Frontage Improvements on Sperry Rd And Airport Way plus all internal streets Railroad Crossings *	Secondary Public Street access to Sperry Road or of Performance Drive and a minimum of 900' away from of Airport Way and 900' away from Performance Drive

Additional Deferred Improvement Agreement Requirements

- 1) The Owners Developer and/or Successors-In Interest shall dedicate necessary right-of-way on Sperry Road and Airport Way to accommodate all identified improvements and mitigation measures as well as to conform to proposed road alignments and adopted Specific Plans - Phase A.
- 2) Any deviation from the three initial agreed upon site locations shall necessitate moving to stage B
- 3) * Depending on site location Railroad crossings may be required in Phase A Phase B or Phase C
- 4) The total Non High Cube warehouse uses as defined by the City's Public Facilities Fee Program shall not exceed 37.5% of the 368 acres
* Intensification above this level assumed in the Cumulative Access Evaluation for Airport Gateway Business Park Dated June 14 1997 shall cause the Owners Developer and/or Successors-In Interest to construct the specified second access
- 5) All mitigation measures of COS EIR 3-82 and all conditions of TM 29-83 apply unless superseded by a more current EIR and/or TM document.
- 6) *** Sewer water and storm drainage improvements are as defined in Interim Utility Plans prepared by Siegfried Engineering and dated _____

InSite Environmental

Fax Memo

TO Jenny Liaw

FROM CHARLIE SIMPSON

PAGES 3, INCLUDING COVER

RE Airport Gateway

DATE 8/4/97

HARD COPY WILL OR WILL NOT FOLLOW BY MAIL

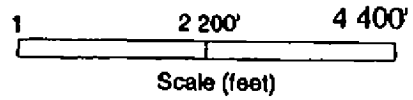
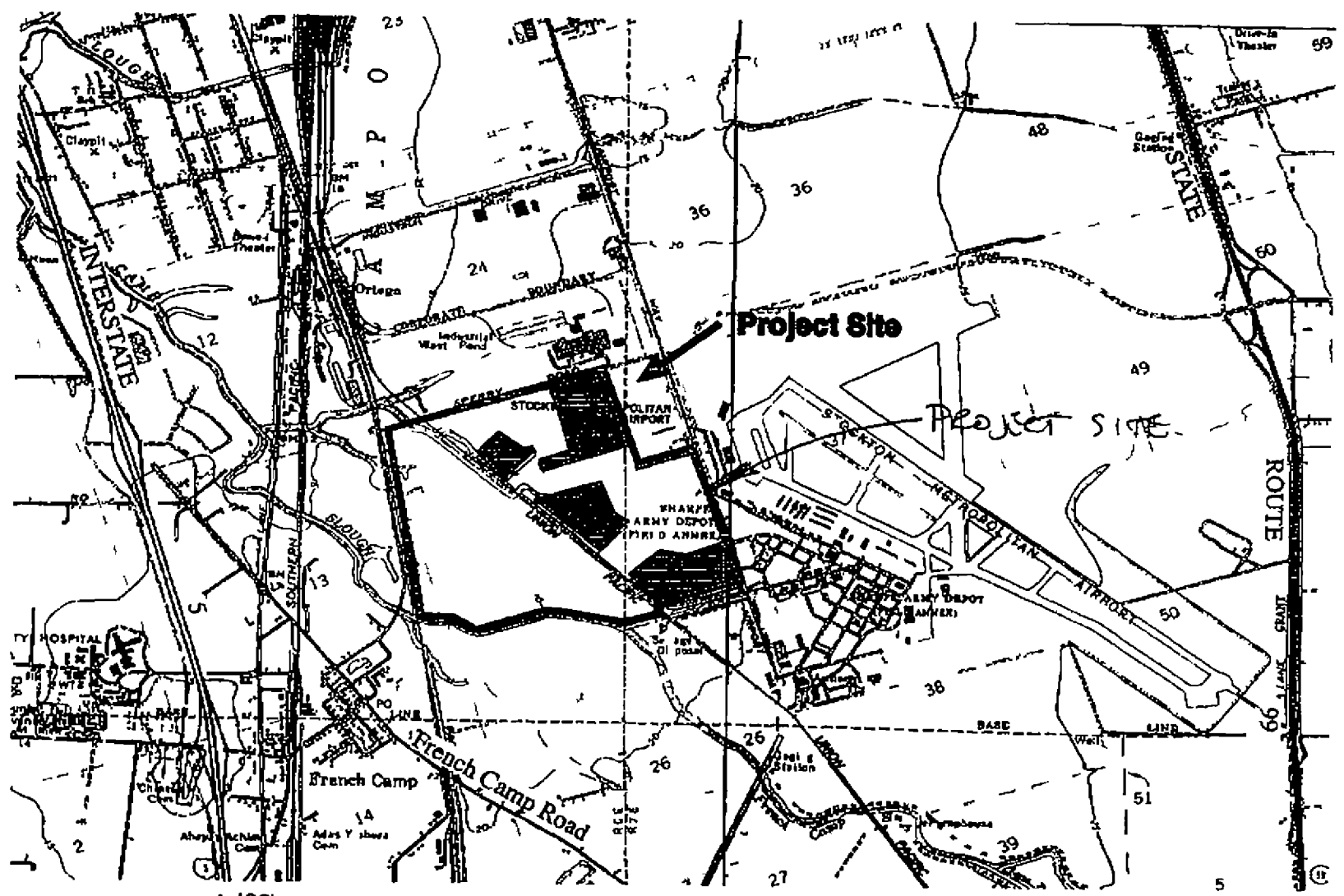


Jenny -

This is a working copy of the sites we are working with. We've assumed 328 acres which excludes Noll, Dopaco, Lever, & Feralloy.

We were planning to submit the Admin Draft today. Please let me know ASAP if staff's concept for this EIR disagrees with ours.

Charlie



Source (basemap) USGS 7.5 Quadrangles
 Stockton West Stockton East



*area should be by
 consistent lettering*

Figure 2
USGS LOCATION MAP

SIMPSON MOORE, INC



CITY HALL
STOCKTON CA 95202
COMMUNITY DEVELOPMENT DEPARTMENT
(209) 944-8266

March 20, 1984

Stockton Airport Industrial, Ltd
c/o TMI
P O Box 2500
Newport Beach, CA 92660

APPROVED TENTATIVE MAP - STOCKTON AIRPORT INDUSTRIAL, LTD
(AIRPORT GATEWAY CENTER) (AMENDED TM29-83)

At its regular meeting of March 8, 1984, the City Planning Commission approved your request to subdivide 447 acres at the southwest corner of Sperry Road and Airport Way, across from the Stockton Metropolitan Airport (Airport Gateway Center) (Amended TM29-83)

This approval is subject to the following conditions

1. Comply with applicable State, County and City codes and regulations and adopted standards
2. Soils reports shall be required prior to any submission of plans for construction
3. Street names shall be approved by the Community Development Director
4. A complete master storm water drainage system, including a detention basin, shall be provided by the owner/developer as described in the engineering report. On-site storm water drains and the detention basin shall be sized to provide adequate capacity for the tributary area. Provisions to serve off-site parcels, such as enlargement of the detention basin, shall be at the expense of the owners/developers of the off-site parcels

Stockton Someplace Special!

The owners shall be responsible for maintenance of the detention basin. When further development of the off-site properties within the tributary area occurs, the City shall require the owners/developers of these parcels to pay an equitable share of the cost of maintaining the drainage system, including the detention basin. The boundaries of the tributary area have been refined, based on data supplied by the applicant, and have been field reviewed by the Department of Public Works staff and are shown in the addendum to EIR 3-82.

The owner/developer shall provide and have approved any interim storm drain plan for phased development. Said approval shall precede approval of the final map.

3

5 If any industries store, transport or use toxic substances on the subdivision site, sufficient design measures shall be incorporated to keep spills of such materials from entering French Camp Slough or the subdivision drainage system. Such design measures shall be approved by the City during the design review process and by the Water Quality Control Board before a Waste Discharge Permit is granted.

4

6 The owner/developer shall obtain permits and shall comply with all requirements of all government agencies with jurisdiction over French Camp Slough including, but not limited to, dedication of right-of-way, access easements, fencing, outfall structure and necessary improvement of French Camp Slough.

7 All building pads shall be raised to conform to flood insurance requirements and City Flood Plain Management Ordinance.

5

8 The owner/developer shall participate in an Area of Benefit for South Industrial Sanitary Sewer Trunk Line.

9 The available capacity for this development in the 27 inch Southern Industrial Sanitary Sewer Trunk Line is very limited. A new sanitary sewer line may be necessary in the future when this area is fully developed. Any sanitary sewer connections to this line must be approved by the Municipal Utilities Department.

10 The area of the subject map shall be annexed to the City of Stockton prior to approval of the final map.

11. Traffic signals shall be required at the following locations and owner/developer participation shall be as indicated

a	Airport Way/Arch Sperry Road	25%
b	Airport Way/C E Dixon-Aviation Way	50%
c	Sperry Road/Hughes Street (required only when Sperry Road is extended westerly of site)	100%
d	Sperry Road/Aviation Way	75%
e	Sperry Road/Belding Street	75%
f	Aviation Way/Lindberg Street	100%
g	Aviation Way/Belding	100%

These traffic signals shall be installed when warranted or at the discretion of the Director of Public Works

12 The owner/developer shall be responsible for all necessary on-site improvements, with the exception of those improvements to be funded through assessment districts or Area of Benefit, which will spread the cost equitably to benefiting owners. In addition, the owner/developer shall contribute to the off-site improvements specified below

- a The owner/developer shall provide three southbound lanes on Airport Way along the tract frontage. A minimum of a 62 foot half section along the tract frontage shall be dedicated to the City of Stockton. Flaring of intersections will be required to accommodate projected traffic volumes, an additional 7 foot dedication will be required for intersection flares. Flaring of the north leg of the Airport Way/Sperry Road intersection will also be necessary and shall be provided by the owner/developer, the City shall attempt to recover a proportionate share of the costs from future developments
- b The owner/developer shall construct full improvements on Sperry Road including four lanes, a raised median with turning lanes (dual left-turn lanes at Airport Way), and intersection flares as required. A 67 foot half section and 134 foot full cross section along the proposed alignment of Sperry Road shall be dedicated to the City of Stockton. Additional dedication will be required for intersection flares. Construction of additional lanes shall be funded through an assessment district or Area of Benefit

If the owner/developer elects to finance construction of the grade separation crossing of the Tidewater Southern Railroad prior to the formation of an assessment district, all costs above the landowner's/developer's assessment shall be credited fully toward equitable participation in any assessment district or Area of Benefit formed to finance improvements to the Sperry Road/Arch Airport Road corridor

The proposed grade separation structure over the Tidewater Southern Railroad shall have a 100 foot full cross section and will contain six lanes, an 8 foot median and sidewalks

- c The owner/subdivider shall participate equitably in any assessment district or Area of Benefit formed to construct Sperry Road or Industrial Drive westerly to Interstate 5/French Camp Turnpike Road including railroad overcrossings, waterway bridges, and Interstate 5 interchange improvements
 - d The owner/subdivider shall participate equitably in any assessment district or Area of Benefit formed to realign and/or widen Arch-Airport Road easterly of Airport Way to provide a 6 lane cross section
 - e The owner/subdivider shall participate equitably in any assessment district or Area of Benefit formed to widen or reconstruct the Arch-Airport Road overcrossing at the interchange with State Route 99
- 13 The owner/developer shall include and adopt a statement in the Conditions, Covenants and Restrictions (CC&Rs) stating that all owners/developers of the subdivision lots must participate in the implementation of a Transportation Systems Management (TSM) Program when requested to do so by the City of Stockton. Possible TSM measures include appointing a transportation coordinator to organize car and van pools, implementing flex-time or staggered work shifts, providing preferential parking to car-pooling vehicles, and other similar measures
- 14 The owner/developer shall dedicate access rights to the City of Stockton at the following locations along the west side of Airport Way, except at the Lindberg

Street and Aviation Way intersections, along the north and south sides of Sperry Road, except at the Belding Street, Aviation Way and Hughes Street intersections, along Belding Street, Aviation Way and Lindberg Street and Hughes Street within 100 feet of Airport Way and Sperry Road, and along Runway Street and Old Sperry Road within 200 feet of the centerline of the Tidewater Southern Railroad right-of-way Direct lot access in these areas shall be prohibited

- 15 The owner/developer shall be responsible for the construction of crossing protection to PUC standards at the at-grade crossing of the Tidewater Southern Railroad by Runway Street. No building shall be located within a sight triangle extending 400 feet down the centerline of the track from each side of the street and 100 feet down the street from each side of the railroad crossing
- 16 All requirements of the Public Utilities Commission pertaining to building setbacks along railroad spur lines shall be the responsibility of the owner/developer
- 17 Before a detailed site plan is developed, the applicable railroad company shall be consulted regarding specific requirements for spur line development
- 18 Until the westerly extension of Sperry Road to Interstate 5 is constructed, the owner/developer shall retain the option of extending Hughes Street to the existing Sperry Road alignment and improving the at-grade crossing of the Tidewater Southern Railroad
- 19 Lindberg Street shall be limited to right-turn access only to Airport Way The existing median opening shall be closed per the Stockton Metropolitan Airport Specific Plan Amendments for Airport Way
- 20 All mitigation measures listed in the Airport Gateway Center Final Environmental Impact Report (EIR 3-82, SCH#82120217) within the purview of the City of Stockton (i.e., Sections IV 4, 7, 8, 11, 16, 17, and 18 of this EIR) are adopted by this reference as conditions of approval for the tentative subdivision map Implementation shall be the responsibility of the owner/developer unless otherwise assigned in the mitigation measures

Stockton Airport Industrial, Ltd.
March 20, 1984
Page 6

*2 yrs after the Amortization is effective -
AUG 31, 1984*

Enclosed is a copy of the approved tentative map which will expire on March 8, 1986. Any request for an extension shall be made in writing, filed with this office and accompanied by the appropriate fee at least thirty (30) days prior to expiration of the tentative map and clearly stating the reasons for requesting the extension. In granting an extension, new conditions may be imposed and existing conditions may be revised.

If you have any questions, please contact Mr. Sam Mah of the Community Development Department, Planning Division, at (209) 944-8266.

John Carlson

JOHN CARLSON, SECRETARY
CITY PLANNING COMMISSION

JC kmg

Enclosure

cc Randall O'Dell w/enclosure
1581 Cummins Drive, Suite 123
Modesto, CA 95351

Luis Manzano and Associates w/enclosure
31320 Via Colinas, Suite 104
West Lake Village, CA 91352

Steve Herum w/enclosure
P O Drawer 20
Stockton, CA 95201

Robert P. Flick w/enclosure
2049 Century Park East
Los Angeles, CA 90067

John Erickson w/enclosure
4560 Enchanted Way
Redding, CA 96001

Planning Associates w/enclosure
P O Box 1527
Redding, CA 96009

Public Works w/sepia



STOCKTON CITY PLANNING COMMISSION
NOTICE OF PUBLIC HEARING

PERMIT CENTER 345 NORTH EL DORADO STREET
STOCKTON CALIFORNIA 95202
TELEPHONE (209) 937 8266

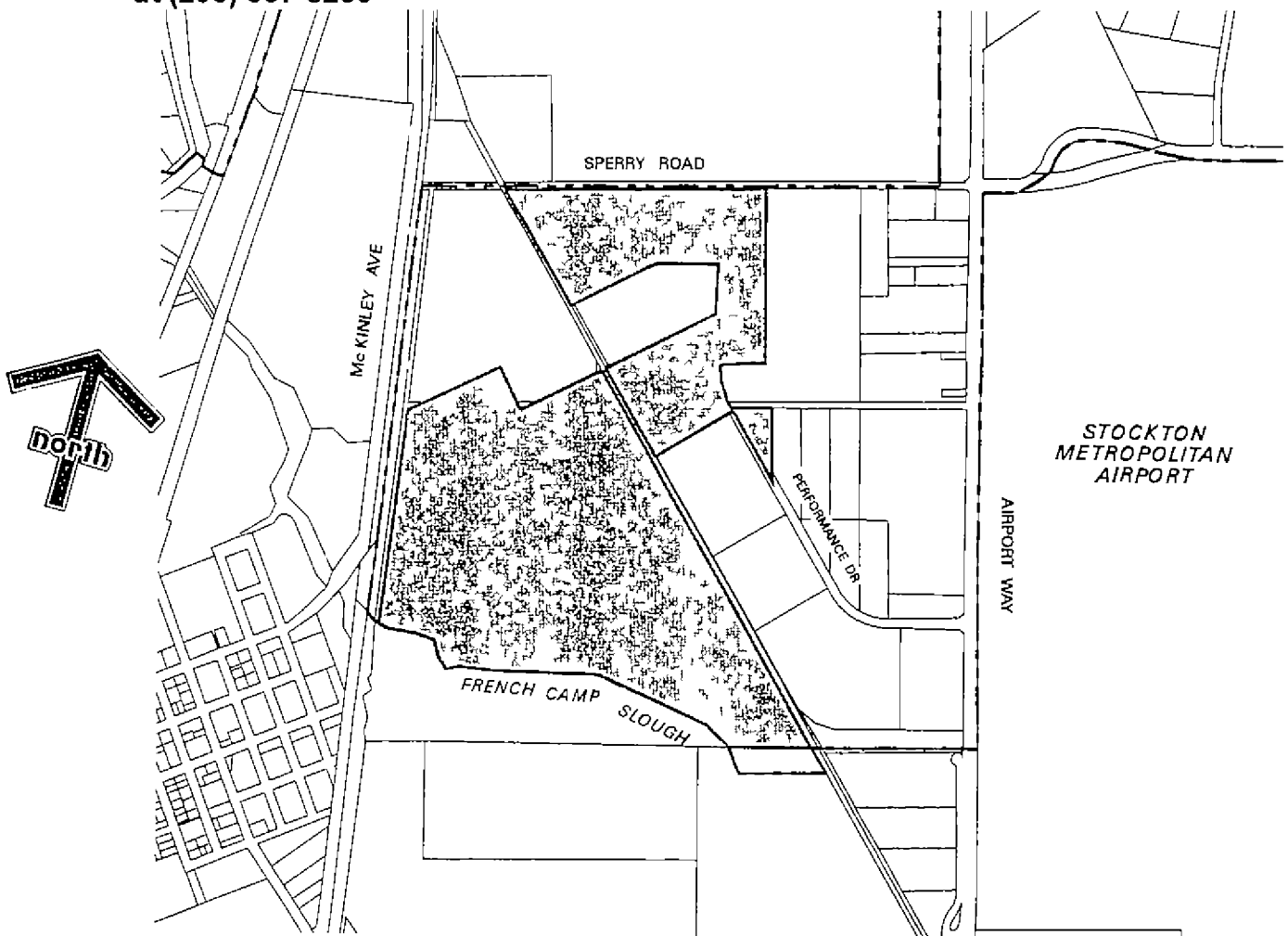
DATE THURSDAY, APRIL 23, 1998

TIME 7 P M OR AS SOON THEREAFTER AS THE MATTER CAN BE HEARD

PLACE COUNCIL CHAMBERS, SECOND FLOOR, CITY HALL, 425 NORTH EL DORADO STREET

SUBJECT Hearing regarding the request of Reno West Investors, et al, for a tentative map to subdivide 190.87± acres into twenty-five parcels varying in size from 1.75 to 29 acres located south of Sperry Road, west of Airport Way, north of French Camp Slough and east of McKinley Avenue (TM3-97--Airport Gateway Center)

Should you have any questions, please contact Associate Planner David Stagnaro at (209) 937-8266



ALL PROCEEDINGS BEFORE THE CITY PLANNING COMMISSION ARE CONDUCTED IN ENGLISH THE CITY OF STOCKTON DOES NOT FURNISH INTERPRETERS AND IF ONE IS NEEDED IT SHALL BE THE RESPONSIBILITY OF THE PERSON NEEDING ONE

ANYONE WISHING TO BE HEARD ON THE ISSUE MAY APPEAR BEFORE THE CITY PLANNING COMMISSION AT THE TIME OF THE PUBLIC HEARING IT DESERVES TO BE NOTED THAT THE LEGAL REQUIREMENT OF PUBLIC NOTICE DOES NOT GUARANTEE NOTICE TO ALL PERSONS RESIDING IN OR OTHERWISE USING PROPERTY IN THE GENERAL VICINITY OF THE PROPERTY IN QUESTION ACCORDINGLY EACH RESIDENT OF THIS NOTICE IS RESPECTFULLY REQUESTED TO BRING THIS NOTICE PROMPTLY TO THE ATTENTION OF ANY OTHER PERSON OR PERSONS WHOM THE RECIPIENT FEELS MAY BE INTERESTED IN OR AFFECTED BY THIS PROPOSAL IN ORDER THAT ALL PERSONS MAY BE GIVEN AN OPPORTUNITY TO BE HEARD ON THE ISSUE

IF YOU CHALLENGE THE PROPOSED ACTION IN COURT YOU MAY BE LIMITED TO RAISING ONLY THOSE ISSUES YOU OR SOMEONE ELSE RAISED AT THE PUBLIC HEARING DESCRIBED IN THIS NOTICE OR IN WRITTEN CORRESPONDENCE DELIVERED TO THE PLANNING COMMISSION AT OR PRIOR TO THE PUBLIC HEARING

Date Mailed April 10, 1998

reno west investors, etal
Tm 3-97

APN 17704007
BILGE BEVERLEY MARY
78 CHIPPEWA
CLAWSON MI 48017

502 5/07

APN 17704012
REYNOSO VINCENT J JR &
4807 S AIRPORT WAY
STOCKTON CA 95206

APN 17704015
BROOKE CLAUDE L TR
16566 SELBY DR
SAN LEANDRA CA 94578

APN 17704018
RENO WEST INVESTORS PTP
3620 FAIR OAKS BLVD #150
SACRAMENTO CA 95864

APN 17 04019
RENO WEST INVESTORS PTP
3620 FAIR OAKS BLVD #150
SACRAMENTO CA 95864

APN 17704020
PERRY GORDON R & PAM
370 TUMBLEWEED CT
FREMONT CA 94539

APN 17704021
NAKAMURA PATRICIA A ETA
3641 PORTSMOUTH CIR
STOCKTON CA 95219

APN 17704022
RENO WEST INVESTORS PTP
3620 FAIR OAKS BLVD #150
SACRAMENTO CA 95864

APN 17704023
RENO WEST INVESTORS PTP
3620 FAIR OAKS BLVD #150
SACRAMENTO CA 95864

APN 17704024
SOLLA CONRADO A EST
4651 S AIRPORT WAY
STOCKTON CA 95206

APN 17704025
CABALAR E JR & S L
10359 AMIEL WAY
LINDEN CA 95236

APN 17704026
UNION PACIFIC CORPORATION
1700 FARNAM ST 10TH FLR SOUTH
OMAHA NE 68102

APN 17704027
SWINERTON & WALBERG CO COR
580 CALIFORNIA ST
SAN FRANCISCO CA 94104

APN 17704028
SWINERTON & WALBERG CO COR
580 CALIFORNIA ST
SAN FRANCISCO CA 94104

APN 17704030
SWINERTON & WALBERG CO CORP
580 CALIFORNIA ST
SAN FRANCISCO CA 94104

APN 17704030
UNION PACIFIC CORPORATION
1700 FARNAM ST 10TH FLR SOUTH
OMAHA NE 68102

APN 17704032
RENO WEST INVESTORS PTP
3620 FAIR OAKS BLVD #150
SACRAMENTO CA 95864

APN 17704033
SWINERTON & WALBERG CO CORP
580 CALIFORNIA ST
SAN FRANCISCO CA 94104

APN 17704034
UNION PACIFIC CORPORATION
1700 FARNAM ST 10TH FLR SOUTH
OMAHA NE 68102

APN 17704035
UNION PACIFIC CORPORATION
1700 FARNAM ST 10TH FLR SOUTH
OMAHA NE 68102

APN 17704036
UNION PACIFIC CORPORATION
1700 FARNAM ST 10TH FLR SOUTH
OMAHA NE 68102

APN 17704037
UNION PACIFIC CORPORATION
1700 FARNAM ST 10TH FLR SOUTH
OMAHA NE 68102

APN 17704042
RENO WEST INVESTORS PTP
3620 FAIR OAKS BLVD #150
SACRAMENTO CA 95864

APN 17704045
PANATTONI CATLIN J/V VII PTP
3620 FAIR OAKS BLVD SUITE 150
SACRAMENTO CA 95864

APN 17704050
NOLL MFG CO CORP
PO BOX 6010
SAN PABLO CA 94806

APN 17704052
APOLLO PROPERTIES INC
8395 JACKSON RD SUITE E
SACRAMENTO CA 95826

APN 17704053
APOLLO PROPERTIES INC
8395 JACKSON RD SUITE E
SACRAMENTO CA 95826

APN 17704054
APOLLO PROPERTIES INC ETAL
8395 JACKSON RD SUITE E
SACRAMENTO CA 95826

APN 17704064
RENO WEST INVESTORS PTP
3620 FAIR OAKS BLVD #150
SACRAMENTO CA 95864

APN 17704065
RENO WEST INVESTORS PTP
3620 FAIR OAKS BLVD #150
SACRAMENTO CA 95864

223

Tm 3-97

APN 17704066
RENO WEST INVESTORS PTP
3620 FAIR OAKS BLVD #150
SACRAMENTO CA 95864

APN 17705003
BEATTIE PATRICIA ETAL
P O BOX 219
FRENCH CAMP CA 95231

APN 17705004
GRAHAM JOEL
6724 PLYMOUTH RD #8
STOCKTON CA 95207

APN 17705004
GRAHAM JOEL
6724 PLYMOUTH RD #8
STOCKTON CA 95207

APN 17705005
NIELSEN PTP
PO BOX 49
GAZELLE CA 96034

APN 17705017
UNION PACIFIC CORPORATION
1700 FARNAM ST 10TH FLR SOUTH
OMAHA NE 68102

APN 17705022
UNION PACIFIC CORPORATION
1700 FARNAM ST 10TH FLR SOUTH
OMAHA NE 68102

APN 17705023
UNION PACIFIC CORPORATION
1700 FARNAM ST 10TH FLR SOUTH
OMAHA NE 68102

APN 17705028
CF PROPERTIES INC
PO BOX 3175
PORTLAND OR 97208

APN 17728018
UNION PACIFIC CORPORATION
1700 FARNAM ST 10TH FLR SOUTH
OMAHA NE 68102

APN 17728036
VALIMET INC
PO BOX 6186
STOCKTON CA 95206

APN 17723037
VALIMET INC
431 SPERRY RD
STOCKTON CA 95206

APN 17728038
UNION PACIFIC CORPORATION
1700 FARNAM ST 10TH FLR SOUTH
OMAHA NE 68102

APN 17728039
J M MANUFACTURING
9 PEACH TREE HILL RD
LIVINGSTON NJ 07039

APN 1930 012
NAKAMURA PATRICIA A ETA
3641 PORTSMOUTH CIR
STOCKTON CA 95219

APN 19302034
BORDENAVE ROBERT LUCIEN
PO BOX B
FRENCH CAMP CA 95231

APN 19302035
SP PACIFIC PROPERTIES INC
201 MISSION ST 3RD FLOOR
SAN FRANCISCO CA 94105

U S Fish and Wildlife Service
3310 El Camino Avenue #130
Sacramento, CA 95821-6340

222

OCCUPANT /
4649 S AIRPORT WY
STOCKTON CA 95206

APN 17704307

OCCUPANT /
4649 S AIRPORT WY
STOCKTON CA 95206

APN 17704007

OCCUPANT /
4807 S AIRPORT WY
STOCKTON CA 95206

APN 17704012

OCCUPANT /
4847 S AIRPORT WY
STOCKTON CA 95206

APN 17704015

OCCUPANT /
4863 S AIRPORT WY
STOCKTON CA 95206

APN 17704015

OCCUPANT /
4865 S AIRPORT WY
STOCKTON CA 95206

APN 17704015

OCCUPANT /
4877 S AIRPORT WY
STOCKTON CA 95206

APN 17704015

OCCUPANT / *Vacant 4/98*
4887 S AIRPORT WY
STOCKTON CA 95206

APN 17704015

OCCUPANT /
4897 S AIRPORT WY
STOCKTON CA 95206

APN 17704015

OCCUPANT /
0
STOCKTON CA 95206

APN 17704015

OCCUPANT /
6922 S MCKINLEY AV
STOCKTON CA 95206

APN 17704018

OCCUPANT /
6810 S MCKINLEY AV
STOCKTON CA 95206

APN 17704019

OCCUPANT /
8993 S AIRPORT WY
STOCKTON CA 95231

APN 17704020

OCCUPANT /
6944 S MCKINLEY AV
STOCKTON CA 95206

APN 17704021

OCCUPANT / *FOR 4/98*
7010 S REDWOOD ST
STOCKTON CA 95231

APN 17704021

OCCUPANT /
6708 S MCKINLEY AV
STOCKTON CA 95206

APN 17704022

OCCUPANT /
6464 S MCKINLEY AV
STOCKTON CA 95206

APN 17704023

OCCUPANT /
4655 S AIRPORT WY
STOCKTON CA 95206

APN 17704024

OCCUPANT /
4653 S AIRPORT WY
STOCKTON CA 95206

APN 17704025

OCCUPANT /
0
STOCKTON CA 0

APN 0

OCCUPANT / *NSW 5/98*
1352 SPERRY RD
STOCKTON CA 95206

APN 17704027

OCCUPANT /
1456 SPERRY RD
STOCKTON CA 95206

APN 17704028

OCCUPANT /
1002 SPERRY RD
STOCKTON CA 95206

APN 17704030

OCCUPANT /
1015 PERFORMANCE DR
STOCKTON CA 95206

APN 17704030

OCCUPANT /
0 PERFORMANCE DR
STOCKTON CA 95206

APN 17704030

OCCUPANT /
936 PERFORMANCE DR
STOCKTON CA 95206

APN 17704030

OCCUPANT /
0 PERFORMANCE DR
STOCKTON CA 95206

APN 17704030

OCCUPANT / *NSW 4/98*
1002 SPERRY RD
STOCKTON CA 95206

APN 17704030

OCCUPANT /
1015 PERFORMANCE DR
STOCKTON CA 95206

APN 17704030

OCCUPANT /
0 PERFORMANCE DR
STOCKTON CA 95206

APN 17704030

Tm3-07

OCCUPANT /
936 PERFORMANCE DR
STOCKTON CA 95206

APN 17704030

OCCUPANT /
0 PERFORMANCE DR
STOCKTON CA 95206

APN 17704030

OCCUPANT / *NSW 4/98*
5502 S MCKINLEY AV
STOCKTON CA 95206

APN 17704032

OCCUPANT /
5502 S MCKINLEY AV
STOCKTON CA 95206

APN 17704032

OCCUPANT /
4735 S AIRPORT WY
STOCKTON CA 95206

APN 17704033

OCCUPANT /
4693 S AIRPORT WY
STOCKTON CA 95206

APN 17704033

OCCUPANT /
0 PERFORMANCE DR
STOCKTON CA 95206

APN 17704042

OCCUPANT /
0 PERFORMANCE DR
STOCKTON CA 95206

APN 17704045

OCCUPANT /
1320 PERFORMANCE DR
STOCKTON CA 95206

APN 17704050

OCCUPANT /
1398 PERFORMANCE DR
STOCKTON CA 95206

APN 17704050

OCCUPANT /
0 PERFORMANCE DR
STOCKTON CA 95206

APN 17704052

OCCUPANT /
0 PERFORMANCE DR
STOCKTON CA 95206

APN 17704053

OCCUPANT / *No Mail*
1110 PERFORMANCE DR
STOCKTON CA 95206 *Receipts 5/98*

APN 17704054

OCCUPANT /
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STOCKTON CA 0

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OCCUPANT /
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STOCKTON CA 0

APN 0

OCCUPANT /
9011 S AIRPORT WY
STOCKTON CA 95231

APN 17705003

OCCUPANT /
5403 S AIRPORT WY
STOCKTON CA 95206

APN 17705004

OCCUPANT /
5403 S AIRPORT WY
STOCKTON CA 95206

APN 17705004

OCCUPANT /
8441 S AIRPORT WY
STOCKTON CA 95206

APN 17705005

OCCUPANT /
8989 S AIRPORT WY
STOCKTON CA 95231

APN 17705005

OCCUPANT /
0
STOCKTON CA 0

APN 0

OCCUPANT / *NSW 5/98*
7521 S AIRPORT WY
STOCKTON CA 95206

APN 17705028

OCCUPANT /
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STOCKTON CA 0

APN 0

OCCUPANT /
431 SPERRY RD
STOCKTON CA 95206

APN 17728036

OCCUPANT /
431 SPERRY RD
STOCKTON CA 95206

APN 17728036

OCCUPANT /
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STOCKTON CA 95206

APN 17728036

OCCUPANT /
303 SPERRY RD
STOCKTON CA 95206

APN 17728037

OCCUPANT /
1051 SPERRY RD
STOCKTON CA 95206

APN 17728039

OCCUPANT /
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STOCKTON CA 95206

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Tm3-97

APN 17728039

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STOCKTON CA 95206

APN 17728039

OCCUPANT /
1051 SPERRY RD
STOCKTON CA 95206

APN 19302012

OCCUPANT /
7150 S REDWOOD ST
STOCKTON CA 95231

APN 19302034

OCCUPANT /
4485 S MCKINLEY AV
STOCKTON CA 95206

APN 19302035

OCCUPANT /
4555 S MCKINLEY AV
STOCKTON CA 95206

		17704030
		17704030
APOLLO PROPERTIES INC		17704053
APOLLO PROPERTIES INC		17704052
APOLLO PROPERTIES INC	ETAL	17704054
BEATTIE	PATRICIA ETAL	17705003
BILGE	BEVERLEY MARY	17704007
BORDENAVE	ROBERT LUCIEN	19302034
BROOKE	CLAUDE L TR	17704015
CABALAR	E JR & S L	17704025
CF PROPERTIES INC		17705028
GRAHAM	JOEL	17705004
GRAHAM	JOEL	17705004
J M MANUFACTURING		17728039
NAKAMURA	PATRICIA A ETA	19302012
NAKAMURA	PATRICIA A ETA	17704021
NIELSEN PTP		17705005
NOLL MFG CO CORP		17704050
PANATTONI CATLIN J/V VII PTP		17704045
PERRY	GORDON R & PAM	17704020
RENO WEST INVESTORS PTP		17704023
RENO WEST INVESTORS PTP		17704022
RENO WEST INVESTORS PTP		17704066
RENO WEST INVESTORS PTP		17704065
RENO WEST INVESTORS PTP		17704064
RENO WEST INVESTORS PTP		17704032
RENO WEST INVESTORS PTP		17704018
RENO WEST INVESTORS PTP		17704042
RENO WEST INVESTORS PTP		17704019
REYNOSO	VINCENT J JR &	17704012
SF PACIFIC PROPERTIES INC		19302035
SOLLA	CONRADO A EST	17704024
SWINERTON &	WALBERG CO COR	17704028
SWINERTON &	WALBERG CO COR	17704027
SWINERTON & WALBERG CO CORP		17704033
UNION PACIFIC CORPORATION		17704034
UNION PACIFIC CORPORATION		17728038
UNION PACIFIC CORPORATION		17704035
UNION PACIFIC CORPORATION		17704036
UNION PACIFIC CORPORATION		17704037
UNION PACIFIC CORPORATION		17705022
UNION PACIFIC CORPORATION		17705017
UNION PACIFIC CORPORATION		17704026
UNION PACIFIC CORPORATION		17705023
UNION PACIFIC CORPORATION		17728018
VALIMET	INC	17728036
VALIMET	INC	17728037

TM 3-97



Date February 5, 1998

TM No 3-97

Public Hearing Date _____

Case Planner DAVID STAGNARD

TENTATIVE SUBDIVISION MAP STATEMENT

1 Name of owner(s) Panattoni - Catlin Venture

Address(es) and phone number(s) 3620 Fair Oaks Blvd , Suite 150

Sacramento, CA 95864 (916) 485-8900

2 Name of subdivider(s) Same as above

Address(es) and phone number(s) _____

3 Contact person(s) regarding the map Michael D Hakeem, Esq

Hakeem, Ellis & Simonelli

Address(es) and phone number(s) 2800 W March Lane, Suite 200

Stockton, CA 95219 (209) 474-2800

4 The attached subdivision map shows proposed right of way lot pattern and other information as required by the City Subdivision Ordinance. The undersigned fully understands the provisions of the Subdivision Ordinance and will comply with all the requirements contained therein.

5 Specific Data

a Name of subdivision Airport Gateway Center

b Total acreage in subdivision 190.87 +/-

c General location of subdivision Please see attached description

d Estimated number of lots 25

e Water service provided by City of Stockton

f If any portion of the map is impacted by noise as defined by the noise contour information contained in the Noise Element of the General Plan, Ldn contours in excess of 60dB must be plotted on the map.

g School District Manteca Unified

If the map is within a school district in which conditions of overcrowding have been designated by the City Council, the following must be answered:

6 In accordance with the provisions of the School Facilities Dedication Ordinance as outlined in the Stockton Municipal Code Chapter 8 Part VII and in compliance with said ordinance the undersigned prefers to

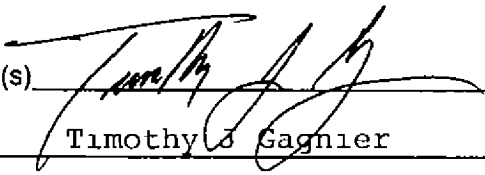
- _____ a dedicate land for interim school facilities or
- _____ b pay a fee in lieu of dedication as established by the City Council or
- _____ c a combination of both

The Planning Commission may approve the map for residential development without requiring the payment of fees or the dedication of land if the Planning Commission finds that there exists specific overriding fiscal economic social or environmental factors which would benefit the community as a whole

7 Remarks (Specify purpose of land division)

Include the following when application is returned

- 1 Two (2) copies of completed application form
- 2 Application fee \$3,410 00 plus _____ per lot
- 3 Initial Study and fee _____
- 4 A L U C fee _____
- 5 One copy of reverse sepi
- 6 Thirty five (35) copies of full size print folded to 8 1/2 by 11
- 7 One copy of 8 1/2 by 11 reduction
- 8 Other _____

Signature(s) 
_____ Timothy J Gagnier
Title Project Manager
_____ Date February 5, 1998

DISCLOSURE OF CAMPAIGN CONTRIBUTIONS

I, Timothy J Gagnier owner for a
(name)

I Timothy J Gagnier applicant for a
(name)

_____ (type and number of permit)

on oath declare that

(Please check appropriate statement and complete)

Owner Applicant
TJG TJG

I have not made any campaign contributions in the amount of \$250 or more to any member of the Planning Commission within the past 12 months

_____ I have made a campaign contribution for (local) (State) (federal) election of \$ _____ to _____ within the last 12 months (name of Commissioner)

I understand that Government Code Section 84308 requires this disclosure and I make this disclosure in compliance therewith. Further, I hereby declare that I shall not make any campaign contributions of \$250 or more to any member of the Planning Commission while my application is pending or within 3 months after a decision thereon by the Planning Commission

I am informed and believe that to do so would be a violation of State law

I declare under penalty of perjury that the foregoing is true and correct

Executed in Stockton, California this 5 day of February 1998

Timothy J Gagnier Owner
Timothy J Gagnier Applicant

Attachment 5(c)
to
Tentative Subdivision Map Statement

AIRPORT GATEWAY CENTER (SEIR3-82/IS8-97) - Subdivision of a 190 87-acre site into 25 parcels for industrial development for property located immediately west of the Stockton Municipal Airport, between Interstate 5 and State Highway 99, south of Sperry Road (446 acres+/- total)

HC/ESS

[Handwritten mark]

Law Offices Of

HAKEEM, ELLIS & SIMONELLI

A Professional Corporation

FEB 1 08

Michael D. Hakeem
Albert M. Ellis
Renata M. Marungo
Peter W. Mamon
Stephen B. Ardis

2800 West March Lane
Suite 200
Stockton, California 95219
TEL 209 474 2800
FAX 209 474 3604

February 10, 1998

James J. Simonelli
(Retired)

DAVID STAGNARO
CITY PLANNING DEPT
c/o City Hall
425 N El Dorado St
Stockton, CA 95202

RE OUR CLIENT PANATTONI - CATLIN VENTURE
OUR FILE NO 04921 1

Dear David

Enclosed please find the original and two copies of the Tentative Subdivision Map Statement which is being provided as an amendment to the existing Application on file. If further information is desired, please do not hesitate to call.

Very truly yours,

HAKEEM, ELLIS & SIMONELLI
A Professional Corporation

By



MICHAEL D HAKEEM

MDH dp
Enclosure

Date February 5, 1998

TM No _____

Public Hearing Date _____

Case Planner _____

TENTATIVE SUBDIVISION MAP STATEMENT

1 Name of owner(s) Panattoni - Catlin Venture

Address(es) and phone number(s) 3620 Fair Oaks Blvd , Suite 150

Sacramento, CA 95864 (916) 485-8900

2 Name of subdivider(s) Same as above

Address(es) and phone number(s) _____

3 Contact person(s) regarding the map Michael D Hakeem, Esq

Hakeem, Ellis & Simonelli

Address(es) and phone number(s) 2800 W March Lane, Suite 200

Stockton, CA 95219 (209) 474-2800

4 The attached subdivision map shows proposed right of way lot pattern and other information as required by the City Subdivision Ordinance. The undersigned fully understands the provisions of the Subdivision Ordinance and will comply with all the requirements contained therein.

5 Specific Data

a Name of subdivision Airport Gateway Center

b Total acreage in subdivision 190.87 +/-

c General location of subdivision Please see attached description

d Estimated number of lots 25

e Water service provided by City of Stockton

f If any portion of the map is impacted by noise as defined by the noise contour information contained in the Noise Element of the General Plan, Ldn contours in excess of 60dB must be plotted on the map.

g School District Manteca Unified

If the map is within a school district in which conditions of overcrowding have been designated by the City Council, the following must be answered:

(OVER)

6 In accordance with the provisions of the School Facilities Dedication Ordinance as outlined in the Stockton Municipal Code Chapter 8 Part VII and in compliance with said ordinance the undersigned prefers to

- _____ a dedicate land for interim school facilities or
- _____ b pay a fee in lieu of dedication as established by the City Council or
- _____ c a combination of both

The Planning Commission may approve the map for residential development without requiring the payment of fees or the dedication of land if the Planning Commission finds that there exists specific overriding fiscal economic social or environmental factors which would benefit the community as a whole

7 Remarks (Specify purpose of land division)

Include the following when application is returned

- 1 Two (2) copies of completed application form
- 2 Application fee \$3,410 00 plus _____ per lot
- 3 Initial Study and fee _____
- 4 A L U C fee _____
- 5 One copy of reverse sepiia
- 6 Thirty-five (35) copies of full size print folded to 8 1/2 by 11
- 7 One copy of 8-1/2 by 11 reduction
- 8 Other _____

Signature(s) _____


Timothy J Gagnier

Title Project Manager

Date February 5, 1998

Attachment 5(c)
to
Tentative Subdivision Map Statement

AIRPORT GATEWAY CENTER (SEIR3-82/IS8-97) - Subdivision of a 190 87-acre site into 25 parcels for industrial development for property located immediately west of the Stockton Municipal Airport, between Interstate 5 and State Highway 99, south of Sperry Road (446 acres+/- total)

DISCLOSURE OF CAMPAIGN CONTRIBUTIONS

I Timothy J Gagnier owner for a
(name)

I Timothy J Gagnier applicant for a
(name)

(type and number of permit)

on oath declare that

(Please check appropriate statement and complete)

Owner Applicant

TJG TJG

I have not made any campaign contributions in the amount of \$250 or more to any member of the Planning Commission within the past 12 months

_____ I have made a campaign contribution for (local) (State) (federal) election of \$ _____ to _____ within the last 12 months (name of Commissioner)

I understand that Government Code Section 84308 requires this disclosure and I make this disclosure in compliance therewith. Further, I hereby declare that I shall not make any campaign contributions of \$250 or more to any member of the Planning Commission while my application is pending or within 3 months after a decision thereon by the Planning Commission.

I am informed and believe that to do so would be a violation of State law.

I declare under penalty of perjury that the foregoing is true and correct.

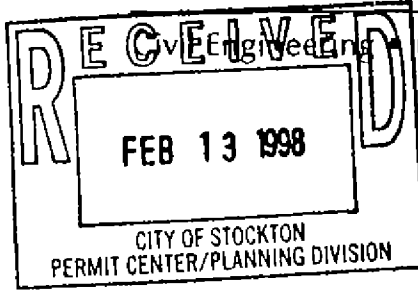
Executed in Stockton, California, this 5 day of February, 1998.

Timothy J Gagnier Owner
Timothy J Gagnier Applicant

SIEGFRIED ENGINEERING, Inc.



Robert W. Siegfried
Founder



FEB 13 1998

CITY OF STOCKTON
PERMIT CENTER/PLANNING DIVISION

Engineering • Land Surveying • Structural Engineering • Planning

SEI Job No 96389
Date 2-13-98
Time 2 45

Wayne M. West
Fax A. Corralles
Stephen R. Thumler
Anthony I. Lupis

To City of Stockton
Attention David Stagnaro
Project Airport Gateway Center
Documents Reduced Tentative Map

Fax No 937-8893

This facsimile is from SIEGFRIED ENGINEERING, Inc of Stockton, California Our fax number is (209) 942-0214

I am sending 2 pages, including this cover letter

If you do not receive all the pages, please telephone (209) 943-2021 immediately

Sincerely,
SIEGFRIED ENGINEERING, Inc

BY Stan Oshita

Message/Special Instructions

Original transmittal will not follow X
Original transmittal will be mailed _____
Original transmittal will be by other _____



CITY

STOCKTON

COMMUNITY DEVELOPMENT DEPARTMENT
CITY HALL
425 N. EL DORADO STREET
STOCKTON, CA 95202 1997 937-8266

January 23 1998

Michael Hakeem Attorney
Law Offices of Hakeem Ellis & Simonelli
2800 West March Lane Suite 200
Stockton CA 95219

STATUS OF TENTATIVE MAP (TM3 97) APPLICATION AND ENVIRONMENTAL PROCESS FOR AIRPORT GATEWAY CENTER PROJECT

This letter is to confirm a recent telephone conversation Associate Planner David Stagnaro had with you regarding the receipt of the Draft Supplemental EIR for the Airport Gateway Center Project. As indicated in our letter dated April 17 1997 this office required payment of the \$4 480 filing fee payable to the City of Stockton for the Draft Supplemental EIR upon receipt of the Administrative Draft of that document. As mentioned above this office has now received the Draft Supplemental EIR for the project and receipt of the above-mentioned fee is necessary for the timely processing of the environmental document as well as the related tentative map application. In addition your client's tentative map application must be revised to include the most recently submitted tentative map (October 1997) for the project. Our letter of April 17, 1997 also mentioned the requirement to submit the \$875 California Department of Fish and Game fee payable to the San Joaquin County Clerk in order to continue processing your tentative map application.

In conclusion the existing tentative map application will be held in abeyance pending a revised tentative map application and payment of the specified filing fees (\$4 480 payable to the City of Stockton and \$875 payable to the San Joaquin County Clerk). Should you have any questions regarding this matter please contact Associate Planner David Stagnaro at 937-8266.

JOHN CARLSON DIRECTOR
COMMUNITY DEVELOPMENT DEPARTMENT

SAM MAH DEPUTY DIRECTOR
PLANNING DIVISION

JC SM gp

cc Lex Corrales
Siegfried Engineering Inc
4045 Coronado Avenue
Stockton CA 95204 2396

Charlie Simpson
In Site Environmental
2155 West March Lane Suite 1C
Stockton CA 95207

emc Bob Sivell Enterprise Zone Manager
Guy Petzold, Deputy City Attorney

Gregg Meissner Senior Transportation Planner
Reed Hogan Junior Engineer

T:\PLANNING\LETTERS\LETGP\TM3 97.DJS



CITY OF STOCKTON

COMMUNITY DEVELOPMENT DEPARTMENT
 CITY HALL
 425 N EL DORADO STREET
 STOCKTON CA 95202 1997

937 8266

April 17, 1997

Michael Hakeem, Attorney
 Law Offices of Hakeem, Ellis & Simonelli
 2800 West March Lane, Suite 200
 Stockton, CA 95219

**STATUS OF TENTATIVE MAP (TM3-97) APPLICATION AND ENVIRONMENTAL
 PROCESS FOR AIRPORT GATEWAY CENTER PROJECT**

(Response to your letter dated 3/12/97)

This letter is in response to your request to waive applicable Government Code and/or Subdivision Map Act statutes regarding timely processing of the tentative map application for the Airport Gateway Center project for property located west of Airport Way and south of Sperry Road. The matter was forwarded to the City Attorney for a response. In their response, the City Attorney's office indicated that the time limits for processing the tentative map application may be waived. Therefore, planning staff has accepted the tentative map application and a receipt for the proposed project has been issued (attached). Per your request, the tentative map will be referred to other City Departments and affected agencies for preliminary review to obtain comments regarding its completeness and consistency with applicable code requirements. You will receive the comments on the proposed project in the next few weeks. However, in accordance with your request for a waiver to the statutes regarding the processing of the application and due to the need for supplemental environmental documentation, the application has not been deemed complete and is being held in abeyance.

On April 9, 1997, City staff met with you to discuss focus areas for the environmental document and related technical studies for the proposed project. As a result of that meeting, it was determined that a Supplement to a prior EIR 3 82 must be prepared and circulated concurrently with the processing of the tentative map application. Therefore, the tentative map shall not be set for hearing pending completion of the processing of the Supplemental EIR. The updated environmental information shall include, but not be limited to, the following:

- 1 Provide a proposed project description, including project changes, subdivision design, construction schedule and location of the proposed project. Describe proposed/required on-site and off site improvements necessary to facilitate project implementation
- 2 Prepare a new/updated traffic/circulation analysis, including the potential effects of heavy truck traffic and other vehicular activity in connection with the proposed project, railroad crossing at Tidewater and Southern Railroad tracks and proposed mitigation measures
- 3 Provide an updated noise study prepared by an acoustical engineer that will identify noise levels that will be produced by industrial land uses on the subject site, related transportation noise, the potential effect on adjacent existing and expected noise-sensitive land and proposed mitigation measures
- 4 Provide a new/updated air quality assessment for the proposed project and use currently acceptable modeling, as required by the San Joaquin Valley Air Pollution Control District (SJVAPCD)
- 5 Retain a qualified wildlife biologist to conduct a new biological site survey and provide an updated biological report which revises and updates the prior assessment of potential impacts and proposed mitigation measures on biological resources
- 6 Describe any important on site and/or adjacent topographical and water feature, including improvements to French Camp Slough and dedication along French Camp Slough. Update flood zone information for the proposed project site as well as water quality impacts and mitigation measures
- 7 Discuss new or expanded/modified public facilities, utilities and infrastructure for the proposed project site, including the need for and location of a proposed detention basin. Please describe the size and location of the detention basin and the point of terminal drainage. Specifically, describe and analyze the environmental effects of and related mitigation measures for any proposed infrastructure/utility master plans, including both on site and off-site improvements

The Supplement to the prior EIR 82 may be accompanied by a Mitigation Agreement executed between the City and the property owner, to provide assurances that the identified mitigation measures will, in fact, be implemented. Recordation of said Mitigation Agreement may be a condition of the tentative map

In conclusion, the existing tentative map application will be held in abeyance pending completion of the required environmental documentation and review process. Upon

Michael Hakeem, Attorney


April 17, 1997

Page 3

submittal of the administrative draft of the Supplemental EIR, this office will require a filing fee of \$4,480 for review and processing of the revised environmental documentation for the project. In addition, please be advised that a California Department of Fish and Game Fee of \$875 (payable to the 'San Joaquin County Clerk) must also be submitted

Should you have any questions regarding the matter please contact Senior Planner Mike Niblock or Assistant Planner Jenny Liaw at 937 8266

JOHN CARLSON, DIRECTOR
COMMUNITY DEVELOPMENT DEPARTMENT



SAM MAH, DEPUTY DIRECTOR
PLANNING DIVISION

JC SM cl

cc Lex Corrales
Siegfried Engineering, Inc
4045 Coronado Avenue
Stockton, CA 95204-2396

Charles Simpson, Principal
Simpson Moore, Inc
555 West Benjamin Holt Drive, Suite 414
Stockton, CA 95207

emc Bob Sivell, Enterprise Zone Manager
Guy Petzold, Deputy City Attorney
Reed Hogan, Junior Engineer Public Works Department
Greg Meissner, Senior Transportation Planner, Public Works Department

Date _____

TM No _____

Public Hearing Date _____

Case Planner _____

TENTATIVE SUBDIVISION MAP STATEMENT

1 Name of owner(s) Panattoni - Catlin Venture

Address(es) and phone no(s) (916) 485-8900

3620 Fair Oaks Blvd , Suite 150, Sacramento, CA 95864

2 Name of subdivider(s) Same as above

Address(es) and phone no(s) _____

3 Contact person(s) regarding the map Lex A Corrales

Address(es) and phone no(s) (209) 943-2021

4045 Coronado Ave , Stockton, CA 95204

4 The attached subdivision map shows proposed right of way, lot pattern, and other information as required by the City Subdivision Ordinance. The undersigned fully understands the provisions of the Subdivision Ordinance and will comply with all the requirements contained therein.

5 Specific Data

a Name of subdivision Airport Gateway Center

b Total acreage in subdivision 328 (±)

c General location of subdivision Southwest corner

of Airport Way and Sperry Road

d Estimated number of lots 48

e Water service provided by City of Stockton

f If any portion of the map is impacted by noise as defined by the noise contour information contained in the Noise Element of the General Plan, Ldn contours in excess of 60dB must be plotted on the map.

g School District Manteca Unified

If the map is within a school district in which conditions of overcrowding have been designated by the City Council, the following must be answered

(over)

6 In accordance with the provisions of the "School Facilities Dedication Ordinance" as outlined in the Stockton Municipal Code, Chapter 8, Part VII, and in compliance with said ordinance, the undersigned prefers to

_____ a dedicate land for interim school facilities, or

_____ b pay a fee in lieu of dedication as established by the City Council, or

_____ c a combination of both

The Planning Commission may approve the map for residential development without requiring the payment of fees or the dedication of land if the Planning Commission finds that there exists specific overriding fiscal, economic, social or environmental factors which would benefit the community as a whole

7 Remarks (Specify purpose of land division)

Include the following as indicated when application is returned

1 Two (2) copies of completed application form

2 Application fee \$3410 00 _____ plus _____ per lot

3 Initial Study and fee _____

4 A L U C fee _____

5 One copy of reverse sepi

6 _____ copies of full size print

7 One copy of 8-1/2" by 11" reduction

8 Other _____

Signature(s) 

Timothy J Gagnier

Title Project Manager

Date January 28, 1997

DISCLOSURE OF CAMPAIGN CONTRIBUTIONS

I Timothy J Gagnier owner for a
(name)

I _____ applicant for a
(name)

Tentative Map
(type and number of permit)

on oath declare that

(Please check appropriate statement and complete)

Owner Applicant

_____ I have not made any campaign contributions in the amount of \$250 or more to any member of the Planning Commission within the past 12 months

_____ I have made a campaign contribution for (local) (State) (federal) election of \$ _____ to _____ within the last 12 months (name of Commissioner)

I understand that Government Code Section 84308 requires this disclosure and I make this disclosure in compliance therewith. Further, I hereby declare that I shall not make any campaign contributions of \$250 or more to any member of the Planning Commission while my application is pending or within 3 months after a decision thereon by the Planning Commission

I am informed and believe that to do so would be a violation of State law

I declare under penalty of perjury that the foregoing is true and correct

Executed in Stockton California this 28 day of January 1997



Owner

Applicant

**CITY OF STOCKTON**

COMMUNITY DEVELOPMENT DEPARTMENT
CITY HALL
425 N. EL DORADO STREET
STOCKTON, CA 95202 1997

937 8266

April 17 1997

Michael Hakeem, Attorney
Law Offices of Hakeem Ellis & Simonelli
2800 West March Lane Suite 200
Stockton, CA 95219

**STATUS OF TENTATIVE MAP (TM3-97) APPLICATION AND ENVIRONMENTAL
PROCESS FOR AIRPORT GATEWAY CENTER PROJECT**

(Response to your letter dated 3/12/97)

This letter is in response to your request to waive applicable Government Code and/or Subdivision Map Act statutes regarding timely processing of the tentative map application for the Airport Gateway Center project for property located west of Airport Way and south of Sperry Road. The matter was forwarded to the City Attorney for a response. In their response, the City Attorney's office indicated that the time limits for processing the tentative map application may be waived. Therefore, planning staff has accepted the tentative map application and a receipt for the proposed project has been issued (attached). Per your request, the tentative map will be referred to other City Departments and affected agencies for preliminary review to obtain comments regarding its completeness and consistency with applicable code requirements. You will receive the comments on the proposed project in the next few weeks. However, in accordance with your request for a waiver to the statutes regarding the processing of the application and due to the need for supplemental environmental documentation, the application has not been deemed complete and is being held in abeyance.

On April 9, 1997, City staff met with you to discuss focus areas for the environmental document and related technical studies for the proposed project. As a result of that meeting, it was determined that a Supplement to a prior EIR 82 must be prepared and circulated concurrently with the processing of the tentative map application. Therefore, the tentative map shall not be set for hearing pending completion of the processing of the Supplemental EIR. The updated environmental information shall include, but not be limited to, the following:



- 1 Provide a proposed project description including project changes, subdivision design construction schedule and location of the proposed project Describe proposed/required on-site and off-site improvements necessary to facilitate project implementation
- 2 Prepare a new/updated traffic/circulation analysis, including the potential effects of heavy truck traffic and other vehicular activity in connection with the proposed project railroad crossing at Tidewater and Southern Railroad tracks and proposed mitigation measures
- 3 Provide an updated noise study prepared by an acoustical engineer that will identify noise levels that will be produced by industrial land uses on the subject site related transportation noise the potential effect on adjacent existing and expected noise-sensitive land and proposed mitigation measures
- 4 Provide a new/updated air quality assessment for the proposed project and use currently acceptable modeling as required by the San Joaquin Valley Air Pollution Control District (SJVAPCD)
- 5 Retain a qualified wildlife biologist to conduct a new biological site survey and provide an updated biological report which revises and updates the prior assessment of potential impacts and proposed mitigation measures on biological resources
- 6 Describe any important on-site and/or adjacent topographical and water feature including improvements to French Camp Slough and dedication along French Camp Slough Update flood zone information for the proposed project site as well as water quality impacts and mitigation measures
- 7 Discuss new or expanded/modified public facilities utilities and infrastructure for the proposed project site including the need for and location of a proposed detention basin Please describe the size and location of the detention basin and the point of terminal drainage Specifically describe and analyze the environmental effects of and related mitigation measures for any proposed infrastructure/utility master plans including both on-site and off-site improvements

The Supplement to the prior EIR3-82 may be accompanied by a Mitigation Agreement executed between the City and the property owner, to provide assurances that the identified mitigation measures will, in fact, be implemented Recordation of said Mitigation Agreement may be a condition of the tentative map


In conclusion the existing tentative map application will be held in abeyance pending completion of the required environmental documentation and review process Upon

Michael Hakeem, Attorney
April 17, 1997
Page 3

submittal of the administrative draft of the Supplemental EIR this office will require a filing fee of \$4 480 for review and processing of the revised environmental documentation for the project. In addition please be advised that a California Department of Fish and Game Fee of \$875 (payable to the 'San Joaquin County Clerk) must also be submitted

Should you have any questions regarding the matter please contact Senior Planner Mike Niblock or Assistant Planner Jenny Liaw at 937-8266

JOHN CARLSON DIRECTOR
COMMUNITY DEVELOPMENT DEPARTMENT



SAM MAH DEPUTY DIRECTOR
PLANNING DIVISION

JC SM cl

cc Lex Corrales
Siegfried Engineering Inc
4045 Coronado Avenue
Stockton CA 95204-2396

Charles Simpson Principal
Simpson Moore Inc
555 West Benjamin Holt Drive Suite 414
Stockton CA 95207

emc Bob Sivell Enterprise Zone Manager
Guy Petzold Deputy City Attorney
Reed Hogan Junior Engineer Public Works Department
Greg Meissner Senior Transportation Planner Public Works Department

SM/RL

URGENT

17N

Law Offices Of

HAKEEM, ELLIS & SIMONELLI

A Professional Corporation

MAR 13 1997

CITY OF STOCKTON
COMMUNITY DEVELOPMENT DEPT.

2800 West March Lane
Suite 200
Stockton California 95219
TEL 209 474 2800
FAX 209 471 3654

Michael D Hakem
Albert M Ellis
Rene M Marango
Peter W Mannon
Francis J Johnson PI

March 12, 1997

James J Simonelli
(Retired)

JOHN CARLSON
City of Stockton
Community Development Dept
425 N El Dorado Street
Stockton, CA 95202

Re STOCKTON AIRPORT GATEWAY PROJECT
Our Client Panattoni-Catlin Venture
Our File No 04921 1


Dear Mr Carlson

As a follow-up to our most recent telephone conversations, in connection with the above matter, this correspondence will confirm that we have met and consulted with Charlie Simpson of Simpson Moore, Inc, regarding CEQA compliance for that certain Tentative Map previously submitted for review and approval. Mr Simpson and I have discussed a scope of work and we intend to meet with your department in the immediate future to review same. As discussed, we would respectfully request that you retain the application fee check and the Tentative Map in anticipation of our meeting to discuss CEQA compliance. We are willing to waive any applicable Government Code and/or Subdivision Map Act statutes regarding your responsibility to respond to the application. As discussed, we can review the statute issue during our meeting and provide further particulars regarding an appropriate agreed upon timeline for your response.

If there are any questions, please do not hesitate to call

Very truly yours,

HAKEEM, ELLIS & SIMONELLI
A Professional Corporation

By 
MICHAEL D HAKEEM

MDH dp
cc Bob Sivell

GM/RL

REC'D
MAY 3 1997

Law Offices Of

HAKEEM, ELLIS & SIMONELLI

A Professional Corporation

CITY OF STOCKTON
COMMUNITY DEVELOPMENT

2800 West March Lane
Suite 200
Stockton California 95219
TEL 209 474 2800
FAX 209 474 3674

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Francis J Johnson PL

March 12, 1997

James J Simonelli
(Retired)

JOHN CARLSON
City of Stockton
Community Development Dept
425 N El Dorado Street
Stockton, CA 95202

Re **STOCKTON AIRPORT GATEWAY PROJECT**
Our Client **Panattoni-Catlin Venture**
Our File No **04921 1**

Dear Mr Carlson

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If there are any questions, please do not hesitate to call

Very truly yours,

HAKEEM, ELLIS & SIMONELLI
A Professional Corporation

By




MICHAEL D HAKEEM

MDH dp
cc Bob Sivell

February 13, 1997

MEMORANDUM

TO Jenny L

FROM Richard b 

SUBJECT REVIEW OF LLA97-04 (AIRPORT GATEWAY CENTER)

I've reviewed the 1996 Lot Line Adjustment file (580 1 in the General Files) and could not find an earlier lot line adjustment for this site. Before you review this one, you will need to contact Reed Hogan of PW and find out if this is the first or second of the two lot line adjustments that (we were lead to believe) have been submitted. You should also let him know that we were told that a third one will be submitted shortly. Please let me know what comments you have before we pass this along to Sam. Thanks Jenny

From Richard Larrouy
To JLIAW
Date 4/1/97 4:08pm
Subject Airport Gateway Project TM

As per my telephone conversation with Guy P. this afternoon, Mike Hakeem's letter stating his decision to waive applicable gov't code and Map Act statutes regarding timely processing of the TM applicable is acceptable. We will therefore accept the application (which is in my office) and issue a receipt, but not deem the application complete. After you've done this (and taken care of this week's staff reports) I would like you to send a letter to Mike H. restating the waiver of those time limits. The letter should also state that the application will not be deemed complete until adequate environmental review has taken place. With respect to that review, Mike Niblock has indicated to me that a meeting has been tentatively scheduled for early next week with Charlie Simpson and City staff to scope the environmental document. Thanks, Jenny.

CC SMAH MNIBLOCK

Planni - info



CITY OF STOCKTON

CIRCULATE
RECEIVED

OCT 16 1996

OFFICE OF THE CITY MANAGER
CITY HALL
425 N. EL DORADO STREET
STOCKTON, CA 95202-1897
(209) 937-8212
FAX (209) 937 7149

CITY OF STOCKTON
COMMUNITY DEVELOPMENT DEPT.

October 10, 1996

Ben Catlin
Tim Gagnier
Panattoni-Catlin
3620 Fair Oaks Blvd, Suite 150
Sacramento CA 95864

AIRPORT GATEWAY PROJECT

As you know over the last several years we have met to discuss the development of the Airport Gateway Center. These meetings have come about because of nationally known companies considering the proposed Industrial Park in their site selection process. Companies such as Nestle Food Distribution, Smith Foods Warehousing and the Noll Company have all shown an interest in your property. During our meetings we have discussed the phasing of the development of the Industrial Park. The first phase was to be the creation of a conveyable parcel through lot line adjustments and parcel mergers. The second phase was to be the planning and development of the remaining proposed industrial park through the subdivision process. The City Planning staff indicated that the second phase would require an Environmental Impact Report (EIR). The projects discussed for the first phase of the industrial park development had short acquisition and construction timelines.

Under the umbrella of economic development and our sensitivity to time and cost issues related to the development of a 450+ acre industrial park, it was agreed and understood by city staff that a catalyst project such as one of the ones named above would be allowed. Creating and conveying the catalyst parcel would provide a cash infusion from the sale and allow your company to begin the subdivision process for the remainder of the proposed industrial park. It should be noted, however, that City staff cautioned that any further development beyond the Noll project would be required to follow the standard subdivision process. The Noll company became the catalyst project and I understand they have purchased a site within the proposed industrial park. In order to give your company the ability to sell a site that met the Noll Company's requirements, the City of Stockton agreed to use the process of lot line adjustment and parcel mergers to create that parcel.

Since the Noll project we have been approached by you several times in the past few weeks to discuss an additional project which location has changed from meeting to meeting. This additional project would require further lot line adjustments and parcel mergers to create a satisfactory parcel for the company. Although staff of various city departments are extremely concerned about the lack of public comment on the development of the industrial park to this juncture and the lack of any real discussion between you, your representatives and the city's Community Development Department on the required process for subdividing the property,



we have agreed to work with you to create this one additional conveyable property This property would be located off the yet to be dedicated public right-of-way tentatively named Gateway Drive To meet this outcome will require close coordination between your engineering team and the City of Stockton's Public Works, Municipal Utilities and Community Development Departments This coordination is required to address such issues as acceptable infrastructure, building permit requirements and storm drainage issues

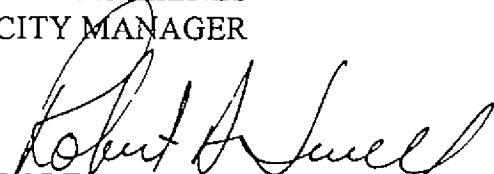
The City of Stockton's consideration of the development of any other sites in the proposed Airport Gateway Center will only come about with the project going through the subdivision and environmental process The original parcels within the proposed industrial park are configured as rural residential and the conversion of these parcels to industrial use will require a subdivision map, master planned infrastructure and an EIR More specifically, development of the industrial park will require dedication of right-of-way, adherence to the Arch Sperry Specific Plan, consideration of traffic signals, resolution of the storm drain detention pond issue and potentially several other items to insure that the public infrastructure requirements are met Additionally, the determination of the infrastructure needs of the industrial park will allow you to factor in all of your costs in order to receive a fair return when marketing your properties

It is essential that you keep me abreast of the progress you are making with the second project and that you begin immediately to start the subdivision and environmental process for the balance of the industrial park John Carlson, Director of Community Development and his staff would be happy to meet with you or your consultants at your convenience to begin this process

We are as anxious as you to have your industrial park subdivided and ready for sale It will be an important economic development element in the City of Stockton We are happy to assist you in your efforts and in marketing the park once completed

If you have any questions regarding this matter, please do not hesitate to contact me My office phone number is (209) 937-8530

DWANE MILNES
CITY MANAGER

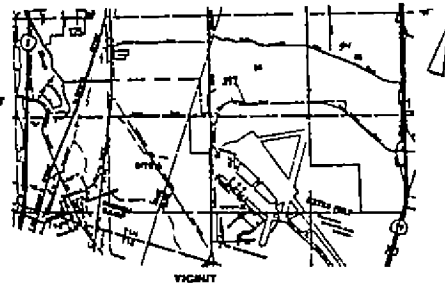


ROBERT A. SIVELL
ECONOMIC DEVELOPMENT
ENTERPRISE ZONE MANAGER

RAS/SLP ja

cc City Attorney
Public Works Department
Municipal Utilities Department
Community Development Department

ORIGINAL SCALE 1/4" = 100' - 1/8" = 100'



MAP OF TEN ATIVE
TRACT No. 1838

**AIRPORT GATEWAY CENTER
A BUSINESS PARK**

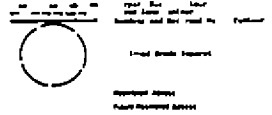
BY
BLOCK ON AIRPORT INDUSTRIAL, LTD.
AND
ONE S MANAGE ENT & INVESTMENT CORPORATION
GENERAL PARTNER

TRAC IN SEC 104 26 AND PORTION OF SECTION 3 OF
C. M. WISE GRANT SAN JOAQUIN COUNTY CALIFORNIA

PERTINENT DATA

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AREA 99	1.00	1.00	1.00
AREA 100	1.00	1.00	1.00

LEGEND



SIGNATURES

LAND PLANNING

LUIS MANZANO & ASSOCIATES
 9188 VIA COLINAS SUITE 104
 WESTLAKE VILLAGE, CA 91361
 (213) 901-2601
CIVIL ENGINEERING
 JORGENSEN-TOLLARAY ENGINEERS
 88 CUMMINS DR (SUITE 1)
 MONTECALA 95061
 (408) 52-6861

TM 29-83 (AMENDED)

TENTATIVE MAP NO. 1188C (STOCK TO U.C. CO.)

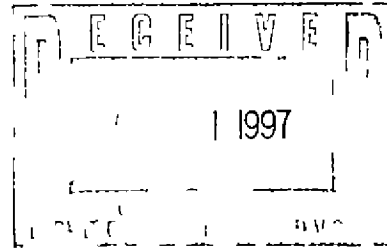
<p>APPROVED BY:</p> <p>_____ _____ _____ _____ _____ _____</p>	<p>APPROVED BY:</p> <p>_____ _____ _____ _____ _____ _____</p>	<p>APPROVED BY:</p> <p>_____ _____ _____ _____ _____ _____</p>
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DS

In te Environment

September 29 1997

Ms Lorna Benne
The California Utilities Public Commission
505 Van Ness Avenue
San Francisco CA 94102



RE Transmittal of Administrative Draft EIR for CPUC Staff Review
Airport Gateway Project, Stockton, San Joaquin County, California
Proposed public street crossing of Tidewater Southern Railroad

Dear Lorna

Enclosed please find two copies of the Administrative Draft EIR for the above-referenced project. Also enclosed are two copies of the proposed crossing application. These copies are provided at the request of the Stockton Community Development and Public Works Department. One copy is for your review and information, the second copy is, for review by any CPUC environmental staff who may be involved in the permit process.

The City of Stockton is currently reviewing this document at the staff level, in anticipation of its publication for public and agency review in October. The enclosed advanced review copies are forwarded to CPUC with the hope that any concerns you have with regard to the project could be surfaced now and incorporated into the published Draft EIR. The ultimate goal is facilitation of the CPUC's environmental and permitting process.

I would appreciate your review of the document to facilitate this review. I have highlighted and tabbed sections which relate directly to the Tidewater Southern crossing. The City will complete its review in the immediate future, and your earliest attention to the document would be appreciated.

If you have any questions regarding the document, please give me a call. If CPUC would like to provide comments ahead of formal publication, please provide those to David Stagnaro at the City of Stockton, 345 North El Dorado Street, Stockton, CA 95202, (Fax 209 937-8893).

Sincerely,

Charlie Simpson

CS/kp

cc Mike Hakeem for the applicant
David Stagnaro City of Stockton ✓
Lex Corrales, Siegfried Engineering

2155 West March Lane Suite 1 C Stockton, CA 95207
(209) 472-8650 Fax (209) 472-8654

TO RICHARD LARROUY
GREG MEISSNER

CC MIKE HAKEEM

FROM Charlie Simpson
PAGES 4 including cover
RE AIRPORT GATEWAY ADEIR

INSITE ENVIRONMENTAL
2155 West March Lane Suite 1C
Stockton, CA 95207

Voice 209-472-8650
Fax 209 472-8654

DATE 9/9/97
HARD COPY WILL OR WILL NOT FOLLOW



FAX

Mike Hakeem brought to my attention that one angle of the traffic analysis did not appear in the Admin. The attached is intended to correct that omission.

Several scenarios for traffic analysis were discussed over the course of project analysis. Two primary scenarios were selected for detailed analysis, and these two scenarios bracket any other potential for buildout of the site in the future. The two scenarios are as follows:

1. 60/40 (more or less) high cube, the "best" case, or least traffic generation.
2. The "worst-case" scenario involving full buildout of all net acres at 0.6 FAR with maximum trip generation per 1000 square feet.

The traffic analysis addressed a third, more realistic scenario addressing the "what if all this high-cube development doesn't occur?" condition. This is the scenario that was not reflected in the ADEIR.

3. All net acres at 80 trips per acre, DPW's average trip generation for industrial development.

kdAnderson analyzed case #3 under cumulative conditions, and this analysis was included in the technical report on attached pp. 68 and 69 (attached). Under case #2, the Arch-Sperry/Performance intersection, even with construction of an additional access, would fail under cumulative conditions. With traffic generation at 80 trips per acre, kdAnderson finds that the two intersections could be expected to operate at LOS D.

I have drafted errata which incorporate this analysis into the ADEIR for your review in conjunction with the remainder of the document. Please give me a call if you have any questions or need additional information.

Charlie

ERRATA TO TRAFFIC SECTION, AIRPORT GATEWAY SUPPLEMENTAL EIR

PAGE IV-38 Add to the paragraph discussing Arch-Airport-Sperry Road/Performance Drive intersection

LOS at this intersection was also analyzed using an assumed industrial trip generation rate of 80 trips per acre, a more realistic case. This analysis is discussed in the following section, 10 Cumulative, Plus Project at 80 Trips/Acre.

PAGE IV-39 Add new section re-number subsequent sections

10 Cumulative, Plus Project at 80 Trips/Acre

The project traffic analysis (Appendix C) also considered the effects of a reduction in "worst-case traffic on intersection operations. This analysis considered "worst-case traffic volumes based on an assumed trip generation factor of 80 trips per acre for industrial development Assumed by the Department of Public Works to represent a mix of potential industrial uses, this factor results in a more realistic worst-case" scenario.

PM peak hour traffic volumes under this scenario are shown in Figure 13 of Appendix C. Under this scenario, operations at intersections which fail under the analysis in the previous section would operate at acceptable levels, with proposed mitigation.

IV-42 (Table 10) Add footnote to "SIGNIFICANT AND UNAVOIDABLE finding for Intersection 11 Arch-Airport-Sperry/Performance as follows

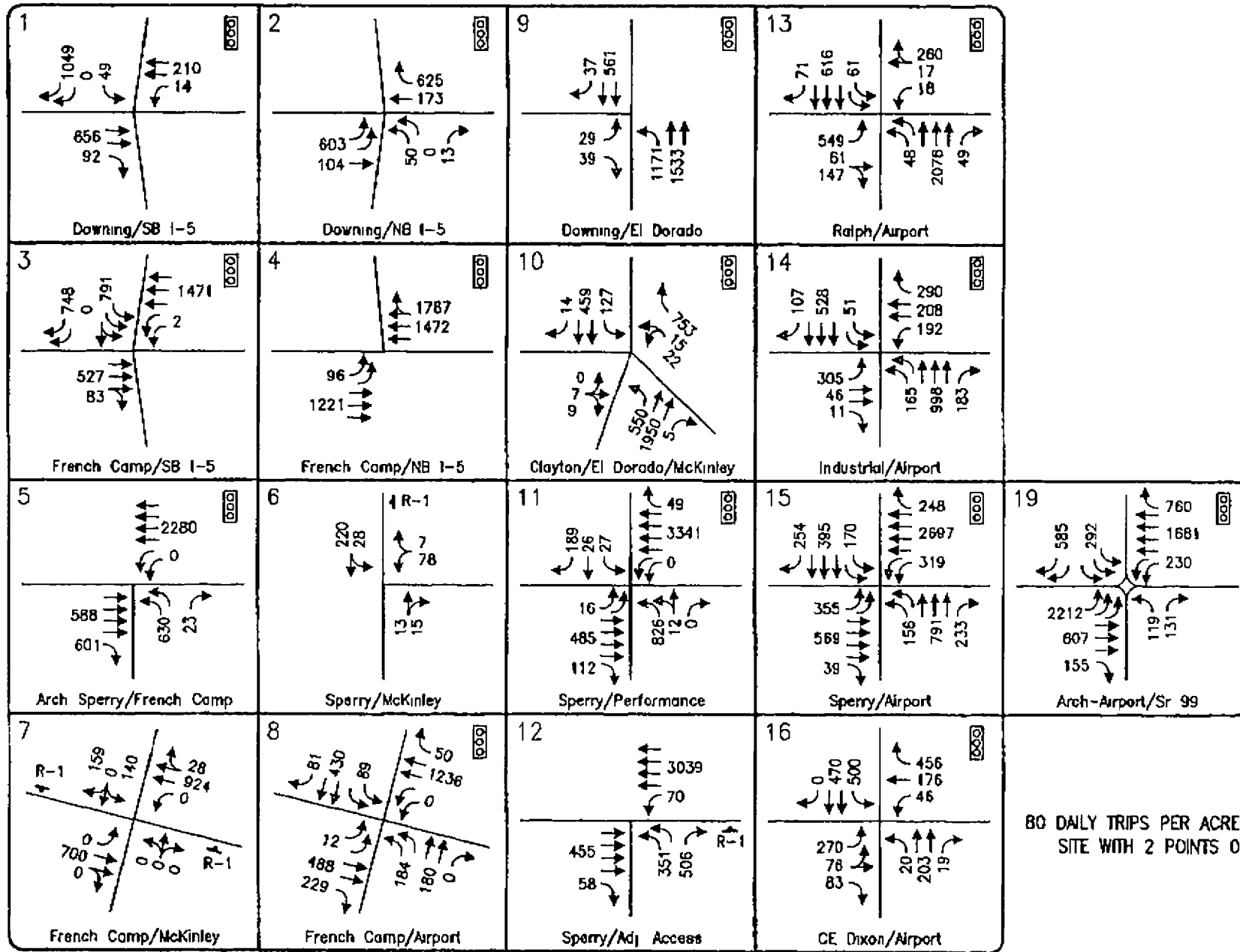
* With project trip generation estimated at 80 trips per net acre, and construction of a second project access to Sperry Road, the two intersections will operate at LOS D

El Dorado Avenue / Clayton Avenue / McKinley Avenue intersection While it could be possible to increase the capacity of this intersection and the route to Interstate 5 via Clayton Avenue, providing additional capacity on El Dorado Avenue and at the El Dorado/ Downing Avenue intersection would appear to be a superior alternative. To provide satisfactory Levels of Service, it would be necessary to carry a third northbound lane between McKinley Avenue and Downing Avenue and install dual left turn lanes on northbound El Dorado Avenue at the Downing Avenue intersection.

Airport Gateway should contribute its fair share to the cost of this improvement.

Secondary Access Developing a second access to Sperry Road would reduce the volume of traffic on Performance Drive. In concert with a reduction in "worst case" trip generation, development of a second access could result in satisfactory traffic conditions at the Airport Way / Sperry-Arch Airport Road intersection and at the Sperry Road / Performance Drive intersection.

Figure 13 presents cumulative PM peak hour traffic volumes assuming that the entire site is developed with uses corresponding to the City's standard Light Industrial trip generation rate (i.e., 80 daily trips per acre). Review of the Levels of Service accompanying these traffic reveal that the Airport Way / Sperry-Arch-Airport Road intersection would operate at LOS 'D' ($v/c = 0.87$). The two intersections on Sperry Road will operate at LOS 'E' ($v/c = 0.94$) and LOS "B" ($v/c = 0.66$). Assuming that Airport Gateway commuters will gravitate towards available capacity, it is reasonable to expect that LOS "D" or better conditions will result at these two intersections.



80 DAILY TRIPS PER ACRE ON PROJECT SITE WITH 2 POINTS OF ACCESS

KD Anderson
 Transportation Engineers

CUMULATIVE TRAFFIC VOLUMES

figure 13

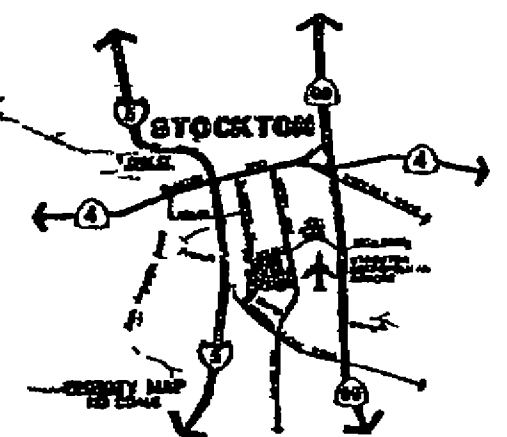
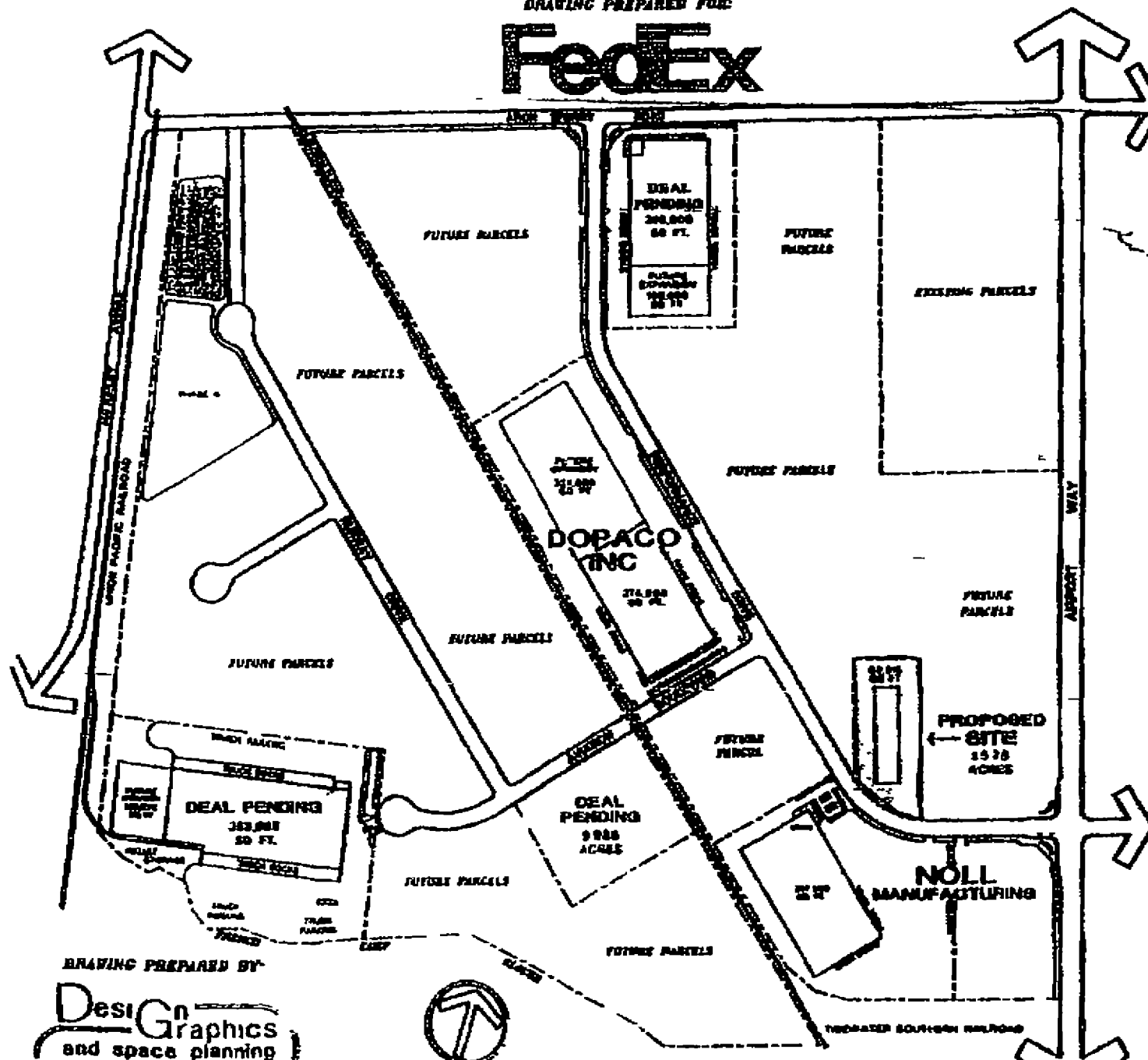
AUG 01 97 04 21 PM SAN JOAQUIN PARTNERSHIP

P 2/2 4/4

DRAWING PREPARED FOR:

FedEx

Another Project By
Panattoni-Catlin
Venture



AIRPORT GATEWAY CENTER

FOR FURTHER INFORMATION CONTACT
GREGORY O'LEARY
VICE PRESIDENT

ECB COMMERCIAL

CB COMMERCIAL
REAL ESTATE GROUP INC.
BRIDGEGATE AND EQUIPMENT
MANAGEMENT SERVICES
876 MARCH LANE
SUITE 170
STOCKTON, CA 95207
TEL: 209 486-2000

DRAWING PREPARED BY:

Design Graphics
and space planning

RE/DE 146
OWN
- RL
Recorded on 07/23/97
Inst # 97071908

Smt

After Recording Return To
City of Stockton
Public Works Dept.
425 N El Dorado Street
Stockton, CA 95202

DEFERRED IMPROVEMENT AGREEMENT

OWNERS

Panattoni-Catlin Venture,

Reno West Investors,

Panattoni-Catlin Joint Venture VII,
A California General Partnership,

Apollo Properties, Inc

ADDRESS

3620 Fair Oaks Blvd Suite 150
Sacramento, CA 95864

THIS AGREEMENT is made and entered into at Stockton, California, this 24th day of JUNE 1997, by and between

Panattoni-Catlin Venture, Reno West Investors, Panattoni Catlin Joint Venture VII, A California General Partnership, and Apollo Properties, Inc. hereinafter referred to as "Owners", and the CITY OF STOCKTON, a Municipal Corporation, located in the County of San Joaquin, State of California hereinafter referred to as "City "

WHEREAS Owners own real property in the City of Stockton, County of San Joaquin, State of California, and more particularly described as follows

That certain real property situated in the City of Stockton, County of San Joaquin, State of California, described as follows

All that certain real property known as Terra Matre Tract as shown on the map filed for record in Book of Maps and Plats, Volume 6, Page 26, San Joaquin Records

WHEREAS Owners are desirous of deferring construction of said improvements associated with the above described parcel

WHEREAS said improvements to be deferred are the following

SEE EXHIBIT "A"

CONSISTING OF BUT NOT LIMITED TO The design and installation of facilities listed in Exhibit "A"

WHEREAS it is in the interest of the City to defer completion of said frontage improvements at this time, pursuant to the authority contained in Stockton Municipal Code Section 16-174 1, because of the lack of frontage improvements on adjacent parcels

NOW THEREFORE, IT IS MUTUALLY STIPULATED AND AGREED

1 That said construction is deferred until required by the City pursuant to Exhibit "A" Such determination shall be made by the City Engineer based upon the totality of circumstances associated with the parcel, such as adjacent development changed conditions of the adjacent area, etc

2 That, upon notification by the City, the Owners, or Owners successors or assigns will engage a civil engineer licensed in the State of California to design and prepare construction drawings for the required improvements to be approved by the City Engineer Said drawings shall be submitted within 90 days after notification and construction shall commence within 30 days after approval of the construction drawings

3 That the Owners, or Owners' successors or assigns shall pay to the City all plan checking, processing and inspection fees pertaining to the improvements associated with this specific agreement.

4 That construction of said improvements shall be in accordance with Chapter 16, Division 4 of the City of Stockton, Municipal Code

5 That in event of default by Owners, or Owners' successors or assigns the City is hereby authorized to cause said construction to be done and charge the entire cost and expense to Owners, or Owners' successors or assigns including interest thereon at the maximum legal rate from the date of notice of said cost and expense until paid Such costs or expenses shall constitute a lien upon the property

6 That this agreement shall be recorded in the office of the Recorder of San Joaquin County, California, at the expense of Owners and shall constitute notice to all successors

and assigns of the title to said real property of the obligation herein set forth, and also a lien in such amount as will fully reimburse the City, including interest as hereinabove set forth, subject to foreclosure in event of default in payment.

7 That in event of litigation occasioned by any default of Owners, or Owners' successors or assigns Owners, or Owners' successors or assigns agree to pay all costs involved, including reasonable attorney's fees, and that the same shall become a part of the lien against said real property

8 That the term "Owners" shall include not only the present Owners but also heirs, successors, executors, administrators and assigns, it being the intent of the parties hereto that the obligations herein undertaken shall run with said real property and constitute a lien there against.


9 The obligations herein undertaken shall be binding upon the heirs, successors, executors, administrators and assigns of the parties hereto

IN WITNESS WHEREOF, the parties hereto have affixed their signatures as of the date first written above

APPROVED AS TO FORM

CITY OF STOCKTON


Deputy City Attorney

BY 
PAUL M. SENSIBAUGH
DEPUTY PUBLIC WORKS
DIRECTOR/CITY ENGINEER

OWNER

DLSC

Panattoni-Catlin Venture

OWNER ACKNOWLEDGMENT

STATE OF CALIFORNIA)
) SS
COUNTY OF SAN JOAQUIN)

On July 23 1997 before me, Timothy J Gagnier
(Name Title of Officer)

personally appeared Benjamin S. Catlin, (X) personally known to me or () proved to me on the basis of satisfactory evidence to be the person(s) whose name(e) is/are subscribed to the within instrument and acknowledged to me that he/she/they executed the same in his/her/their authorized capacity(ies) and that by his/her/their signature(s) on the instrument the person(s) or the entity upon behalf of which the person(s) acted executed the instrument.

WITNESS my hand and official seal

Timothy J Gagnier
Signature of Notary



OWNER.

DLSC

Reno West Investors

OWNER ACKNOWLEDGMENT

STATE OF CALIFORNIA)
) SS
COUNTY OF SAN JOAQUIN)

On July 23 1997 before me, Timothy J. Gagnier
(Name Title of Officer)

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WITNESS my hand and official seal

Timothy J Gagnier
Signature of Notary



OWNER.

Be 80
Panattoni-Catlin Joint Venture VII, A California General Partnership

OWNER ACKNOWLEDGMENT

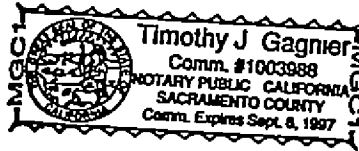
STATE OF CALIFORNIA)
) SS
COUNTY OF SAN JOAQUIN)

On July 23 1997 before me Timothy J Gagner
(Name Title of Officer)

personally appeared Benjamin S Catlin, () personally known to me or () proved to me on the basis of satisfactory evidence to be the person(s) whose name(e) is/are subscribed to the within instrument and acknowledged to me that he/she/they executed the same in his/her/their authorized capacity(ies), and that by his/her/their signature(s) on the instrument the person(s) or the entity upon behalf of which the person(s) acted executed the instrument.

WITNESS my hand and official seal

Timothy J Gagner
Signature of Notary



OWNER.

Linda M Stanley
Apollo Properties, Inc

OWNER ACKNOWLEDGMENT

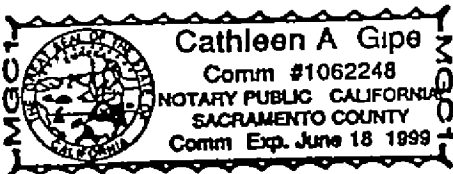
STATE OF CALIFORNIA)
) SS
COUNTY OF Sacramento SAN JOAQUIN)

On July 24 1997 before me Cathleen A Gipe, Notary Public
(Name Title of Officer)

personally appeared Linda M Stanley, () personally known to me or () proved to me on the basis of satisfactory evidence to be the person(s) whose name(e) is/are subscribed to the within instrument and acknowledged to me that he/she/they executed the same in his/her/their authorized capacity(ies) and that by his/her/their signature(s) on the instrument the person(s) or the entity upon behalf of which the person(s) acted executed the instrument.

WITNESS my hand and official seal

Cathleen A Gipe
Signature of Notary



CALIFORNIA ALL-PURPOSE ACKNOWLEDGMENT

No. 5907

State of California

County of San Joaquin

On July 22, 1997 before me, Carolyn A Camper, Notary Public

DATE

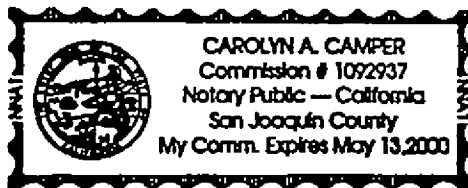
NAME, TITLE OF OFFICER E.G., JANE DOE, NOTARY PUBLIC

personally appeared Paul M Sensibaugh

NAME(S) OF SIGNER(S)

personally known to me - ~~OR~~ proved to me on the basis of satisfactory evidence

to be the person(s) whose name(s) is subscribed to the within instrument and acknowledged to me that he executed the same in his authorized capacity (as), and that by his signature (s) on the instrument the person (s), or the entity upon behalf of which the person (s) acted, executed the instrument



WITNESS my hand and official seal

Carolyn A Camper
SIGNATURE OF NOTARY

OPTIONAL

Though the data below is not required by law it may prove valuable to persons relying on the document and could prevent fraudulent reattachment of this form

CAPACITY CLAIMED BY SIGNER

DESCRIPTION OF ATTACHED DOCUMENT

- INDIVIDUAL
- CORPORATE OFFICER

TITLE(S)

TITLE OR TYPE OF DOCUMENT

- PARTNER(S) LIMITED
- GENERAL

ATTORNEY IN-FACT

TRUSTEE(S)

GUARDIAN/CONSERVATOR

OTHER _____

NUMBER OF PAGES

DATE OF DOCUMENT

SIGNER IS REPRESENTING
NAME OF PERSON(S) OR ENTITY(ES)

SIGNER(S) OTHER THAN NAMED ABOVE

EXHIBIT "A"

	PHASE A	PHASE B	PHASE C	PHASE D **
THRESHOLD TRIGGERS	Less than 65 acres and/or maximum of three sites (whichever is smaller)	65 acres or greater than 3 sites	200 acres or greater	Land use intensification as defined in note #4
DRAINAGE***	Ditch storage (if calculations show) plus 135,000 square feet at 8 feet deep with ditch/basins	Addition 1 pump and redundant and extra storage	Full build out per master plans 2 pumps and redundant	
SANITARY SEWER***	Construct master line to Sperry Road		Full build out per master plans	
WATER***	Water line southeast of second temporary cul-de-sac			
STREETS	<p>Performance Drive connect to Sperry</p> <p>Ultimate Geometrics at Sperry Rd/ Performance Dr Intersection and Airport Way/Performance Dr Intersection</p> <p>Left turn west-bound on Sperry to south-bound on Performance</p> <p>Underground signal conduits on Performance</p> <p>Signal at Airport Way/Performance Dr Intersection</p> <p>Railroad crossing *</p>	<p>Signal at Sperry Road/Performance Drive Intersection</p> <p>Widen Sperry for 2 lanes in each direction plus turn lanes and transitions plus widening onto Arch Airport</p> <p>Railroad Crossings *</p>	<p>Full Frontage Improvements on Sperry Rd And Airport Way plus all Internal streets</p> <p>Railroad Crossings *</p>	<p>Secondary Public Street access to Sperry Road east of Performance Drive and a minimum of 800' away from of Airport Way and 900' away from Performance Drive</p>

Additional Deferred Improvement Agreement Requirements

- 1) The Owners Developer and/or Successors-In-Interest shall dedicate necessary right-of-way on Sperry Road and Airport Way to accommodate all identified improvements and mitigation measures as well as to conform to proposed road alignments and adopted Specific Plans - Phase A.
- 2) Any deviation from the three initial agreed upon site locations shall necessitate moving to stage B
- 3) * Depending on site location Railroad crossings may be required in Phase A, Phase B, or Phase C
- 4) The total Non High Cube warehouse uses as defined by the City's Public Facilities Fee Program shall not exceed 37.5% of the 368 acres *Michael Wolf*
- ** Intensification above this level assumed in the Cumulative Access Evaluation for Airport Gateway Business Park Dated June 14, 1997 shall cause the Owners, Developer and/or Successors-In-Interest to construct the specified second access
- 5) All mitigation measures of COS EIR 3-82 and all conditions of TM 29-83 apply unless superseded by a more current EIR and/or TM document.
- 6) *** Sewer, water and storm drainage improvements are as defined in Interim Utility Plans prepared by Siegfried Engineering and dated _____

Michael Wolf

Michael Wolf

COMMUNITY DEVELOPMENT DEPARTMENT
 PLANNING DIVISION
 CITY PERMIT CENTER
 425 NORTH EL DORADO STREET
 STOCKTON CALIFORNIA 95202-1997
 (209) 937-8266

APRIL 28 1997

City of Stockton

Comm. Dev./Planning
 Ramirez w/MAP

Comm. Dev./Building
 Himes w/SM

Housing & Redevel. Dept
 Pinkerton w/SM

Public Works/Engineering
 Sensibaugh w/M
 Meissner w/M

Public Works/Op. & Main
 Escobar w/M

Public Works/Solid Waste
 Stephen Chen w/M

Parks & Recreation
 Machado w/M

Fire Department
 Gillis w/M

Police Department
 Marconi w/M

Municipal Utilities Dept
 Don King w/M

MIS
 Edgcomb w/M

File w/M

San Joaquin County

Planning
 w/M

Public Works
 w/M

Pub. Health Services/
 Env Health w/M

Flood Control
 w/M

Sheriff's Dept
 w/M

School District

Stockton Unified
 w/M

Lincoln Unified

Lodi Unified

Manteca Unified

State

Cal. Trans
 w/M

Other Agencies

PG&E
 w/M

W/SM

P T & T
 w/M

Continental Cable
 w/M

SMART
 w/M

USPS Post Office
 w/M

PLAFCON
 w/M

COG/ALUC

Cal Water
 w/M

Other
 City Manager - Bob Swell
 City Attorney - [Signature]

Enclosed is a PRELIMINARY TENTATIVE MAP of AIRPORT GATEWAY CENTER (TM3-97). We would appreciate your reviewing this proposal and submitting any comments to this office by MAY 9, 1997.

Please answer and return the questionnaire on the back of this page as well as the map

If you have any questions regarding this proposal, please contact ASSISTANT PLANNER JENNY LIAW at (209) 937-8266

To Community Development Department/Planning Division

From _____

Subject Response to tentative map or proposal of _____

1 We have reviewed the tentative map or proposal and our comments are as follows

a map is acceptable _____

b map is not acceptable _____ for reasons itemized below

c other comments

2 Recommendation

3 Person reviewing the map _____ phone _____

T \PLANNING\DRC2\TM2NDPG

MEMORANDUM

April 28 1997

TO See Attached Referral List

FROM Sam Mah Deputy Director
Community Development Department Planning Division

SUBJECT **REVIEW OF PRELIMINARY TENTATIVE MAP FOR AIRPORT GATEWAY
CENTER (WEST SIDE OF AIRPORT WAY AND NORTH OF FRENCH CAMP
SLOUGH)**

Siegfried Engineers has submitted a tentative map to subdivide the above-noted property containing approximately 328 acres into 48 parcels for industrial development. We have been asked to review the preliminary document and provide comments regarding its completeness and consistency with applicable code requirements. This preliminary review is not intended to be exhaustive or take the place of the formal tentative map review that will occur when the project's environmental review has been completed. Please provide your comments to the Community Development Department Planning Division no later than May 9, 1997.

The following information (and attached zoning and tentative maps) regarding the site's background may be helpful in your review of the tentative map. The area containing approximately 516 acres located on the south side of Sperry Road and west of Airport Way was annexed and rezoned in 1983 to E-P Enterprise Performance District, A-1 Agricultural District, and M-2 Heavy-Industrial District (A-82-2 and Z-12-83). At the same time, a tentative map (TM29-83) was filed to subdivide a 447-acre portion of the 516-acre site into 84 parcels for the development of an industrial/office park. The tentative map expired due to no final map having been recorded. The applicant is now proposing to subdivide a 328-acre portion site of this 447 acres for the development of industrial uses. All of the above maps (A-82-2, Z-12-98 and TM29-83) were processed with an Environmental Impact Report (EIR3-82).

Should you have any questions regarding the tentative map, please contact Assistant Planner Jenny Liaw at 937-8266.

JOHN CARLSON, DIRECTOR
COMMUNITY DEVELOPMENT DEPARTMENT

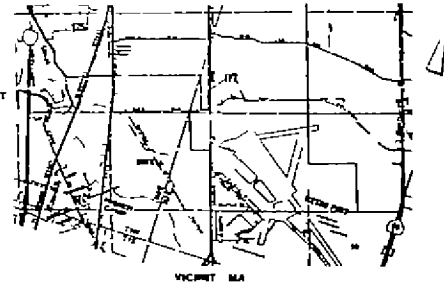
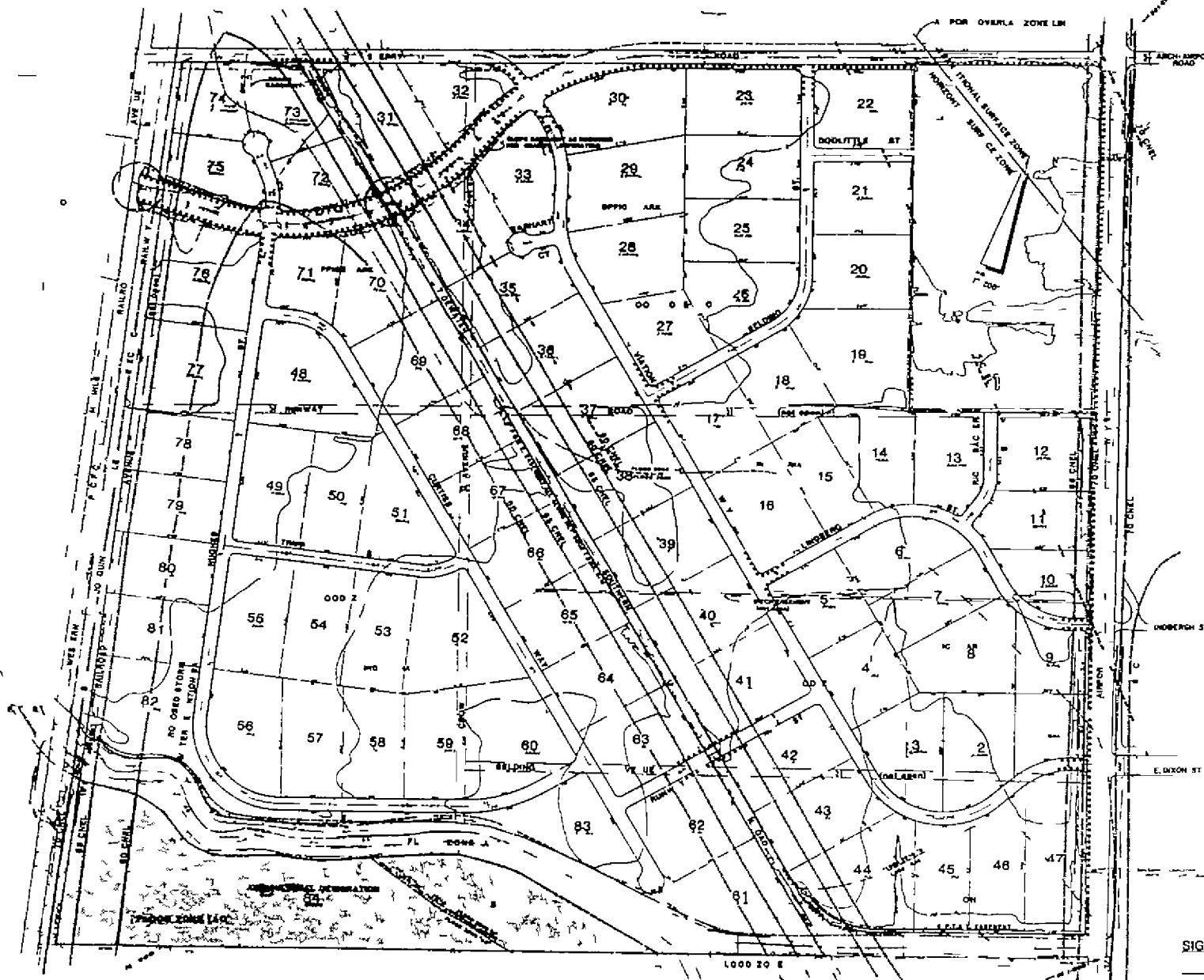


SAM MAH, DEPUTY DIRECTOR
COMMUNITY DEVELOPMENT DEPARTMENT

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Attachments

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AP OF
TRACT No 1836

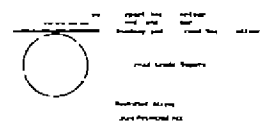
AIRPORT GATEWAY CENTER
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GENERAL PARTNER
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C. WELER GRANT AN SO LUM COUNTY ALABAMA

PERTINENT DATA

Area	1.2	1.2
Permit Fee	1.2	1.2
Other Fees	1.2	1.2
Total	1.2	1.2

LEGEND



SIGNATURES

LAND PLANNING

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CIVIL ENGINEERING
ENS HGR

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TM 29-83 (AMENDED)



COMMUNITY DEVELOPMENT DEPARTMENT
CITY HALL
425 N. EL DORADO STREET
STOCKTON, CA 95202 1997

937-8266

April 17 1997

Michael Hakeem Attorney
Law Offices of Hakeem Ellis & Simonelli
2800 West March Lane Suite 200
Stockton CA 95219

**STATUS OF TENTATIVE MAP (TM3-97) APPLICATION AND ENVIRONMENTAL
PROCESS FOR AIRPORT GATEWAY CENTER PROJECT**

(Response to your letter dated 3/12/97)

This letter is in response to your request to waive applicable Government Code and/or Subdivision Map Act statutes regarding timely processing of the tentative map application for the Airport Gateway Center project for property located west of Airport Way and south of Sperry Road. The matter was forwarded to the City Attorney for a response. In their response, the City Attorney's office indicated that the time limits for processing the tentative map application may be waived. Therefore, planning staff has accepted the tentative map application and a receipt for the proposed project has been issued (attached). Per your request, the tentative map will be referred to other City Departments and affected agencies for preliminary review to obtain comments regarding its completeness and consistency with applicable code requirements. You will receive the comments on the proposed project in the next few weeks. However, in accordance with your request for a waiver to the statutes regarding the processing of the application and due to the need for supplemental environmental documentation, the application has not been deemed complete and is being held in abeyance.

On April 9, 1997, City staff met with you to discuss focus areas for the environmental document and related technical studies for the proposed project. As a result of that meeting, it was determined that a Supplement to a prior EIR3-82 must be prepared and circulated concurrently with the processing of the tentative map application. Therefore, the tentative map shall not be set for hearing pending completion of the processing of the Supplemental EIR. The updated environmental information shall include, but not be limited to, the following:



Michael Hakeem, Attorne,

April 17, 1997

Page 2

- 1 Provide a proposed project description including project changes subdivision design, construction schedule and location of the proposed project Describe proposed/required on-site and off-site improvements necessary to facilitate project implementation
- 2 Prepare a new/updated traffic/circulation analysis including the potential effects of heavy truck traffic and other vehicular activity in connection with the proposed project railroad crossing at Tidewater and Southern Railroad tracks and proposed mitigation measures
- 3 Provide an updated noise study prepared by an acoustical engineer that will identify noise levels that will be produced by industrial land uses on the subject site related transportation noise the potential effect on adjacent existing and expected noise-sensitive land and proposed mitigation measures
- 4 Provide a new/updated air quality assessment for the proposed project and use currently acceptable modeling as required by the San Joaquin Valley Air Pollution Control District (SJVAPCD)
- 5 Retain a qualified wildlife biologist to conduct a new biological site survey and provide an updated biological report which revises and updates the prior assessment of potential impacts and proposed mitigation measures on biological resources
- 6 Describe any important on-site and/or adjacent topographical and water feature including improvements to French Camp Slough and dedication along French Camp Slough Update flood zone information for the proposed project site as well as water quality impacts and mitigation measures
- 7 Discuss new or expanded/modified public facilities utilities and infrastructure for the proposed project site including the need for and location of a proposed detention basin Please describe the size and location of the detention basin and the point of terminal drainage Specifically describe and analyze the environmental effects of and related mitigation measures for any proposed infrastructure/utility master plans including both on site and off site improvements

The Supplement to the prior EIR3-82 may be accompanied by a Mitigation Agreement executed between the City and the property owner to provide assurances that the identified mitigation measures will in fact be implemented Recordation of said Mitigation Agreement may be a condition of the tentative map

In conclusion the existing tentative map application will be held in abeyance pending completion of the required environmental documentation and review process Upon

Michael Hakeem, Attorney,

April 17, 1997

Page 3

submittal of the administrative draft of the Supplemental EIR this office will require a filing fee of \$4 480 for review and processing of the revised environmental documentation for the project In addition please be advised that a California Department of Fish and Game Fee of \$875 (payable to the "San Joaquin County Clerk') must also be submitted

Should you have any questions regarding the matter please contact Senior Planner Mike Niblock or Assistant Planner Jenny Liaw at 937-8266

JOHN CARLSON, DIRECTOR
COMMUNITY DEVELOPMENT DEPARTMENT



SAM MAH DEPUTY DIRECTOR
PLANNING DIVISION

JC SM cl

cc Lex Corrales
Siegfried Engineering Inc
4045 Coronado Avenue
Stockton CA 95204-2396

Charles Simpson Principal
Simpson Moore Inc
555 West Benjamin Holt Drive Suite 414
Stockton CA 95207

emc Bob Sivell Enterprise Zone Manager
Guy Petzold Deputy City Attorney
Reed Hogan Junior Engineer, Public Works Department
Greg Meissner Senior Transportation Planner Public Works Department

ORIGINAL TO PLANNING

SM/RL

RECEIVED

MN

Law Offices Of

HAKEEM, ELLIS & SIMONELLI

A Professional Corporation

MAR 13 1997

CITY OF STOCKTON
COMMUNITY DEVELOPMENT DEPT.

2800 West March Lane
Suite 200
Stockton California 95219
TEL 209 474 2800
FAX 209 474 3654

Michael D Hakem
Albert M Ellis
Renee M Almenzo
Peter W Mamon
Francis J Johnson PL

March 12, 1997

James J Simonelli
(Retired)

JOHN CARLSON
City of Stockton
Community Development Dept
425 N El Dorado Street
Stockton, CA 95202

Re **STOCKTON AIRPORT GATEWAY PROJECT**
Our Client **Panattoni-Catlin Venture**
Our File No **04921 1**

Dear Mr Carlson

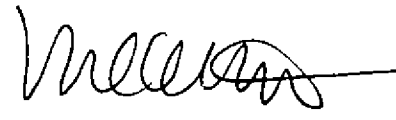
As a follow-up to our most recent telephone conversations, in connection with the above matter, this correspondence will confirm that we have met and consulted with Charlie Simpson of Simpson Moore, Inc , regarding CEQA compliance for that certain Tentative Map previously submitted for review and approval Mr Simpson and I have discussed a scope of work and we intend to meet with your department in the immediate future to review same As discussed, we would respectfully request that you retain the application fee check and the Tentative Map in anticipation of our meeting to discuss CEQA compliance We are willing to waive any applicable Government Code and/or Subdivision Map Act statutes regarding your responsibility to respond to the application As discussed, we can review the statute issue during our meeting and provide further particulars regarding an appropriate agreed upon timeline for your response

If there are any questions, please do not hesitate to call

Very truly yours,

HAKEEM, ELLIS & SIMONELLI
A Professional Corporation

By



MICHAEL D HAKEEM

MDH dp
cc Bob Sivell

MEMORANDUM

April 28 1997

TO See Attached Referral List

FROM Sam Mah Deputy Director
Community Development Department Planning Division

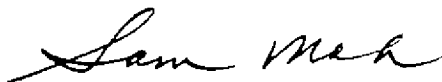
SUBJECT **REVIEW OF PRELIMINARY TENTATIVE MAP FOR AIRPORT GATEWAY
CENTER (WEST SIDE OF AIRPORT WAY AND NORTH OF FRENCH CAMP
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JOHN CARLSON DIRECTOR
COMMUNITY DEVELOPMENT DEPARTMENT



SAM MAH DEPUTY DIRECTOR
COMMUNITY DEVELOPMENT DEPARTMENT

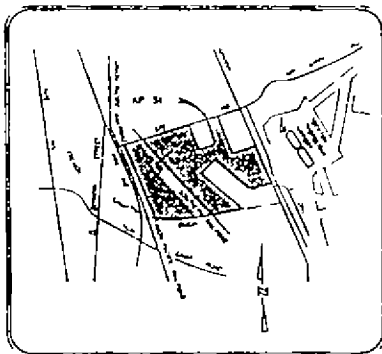
JC SM rw

Attachments

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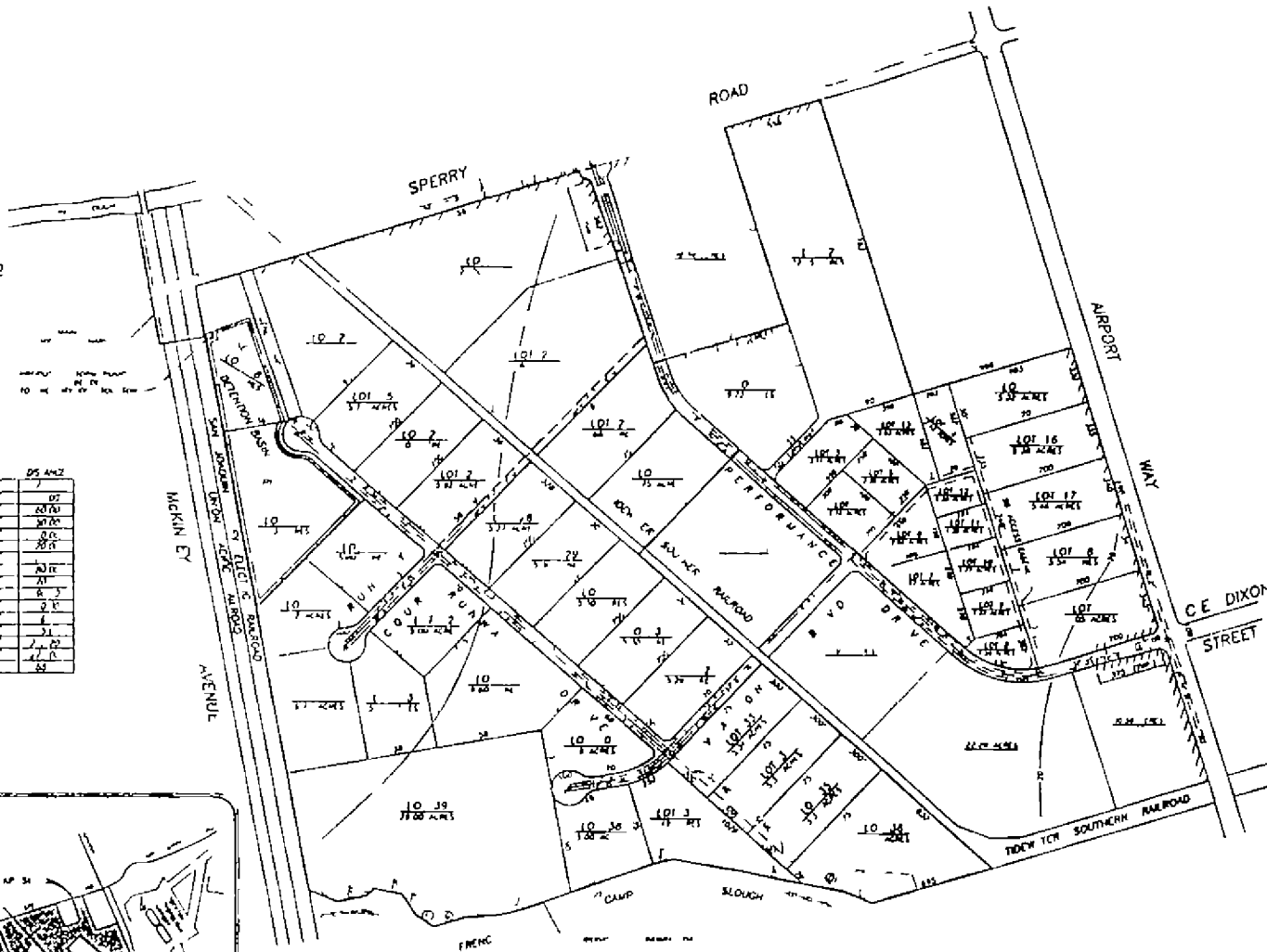


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VICINITY MAP
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NOTES:

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BEING THE CITY ENGINEER AND SIGN OF STATE OF CALIFORNIA THESE PLANS AND PLATS SHALL BE SUBJECT OF PUBLIC HEARING AND APPROVAL BY THE CITY OF STOCKTON

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PROPOSED WATER MAIN	---

OWNER:

PERFORMANCE BLVD DEVELOPMENT
2070 AND 2080 BLDG. 200
SACRAMENTO, CALIFORNIA 95804
PHONE: 442-9900
THE COMPANY

SUBDIVIDER:

PERFORMANCE BLVD DEVELOPMENT
2070 AND 2080 BLDG. 200
SACRAMENTO, CALIFORNIA 95804
PHONE: 442-9900
THE COMPANY

PL NO. 17486 MAP THIS _____ SH OF _____

CITY OF STOCKTON PLANNING COMMISSION

APPROVED BY THE CITY OF STOCKTON PLANNING COMMISSION THIS _____ DAY OF _____

CITY ENGINEER

APPROVED THE CITY ENGINEER OF THE CITY OF STOCKTON THIS _____ DAY OF _____

CITY ENGINEER

TENTATIVE MAP
TRACT NO 2778
SUBDIVISIONS OF SAN JOAQUIN COUNTY
AIRPORT GATEWAY CENTER

BEING PORTION OF SECTION 25, C. H. WEBER GRANT
CITY OF STOCKTON, SAN JOAQUIN COUNTY, CALIFORNIA
JANUARY 1997

SIEGFRIED ENGINEERING Inc			JOB NO. 24386 DATE: JAN 1997
Civil Engineering	Land Surveying		

TENTATIVE MAP		PAGE 1
AIRPORT GATEWAY CENTER STOCKTON CALIFORNIA		



COMMUNITY DEVELOPMENT DEPARTMENT
CITY HALL
425 N. EL DORADO STREET
STOCKTON CA 95202 1997

937-8266

April 17 1997

Michael Hakeem Attorney
Law Offices of Hakeem Ellis & Simonelli
2800 West March Lane Suite 200
Stockton CA 95219

**STATUS OF TENTATIVE MAP (TM3-97) APPLICATION AND ENVIRONMENTAL
PROCESS FOR AIRPORT GATEWAY CENTER PROJECT**

(Response to your letter dated 3/12/97)

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On April 9 1997 City staff met with you to discuss focus areas for the environmental document and related technical studies for the proposed project. As a result of that meeting it was determined that a Supplement to a prior EIR 3 82 must be prepared and circulated concurrently with the processing of the tentative map application. Therefore the tentative map shall not be set for hearing pending completion of the processing of the Supplemental EIR. The updated environmental information shall include but not be limited to the following:

Michael Hakeem, Attorne,

April 17, 1997

Page 2

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- 6 Describe any important on-site and/or adjacent topographical and water feature including improvements to French Camp Slough and dedication along French Camp Slough Update flood zone information for the proposed project site as well as water quality impacts and mitigation measures
- 7 Discuss new or expanded/modified public facilities utilities and infrastructure for the proposed project site including the need for and location of a proposed detention basin Please describe the size and location of the detention basin and the point of terminal drainage Specifically describe and analyze the environmental effects of and related mitigation measures for any proposed infrastructure/utility master plans including both on site and off-site improvements

The Supplement to the prior EIR3 82 may be accompanied by a Mitigation Agreement executed between the City and the property owner to provide assurances that the identified mitigation measures will in fact be implemented Recordation of said Mitigation Agreement may be a condition of the tentative map

In conclusion the existing tentative map application will be held in abeyance pending completion of the required environmental documentation and review process Upon

Michael Hakeem, Attorne,

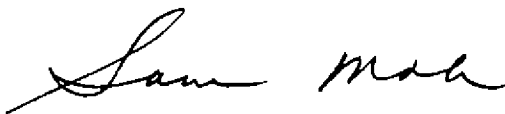
April 17, 1997

Page 3

submittal of the administrative draft of the Supplemental EIR this office will require a filing fee of \$4 480 for review and processing of the revised environmental documentation for the project In addition please be advised that a California Department of Fish and Game Fee of \$875 (payable to the "San Joaquin County Clerk") must also be submitted

Should you have any questions regarding the matter please contact Senior Planner Mike Niblock or Assistant Planner Jenny Liaw at 937-8266

JOHN CARLSON DIRECTOR
COMMUNITY DEVELOPMENT DEPARTMENT



SAM MAH DEPUTY DIRECTOR
PLANNING DIVISION

JC SM cl

cc Lex Corrales
Siegfried Engineering Inc
4045 Coronado Avenue
Stockton CA 95204-2396

Charles Simpson Principal
Simpson Moore Inc
555 West Benjamin Holt Drive Suite 414
Stockton CA 95207

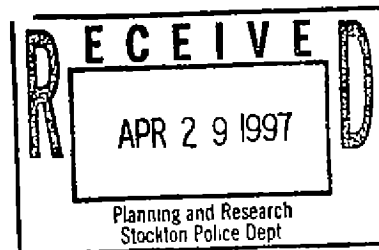
emc Bob Sivell Enterprise Zone Manager
Guy Petzold Deputy City Attorney
Reed Hogan Junior Engineer Public Works Department
Greg Meissner Senior Transportation Planner Public Works Department

D.I. / 50

fms

COMMUNITY DEVELOPMENT DEPARTMENT
PLANNING DIVISION
CITY PERMIT CENTER
425 NORTH EL DORADO STREET
STOCKTON CALIFORNIA 95202-1997
(209) 937-8266

APRIL 28 1997



City of Stockton

San Joaquin County

State

___ Comm Dev /Planning
Ramirez w/MAP

___ Planning
w/M

___ Cal Trans
w/M

___ Comm Dev /Building
Himes w/SM

___ Public Works
w/M

Other Agencies

___ Housing & Redevel Dept
Pinkerton w/SM

___ Pub Health Services/
Env Health w/M

___ P G & E
w/M
___ w/SM M

___ Public Works/Engineering
Sensibaugh w/M
___ Meissner w/M

___ Flood Control
w/M

___ P T & T
w/M

___ Public Works/Op & Main
Escobar w/M

___ Sheriff's Dept
w/M

___ Continental Cable
w/M

___ Public Works/Solid Waste
Stephen Chen w/M

School District

___ SMART
w/M

___ Parks & Recreation
Machado w/M

___ Stockton Unified
w/M

___ U S Post Office
w/M

___ Fire Department
Gillis w/M

___ Lincoln Unified

___ LAFCO
w/M

✓ Police Department
Marconi w/M

___ Lodi Unified

___ COG/ALUC

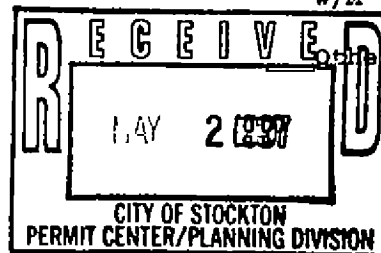
___ Municipal Utilities Dept
Don King w/M

___ Manteca Unified

___ Cal Water
w/M

___ MIS
Edgecomb w/M

___ File w/M



Enclosed is a PRELIMINARY TENTATIVE MAP of AIRPORT GATEWAY CENTER (TM3-97). We would appreciate your reviewing this proposal and submitting any comments to this office by MAY 9 1997.

Please answer and return the questionnaire on the back of this page as well as the map

If you have any questions regarding this proposal, please contact ASSISTANT PLANNER JENNY LIAW at (209) 937-8266

To Community Development Department/Planning Division

From: Stöckton Police Department/Planning & Research Section

Subject: Response to tentative map or proposal of _____

TM3-97

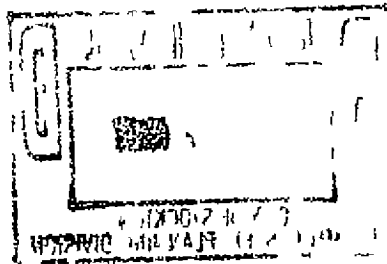
1 We have reviewed the tentative map or proposal and our comments are as follows

a map is acceptable x

b map is not acceptable _____ for reasons itemized below

c other comments

2 Recommendation



3 Person reviewing the map

Bob Marconi

phone 937-8651
Bob Marconi, Police Planning Analyst

T \PLANNING\DRC2\TM2NDPG

COMMUNITY DEVELOPMENT DEPARTMENT
 PLANNING DIVISION
 CITY PERMIT CENTER
 425 NORTH EL DORADO STREET
 STOCKTON, CALIFORNIA 95202-1997
 (209) 937-8266

APRIL 28, 1997

<u>City of Stockton</u>	<u>San Joaquin County</u>	<u>State</u>
<input type="checkbox"/> Comm Dev /Planning Ramirez w/MAP	<input type="checkbox"/> Planning w/M	<input type="checkbox"/> Cal Trans w/M
<input type="checkbox"/> Comm Dev /Building Himes w/SM	<input type="checkbox"/> Public Works w/M	<u>Other Agencies</u>
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<input type="checkbox"/> Public Works/Engineering Sensibaugh w/M Meissner w/M	<input type="checkbox"/> Flood Control w/M	<input type="checkbox"/> P T & T w/M
<input type="checkbox"/> Public Works/Op & Main Escobar w/M	<input type="checkbox"/> Sheriff's Dept w/M	<input type="checkbox"/> Continental Cable w/M
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	<u>School District</u>	
<input type="checkbox"/> Public Works/Solid Waste Stephen Chen w/M	<input type="checkbox"/> Stockton Unified w/M	<input type="checkbox"/> U S Post Office w/M
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<input type="checkbox"/> Police Department Marconi w/M	<input type="checkbox"/> Manteca Unified	<input type="checkbox"/> Stormwater MG Cal Water w/M
<input type="checkbox"/> <u>Municipal Utilities Dept</u> Don King w/M		<input type="checkbox"/> Other
<input type="checkbox"/> MIS Edgecomb w/M		<input type="checkbox"/> Tech Service
<input type="checkbox"/> File w/M		

MUNICIPAL UTILITIES	
PLAN CHECK	
DATE RECEIVED	4-28-97
Env Control	COG/ALUC
Stormwater MG	Cal Water
Water/SC JOT	w/M
Tech Service	
RELEASED	5/12/97

Enclosed is a PRELIMINARY TENTATIVE MAP of AIRPORT GATEWAY CENTER (TM3-97). We would appreciate your reviewing this proposal and submitting any comments to this office by MAY 9, 1997.

Please answer and return the questionnaire on the back of this page as well as the map.

If you have any questions regarding this proposal please contact ASSISTANT PLANNER JENNY LIAW at (209) 937-8266

To Community Development Department/Planning Division

From. City Manager's Office / Stormwater Div.

Subject Response to tentative map or proposal of _____
Airport Gateway Center (TM3-97)

1 We have reviewed the tentative map or proposal and our comments are as follows

a map is acceptable

b map is not acceptable for reasons itemized below

c other comments

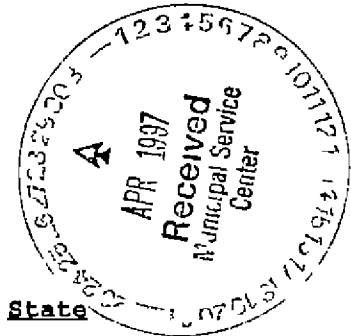
2 Recommendation

3 Person reviewing the map Michael Gutierrez phone 8757

T \PLANNING\DRC2\TM2NDPG

MUNICIPALITY DEVELOPMENT DEPARTMENT
 PLANNING DIVISION
 CITY PERMIT CENTER
 425 NORTH EL DORADO STREET
 STOCKTON CALIFORNIA 95202-1997
 (209) 937-8266

APRIL 28, 1997



City of Stockton

San Joaquin County

State

Comm Dev /Planning
 Ramirez w/MAP

Planning
 w/M

Cal Trans
 w/M

Comm Dev /Building
 Himes w/SM

Public Works
 w/M

Other Agencies

Housing & Redevel Dept
 Pinkerton w/SM

Pub Health Services/
 Env Health w/M

P G & E
 w/M
 w/SM M

Public Works/Engineering
 Sensibaugh w/M
 Meissner w/M

Flood Control
 w/M

P T & T
 w/M

Public Works/Op & Main
 Escobar w/M

Sheriff's Dept
 w/M

Continental Cable
 w/M

School District

SMART
 w/M

Public Works/Solid Waste
 Stephen Chen w/M

Stockton Unified
 w/M

U S Post Office
 w/M

Parks & Recreation
 Machado w/M

Lincoln Unified

LAFCO
 w/M

Fire Department
 Gillis w/M

Lodi Unified

COG/ALUC

Police Department
 Marconi w/M

Manteca Unified

Cal Water
 w/M

Municipal Utilities Dept
 Don King w/M

Other

MIS
 Edgecomb w/M

File w/M

Enclosed is a PRELIMINARY TENTATIVE MAP of AIRPORT GATEWAY CENTER (TM3-97). We would appreciate your reviewing this proposal and submitting any comments to this office by MAY 9, 1997.

Please answer and return the questionnaire on the back of this page as well as the map

If you have any questions regarding this proposal please contact ASSISTANT PLANNER JENNY LIAW at (209) 937-8266

To: Community Development Department/Planning Division

From

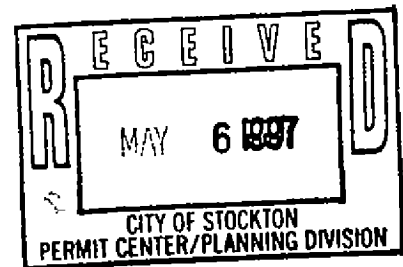
PW/OEM

Subject Response to tentative map or proposal of _____

1 We have reviewed the tentative map or proposal and our comments are as follows

a map is acceptable

b map is not acceptable for reasons itemized below



c other comments

2 Recommendation

3 Person reviewing the map Hecker phone _____

T \PLANNING\DRC2\TM2NDPG

MUNITY DEVELOPMENT DEPARTMENT
 PLANNING DIVISION
 CITY PERMIT CENTER
 425 NORTH EL DORADO STREET
 STOCKTON CALIFORNIA 95202-1997
 (209) 937-8266

APRIL 28 1997

RECEIVED
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City of Stockton

San Joaquin County

State

Comm Dev /Planning
 Ramirez w/MAP

Planning
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Cal Trans
 w/M

Comm Dev /Building
 Himes w/SM

Public Works
 w/M

Other Agencies

Housing & Redevel Dept
 Pinkerton w/SM

Pub Health Services/
 Env Health w/M

P G & E
 w/M
 w/SM M

Public Works/Engineering
 Sensibaugh w/M
 Meissner w/M

Flood Control
 w/M

P T & T
 w/M

Public Works/Op & Main
 Escobar w/M

Sheriff s Dept
 w/M

Continental Cable
 w/M

Public Works/Solid Waste
 Stephen Chen w/M

School Distract

SMART
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Stockton Unified
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U S Post Office
 w/M

Parks & Recreation
 Machado w/M

Lincoln Unified

LAFCO
 w/M

Fire Department
 Gillis w/M

Lodi Unified

COG/ALUC

Police Department
 Marconi w/M

Manteca Unified

Cal Water
 w/M

Municipal Utilities Dept
 Don King w/M

Other

MIS
 Edgecomb w/M

File w/M

Enclosed is a PRELIMINARY TENTATIVE MAP of AIRPORT GATEWAY CENTER (TM3-97). We would appreciate your reviewing this proposal and submitting any comments to this office by MAY 9, 1997.

Please answer and return the questionnaire on the back of this page as well as the map.

If you have any questions regarding this proposal please contact ASSISTANT PLANNER JENNY LIAW at (209) 937-8266.

To Community Development Department/Planning Division

From San Joaquin County Department of Public Works

Subject Response to tentative map or proposal of

AIRPORT GATEWAY CENTER (TM3-97)

1 We have reviewed the tentative map or proposal and our comments are as follows

a map is acceptable _____

b map is not acceptable X for reasons itemized below

See attached sheet for comments

c other comments

2 Recommendation

3 Person reviewing the map Paterno A. Dichupa phone (209) 468-3024

A \FORMS\DRC\2NDPG

NOTES

THE SUBDIVISION, RE-DESCRIPTION OF BLOCK AND PORTION OF BLOCK AND OF TRACT WITHIN THIS SUBDIVISION CONTAINS ACRES. DIMENSIONS ARE GIVEN IN FEET AND INCHES. ALL DIMENSIONS SHALL BE INTERPRETED ACCORDANCE TO THE CITY OF STOCKTON STANDARDS. THIS SUBDIVISION SHALL BE DEVELOPED IN ACCORDANCE WITH THE CITY OF STOCKTON STANDARDS. THE CITY OF STOCKTON HAS REVIEWED THIS SUBDIVISION AND HAS FOUND IT TO BE IN ACCORDANCE WITH THE CITY OF STOCKTON STANDARDS. THE CITY OF STOCKTON HAS REVIEWED THIS SUBDIVISION AND HAS FOUND IT TO BE IN ACCORDANCE WITH THE CITY OF STOCKTON STANDARDS. THE CITY OF STOCKTON HAS REVIEWED THIS SUBDIVISION AND HAS FOUND IT TO BE IN ACCORDANCE WITH THE CITY OF STOCKTON STANDARDS.

LEGEND

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PROPOSED	EXISTING	ADJACENT
PROPOSED	EXISTING	ADJACENT
PROPOSED	EXISTING	ADJACENT

OWNER

PARSONS-CORREIA
1500 10th Street
STOCKTON, CA 95210
PHONE: (209) 938-1100

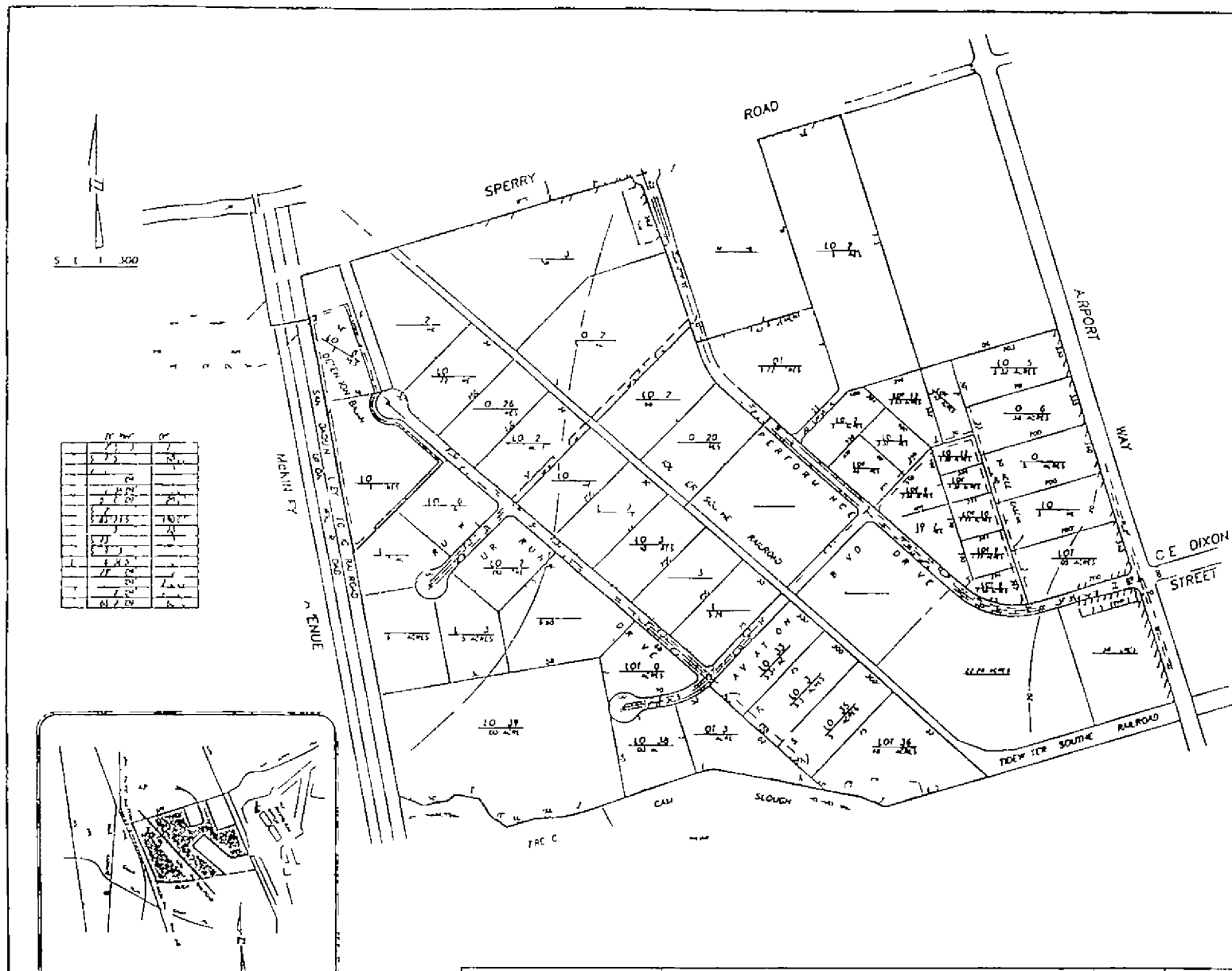
SUBDIVIDER

PARSONS-CORREIA
1500 10th Street
STOCKTON, CA 95210
PHONE: (209) 938-1100

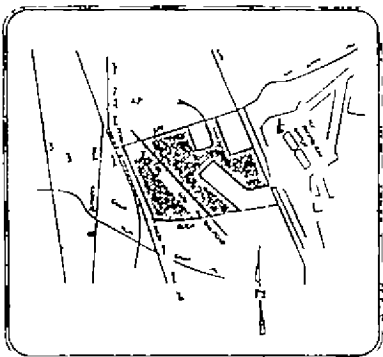
APPROVED BY THE CITY OF STOCKTON PLANNING DIVISION BY _____
OF _____
CITY ENGINEER

APPROVED BY THE CITY ENGINEER OF THE CITY OF STOCKTON BY _____
OF _____
CITY ENGINEER

TENTATIVE MAP
TRACT NO. 2778
SUBDIVISIONS OF SAN JOAQUIN COUNTY
AIRPORT GATEWAY CENTER
BEING PORTION OF SECTION C. W. WEBB GRANT
CITY OF STOCKTON, SAN JOAQUIN COUNTY, CALIFORNIA
JANUARY 1997



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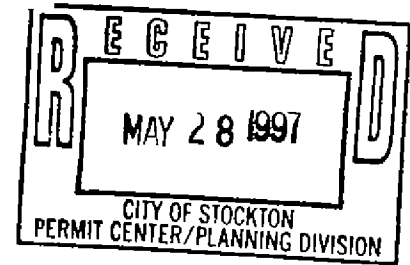


(00 SH ET NO 33)
VICINITY MAP
MAP NO. 304

<p>SIEGFRIED ENGINEERING Inc</p> <p>Civil Engineering Land Surveying Structural Engineering Planning</p>		<p>TENTATIVE MAP</p> <p>AIRPORT GATEWAY CENTER</p> <p>STOCKTON CALIFORNIA</p>	<p>1</p>

MEMORANDUM

*See per used
date June 12, 1997*



May 28, 1997

TO John Carlson Community Development Director
Attention Jenny Liaw Assistant Planner

FROM Gregg S Meissner Senior Transportation Planner

SUBJECT **COMMENTS ON A PRELIMINARY TENTATIVE MAP FOR PANATTONI-CATLIN VENTURE (TM3-97) AIRPORT GATEWAY CENTER**

Public Works staff reviewed the subject preliminary tentative map. Based on their review standard Public Works conditions and conformance to the adopted Standard Plans and Specifications are recommended.

Further, the following information should be forwarded to the owners/developers for their incorporation into the tentative map submittal:

- 1 The right-of-way dedication including appropriate slope easements for the grade separated crossing of Tidewater Southern Railroad/Union Pacific Railroad/McKinley Avenue for a minimum 134-foot wide realigned Sperry Road shall be shown on this map.
- 2 The west right-of-way line of Performance Drive shall be located a minimum of 1600 feet, measured along the centerline of the future Sperry Road alignment east of the east right-of-way line for Sperry Road, unless the project's engineer can demonstrate that a distance less than 1600 feet will be adequate to provide for an at grade intersection of Sperry Road and Performance Drive.
- 3 Emergency access as proposed may not be feasible due to the potential for the area south of existing Sperry Road to be a "fill" area for the grade-separated crossing.
- 4 The internal circulation appears to be inadequate to serve a project of this size and nature. Further, a cursory review by KD Anderson Transportation Consultants of traffic projections revealed that an additional access point to Sperry Road between the proposed Performance Drive and Airport Way is needed to adequately serve the area bounded by Sperry Road, Airport Way, South Little Johns Creek and Union Pacific Railroad.
- 5 The 50-foot wide access easement through the area of Lots 3 through Lots 19 is

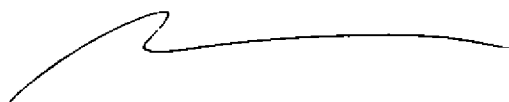
John Carlson
May 28, 1997
Page 2

**COMMENTS ON A PRELIMINARY TENTATIVE MAP FOR PANATTONI-CATLIN
VENTURE (TM3-97) AIRPORT GATEWAY CENTER**

- awkward and may result in the need for additional improvements such as wider street widths or signalization of intersections that would otherwise not be necessary
- 6 The 50-foot wide access easement along the north property line of Lot 1 is not acceptable
 - 7 The Runway Drive cul-de-sac does not meet City standards (The maximum length of cul-de-sacs permitted in industrial zoned areas is 1000 feet)
 - 8 Comply with all Public Utilities Commission requirements related to building setback and adequate sight distances for railroad crossings

The above information is a summary of necessary corrections needed for a complete tentative map and does not constitute Public Works conditions of approval. Tentative map conditions shall be developed when the project's environmental review has been completed and the map is formally submitted for review.

In addition, the attached checklist for Solid Waste issues should be forwarded to the Simpson-Moore Inc. for their use in preparation of the environmental document for this project.



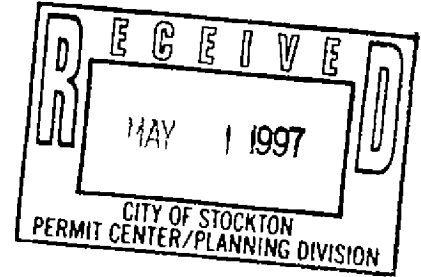
GREGG S. MEISSNER
SENIOR TRANSPORTATION PLANNER

GSM KT/HS

RF JL

E

MUNICIPAL DEVELOPMENT DEPARTMENT
 PLANNING DIVISION
 CITY PERMIT CENTER
 425 NORTH EL DORADO STREET
 STOCKTON, CALIFORNIA 95202-1997
 (209) 937-8266



APRIL 28, 1997

City of Stockton

San Joaquin County

State

___ Comm Dev /Planning
 Ramirez w/MAP

___ Planning
 w/M

___ Cal Trans
 w/M

___ Comm Dev /Building
 Himes w/SM

___ Public Works
 w/M

Other Agencies

___ Housing & Redevel Dept
 Pinkerton w/SM

___ Pub Health Services/
 Env Health w/M

___ P G & E
 w/M
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___ Public Works/Engineering
 Sensibaugh w/M
 ___ Meissner w/M

___ Flood Control
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___ P T & T
 w/M

___ Public Works/Op & Main
 Escobar w/M

___ Sheriff's Dept
 w/M

___ Continental Cable
 w/M

___ Public Works/Solid Waste
 Stephen Chen w/M

School District

___ SMART
 w/M

___ Parks & Recreation
 Machado w/M

___ Stockton Unified
 w/M

___ U S Post Office
 w/M

Fire Department
 Gillis w/M

___ Lincoln Unified

___ LAFCO
 w/M

___ Police Department
 Marconi w/M

___ Lodi Unified

___ COG/ALUC

___ Municipal Utilities Dept
 Don King w/M

___ Manteca Unified

___ Cal Water
 w/M

___ MIS
 Edgecomb w/M

___ Other

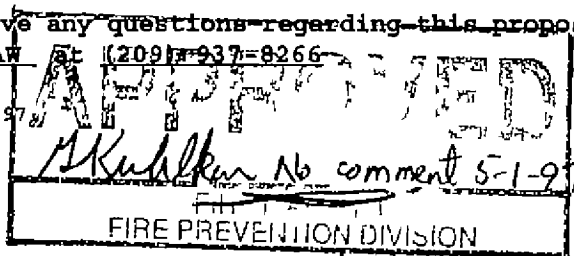
___ File w/M

Enclosed is a PRELIMINARY TENTATIVE MAP of AIRPORT GATEWAY CENTER (TM3-97). We would appreciate your reviewing this proposal and submitting any comments to this office by MAY 9, 1997.

Please answer and return the questionnaire on the back of this page as well as the map

If you have any questions regarding this proposal please contact ASSISTANT PLANNER JENNY LIAW at (209) 937-8266

DRC2\TMREF 97



To: Community Development Department/Planning Division

From: _____

Subject Response to tentative map or proposal of _____

1 We have reviewed the tentative map or proposal and our comments are as follows

a map is acceptable _____

b map is not acceptable _____ for reasons itemized below

c other comments

2 Recommendation

3 Person reviewing the map _____ phone _____

T \PLANNING\DRC2\TM2NDPG

MEMORANDUM

April 28 1997

TO See Attached Referral List

FROM Sam Mah Deputy Director
Community Development Department Planning Division

SUBJECT **REVIEW OF PRELIMINARY TENTATIVE MAP FOR AIRPORT GATEWAY
CENTER (WEST SIDE OF AIRPORT WAY AND NORTH OF FRENCH CAMP
SLOUGH)**

Siegfried Engineers has submitted a tentative map to subdivide the above-noted property containing approximately 328 acres into 48 parcels for industrial development. We have been asked to review the preliminary document and provide comments regarding its completeness and consistency with applicable code requirements. This preliminary review is not intended to be exhaustive or take the place of the formal tentative map review that will occur when the project's environmental review has been completed. Please provide your comments to the Community Development Department Planning Division no later than May 9, 1997.

The following information (and attached zoning and tentative maps) regarding the site's background may be helpful in your review of the tentative map. The area containing approximately 516 acres located on the south side of Sperry Road and west of Airport Way was annexed and rezoned in 1983 to E-P Enterprise Performance District, A-1 Agricultural District, and M-2 Heavy-Industrial District (A-82-2 and Z-12-83). At the same time, a tentative map (TM29-83) was filed to subdivide a 447-acre portion of the 516-acre site into 84 parcels for the development of an industrial/office park. The tentative map expired due to no final map having been recorded. The applicant is now proposing to subdivide a 328-acre portion site of this 447 acres for the development of industrial uses. All of the above maps (A-82-2, Z-12-83 and TM29-83) were processed with an Environmental Impact Report (EIR 3-82).

Should you have any questions regarding the tentative map, please contact Assistant Planner Jenny Liaw at 937-8266.

JOHN CARLSON, DIRECTOR
COMMUNITY DEVELOPMENT DEPARTMENT

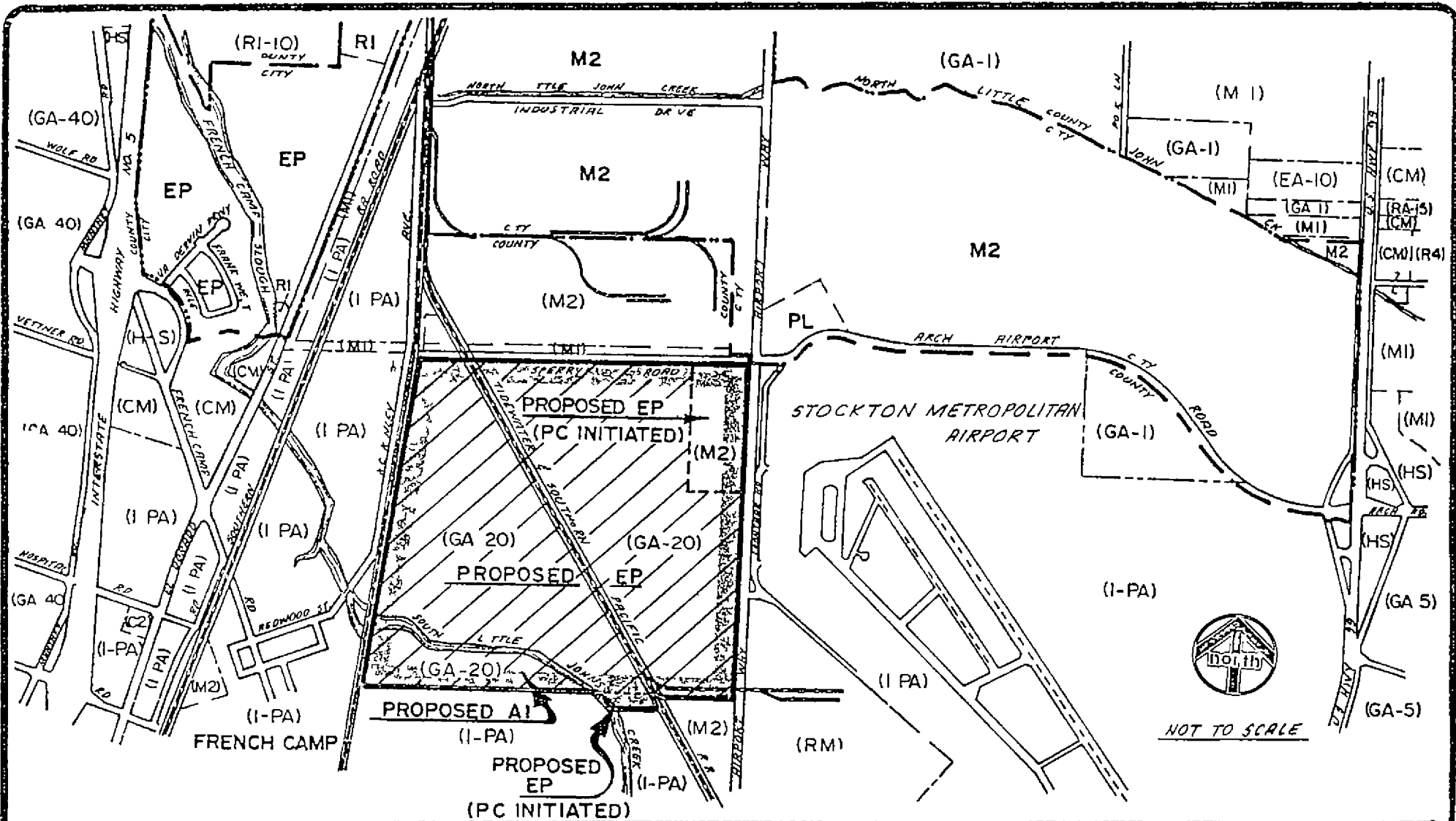


SAM MAH, DEPUTY DIRECTOR
COMMUNITY DEVELOPMENT DEPARTMENT

JC SM rw

Attachments

T:\PLANNING\MEMO RW\TM3 97 PRE



ZONING MAP

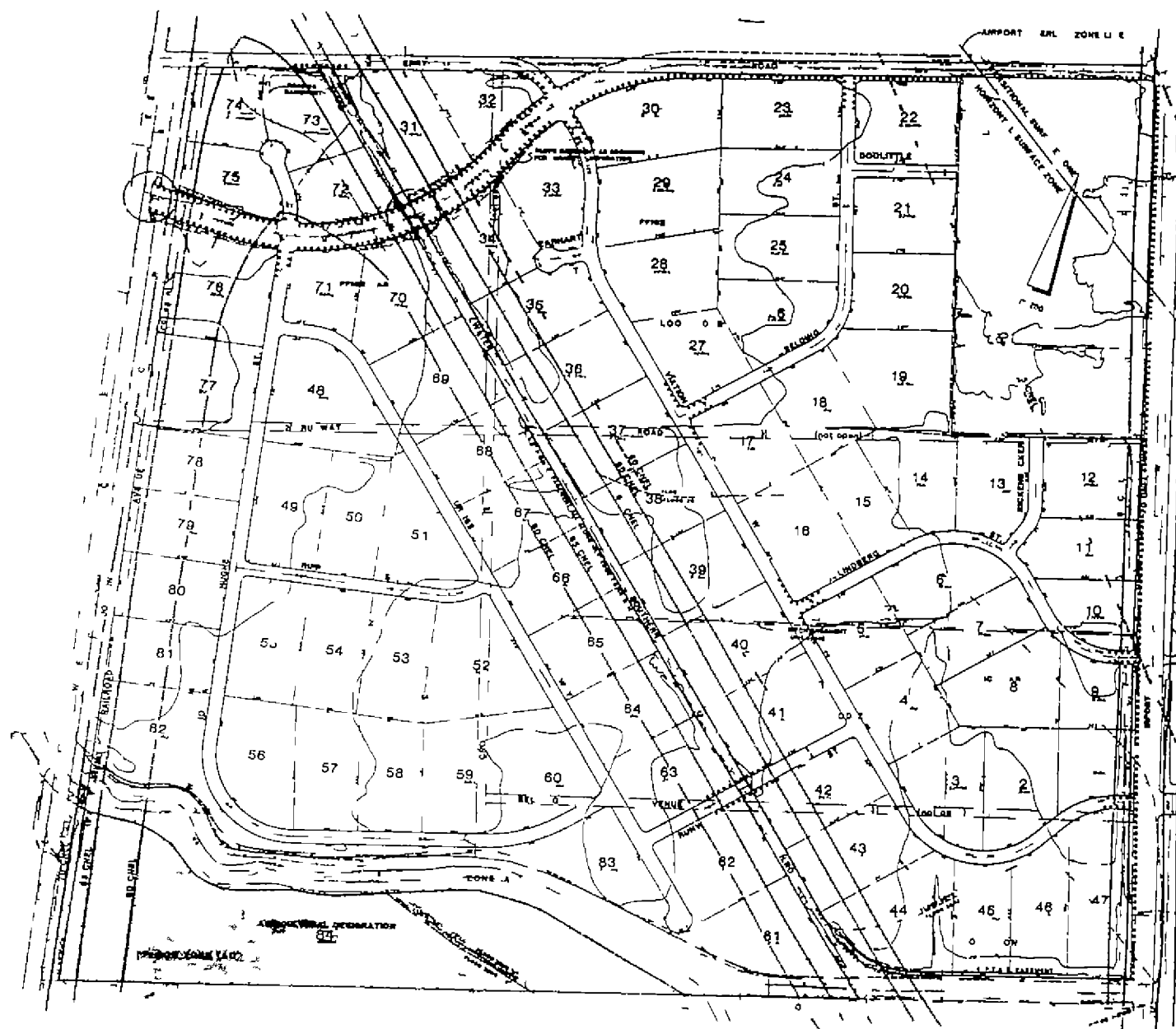
LEGEND

STOCKTON AIRPORT INDUSTRIAL, LTD.
and CITY of STOCKTON - EIR 3-82



PROPOSED ANNEXATION AREA (516 Acres)
 PROPOSED INDUSTRIAL SUBDIVISION (447 Acres)
 M2 CITY ZONING
 (M2) COUNTY ZONING

STOCKTON CITY PLANNING COMMISSION



TRACT No 1838

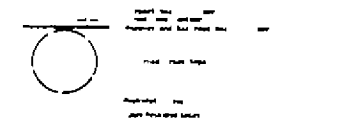
AIRPORT GATEWAY CENTER
A BUSINESS PARK

BY ORDER OF THE BOARD OF SUPERVISORS
CITY OF STOCKTON
CALIFORNIA

PERTINENT DATA

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LEGEND



SIGNATURES

LAND PLANNING

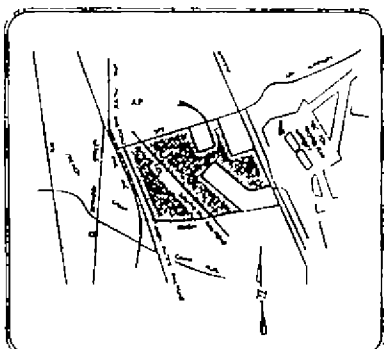
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CIVIL ENGINEERING
ORG No DL QARE
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TM 29-83 (AMENDED)

STOCKTON WATER UTILITIES DEPARTMENT
CITY OF STOCKTON
1000 N. G ST.
STOCKTON, CALIF. 95202
TEL: (209) 477-1000



NO	BEARING	DISTANCE
1	S 71° 12' E	11.2
2	S 81° 00' E	11.0
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6	S 81° 00' E	11.0
7	S 81° 00' E	11.0
8	S 81° 00' E	11.0
9	S 81° 00' E	11.0
10	S 81° 00' E	11.0
11	S 81° 00' E	11.0
12	S 81° 00' E	11.0
13	S 81° 00' E	11.0
14	S 81° 00' E	11.0
15	S 81° 00' E	11.0
16	S 81° 00' E	11.0
17	S 81° 00' E	11.0
18	S 81° 00' E	11.0
19	S 81° 00' E	11.0
20	S 81° 00' E	11.0



LOCALITY MAP
(D SHEET NO 33)



NOTES

1. THIS SUBDIVISION IS A SUBDIVISION OF BLOCK AND PORTIONS OF BLOCK AND OF TRACTS WITHIN THE CITY OF STOCKTON, CALIFORNIA.

2. THIS SUBDIVISION CONTAINS 216 ACRES.

3. THE LOTS ARE REFERRED TO BY BLOCK AND LOT NUMBER.

4. PUBLIC IMPROVEMENTS WILL BE INSTALLED IN ACCORDANCE WITH THE CITY OF STOCKTON ORDINANCES.

5. OTHER NOTES: (a) THE CITY ENGINEER SHALL BE ADVISED BY THE SUBDIVIDER OF THE LOCATION OF ALL UTILITIES AND OF THE LOCATION OF ALL EASEMENTS AND RIGHTS OF WAY.

6. THE SUBDIVIDER SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UTILITIES AND RIGHTS OF WAY.

7. THE SUBDIVIDER SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL EASEMENTS AND RIGHTS OF WAY.

8. THE SUBDIVIDER SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL EASEMENTS AND RIGHTS OF WAY.

9. THE SUBDIVIDER SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL EASEMENTS AND RIGHTS OF WAY.

10. THE SUBDIVIDER SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL EASEMENTS AND RIGHTS OF WAY.

LEGEND

---	1/2" PL. BOUNDARY LINE
---	1/4" PL. BOUNDARY LINE
---	PROPOSED BOUNDARY LINE
---	PROPOSED LOT BOUNDARY
---	PROPOSED STATE ROAD

OWNER

PROPOSITION-CALIFORNIA WEBER GRANT
3470 AIR DIXON BLVD SUITE 10
STOCKTON, CA 95210-3888
PHONE 916-922-3800
FAX 916-922-3800

SUBDIVIDER

PROPOSITION-CALIFORNIA WEBER GRANT
3470 AIR DIXON BLVD SUITE 10
STOCKTON, CA 95210-3888
PHONE 916-922-3800
FAX 916-922-3800

AS SHOWN ON THIS MAP THIS _____ DAY OF _____ 1997

CITY OF STOCKTON PLANNING COMMISSION

APPROVED BY THE CITY OF STOCKTON PLANNING COMMISSION THIS _____ DAY OF _____ 1997

Chairman

APPROVED BY THE CITY ENGINEER OF THE CITY OF STOCKTON THIS _____ DAY OF _____ 1997

CITY ENGINEER

TENTATIVE MAP
TRACT NO 2778
SUBDIVISIONS OF SAN JOAQUIN COUNTY
AIRPORT GATEWAY CENTER
BEING PORTION OF SECTION 25, T. WEBER GRANT
CITY OF STOCKTON, SAN JOAQUIN COUNTY, CALIFORNIA
JANUARY 1997

<p>SIEGFRIED ENGINEERING, Inc</p> <p>Civil Engineering Land Surveying Instrumental Engineering Planning</p> <p>Stockton, CA 95210-3888 916-922-3800 FAX 916-922-3800</p>		<p>TENTATIVE MAP</p> <p>AIRPORT GATEWAY CENTER STOCKTON CALIFORNIA</p>	<p>1</p>
---	--	---	----------

**CITY OF STOCKTON**

COMMUNITY DEVELOPMENT DEPARTMENT
CITY HALL
425 N. EL DORADO STREET
STOCKTON, CA 95202-1997

937-8266

April 17, 1997

Michael Hakeem Attorney
Law Offices of Hakeem Ellis & Simonelli
2800 West March Lane Suite 200
Stockton, CA 95219

**STATUS OF TENTATIVE MAP (TM3-97) APPLICATION AND ENVIRONMENTAL
PROCESS FOR AIRPORT GATEWAY CENTER PROJECT**

(Response to your letter dated 3/12/97)

This letter is in response to your request to waive applicable Government Code and/or Subdivision Map Act statutes regarding timely processing of the tentative map application for the Airport Gateway Center project for property located west of Airport Way and south of Sperry Road. The matter was forwarded to the City Attorney for a response. In their response, the City Attorney's office indicated that the time limits for processing the tentative map application may be waived. Therefore, planning staff has accepted the tentative map application and a receipt for the proposed project has been issued (attached). Per your request, the tentative map will be referred to other City Departments and affected agencies for preliminary review to obtain comments regarding its completeness and consistency with applicable code requirements. You will receive the comments on the proposed project in the next few weeks. However, in accordance with your request for a waiver to the statutes regarding the processing of the application and due to the need for supplemental environmental documentation, the application has not been deemed complete and is being held in abeyance.

On April 9, 1997, City staff met with you to discuss focus areas for the environmental document and related technical studies for the proposed project. As a result of that meeting, it was determined that a Supplement to a prior EIR3-82 must be prepared and circulated concurrently with the processing of the tentative map application. Therefore, the tentative map shall not be set for hearing pending completion of the processing of the Supplemental EIR. The updated environmental information shall include, but not be limited to, the following:



Michael Hakeem, Attorney

April 17, 1997

Page 2

- 1 Provide a proposed project description including project changes subdivision design construction schedule and location of the proposed project Describe proposed/required on site and off site improvements necessary to facilitate project implementation
- 2 Prepare a new/updated traffic/circulation analysis, including the potential effects of heavy truck traffic and other vehicular activity in connection with the proposed project railroad crossing at Tidewater and Southern Railroad tracks and proposed mitigation measures
- 3 Provide an updated noise study prepared by an acoustical engineer that will identify noise levels that will be produced by industrial land uses on the subject site related transportation noise the potential effect on adjacent existing and expected noise-sensitive land and proposed mitigation measures
- 4 Provide a new/updated air quality assessment for the proposed project and use currently acceptable modeling as required by the San Joaquin Valley Air Pollution Control District (SJVAPCD)
- 5 Retain a qualified wildlife biologist to conduct a new biological site survey and provide an updated biological report which revises and updates the prior assessment of potential impacts and proposed mitigation measures on biological resources
- 6 Describe any important on-site and/or adjacent topographical and water feature including improvements to French Camp Slough and dedication along French Camp Slough Update flood zone information for the proposed project site as well as water quality impacts and mitigation measures
- 7 Discuss new or expanded/modified public facilities utilities and infrastructure for the proposed project site including the need for and location of a proposed detention basin Please describe the size and location of the detention basin and the point of terminal drainage Specifically describe and analyze the environmental effects of and related mitigation measures for any proposed infrastructure/utility master plans including both on site and off site improvements

The Supplement to the prior EIR3 82 may be accompanied by a Mitigation Agreement executed between the City and the property owner to provide assurances that the identified mitigation measures will in fact be implemented Recordation of said Mitigation Agreement may be a condition of the tentative map

In conclusion the existing tentative map application will be held in abeyance pending completion of the required environmental documentation and review process Upon

Michael Hakeem, Attorney

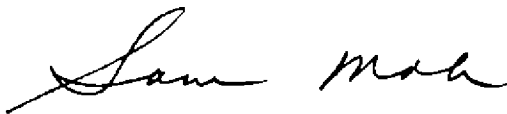
April 17, 1997

Page 3

submittal of the administrative draft of the Supplemental EIR this office will require a filing fee of \$4 480 for review and processing of the revised environmental documentation for the project In addition please be advised that a California Department of Fish and Game Fee of \$875 (payable to the "San Joaquin County Clerk") must also be submitted

Should you have any questions regarding the matter please contact Senior Planner Mike Niblock or Assistant Planner Jenny Liaw at 937-8266

JOHN CARLSON DIRECTOR
COMMUNITY DEVELOPMENT DEPARTMENT



SAM MAH DEPUTY DIRECTOR
PLANNING DIVISION

JC SM cl

cc Lex Corrales
Siegfried Engineering Inc
4045 Coronado Avenue
Stockton CA 95204-2396

Charles Simpson Principal
Simpson Moore Inc
555 West Benjamin Holt Drive Suite 414
Stockton CA 95207

emc Bob Sivell Enterprise Zone Manager
Guy Petzold Deputy City Attorney
Reed Hogan Junior Engineer Public Works Department
Greg Meissner Senior Transportation Planner Public Works Department

SM/RL

RECEIVED

MN

Law Offices Of

HAKEEM, ELLIS & SIMONELLI

A Professional Corporation

MAR 13 1997

CITY OF STOCKTON
COMMUNITY DEVELOPMENT DEPT.

2800 West March Lane
Suite 200
Stockton California 95219
TEL 209 474 2800
FAX 209 474 3654

Michael D Hakem
Albert M Ellis
Rena M Maringo
Peter W Manton
Francis J Johnson PL

March 12, 1997

James J Simonelli
(Retired)

JOHN CARLSON
City of Stockton
Community Development Dept
425 N El Dorado Street
Stockton, CA 95202

Re **STOCKTON AIRPORT GATEWAY PROJECT**
Our Client **Panattoni-Catlin Venture**
Our File No **04921 1**

Dear Mr Carlson

As a follow-up to our most recent telephone conversations, in connection with the above matter, this correspondence will confirm that we have met and consulted with Charlie Simpson of Simpson Moore, Inc, regarding CEQA compliance for that certain Tentative Map previously submitted for review and approval. Mr Simpson and I have discussed a scope of work and we intend to meet with your department in the immediate future to review same. As discussed, we would respectfully request that you retain the application fee check and the Tentative Map in anticipation of our meeting to discuss CEQA compliance. We are willing to waive any applicable Government Code and/or Subdivision Map Act statutes regarding your responsibility to respond to the application. As discussed, we can review the statute issue during our meeting and provide further particulars regarding an appropriate agreed upon timeline for your response.

If there are any questions, please do not hesitate to call

Very truly yours,

HAKEEM, ELLIS & SIMONELLI
A Professional Corporation

By

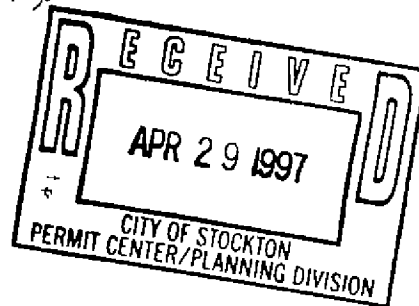


MICHAEL D HAKEEM

MDH dp
cc Bob Sivell

COMMUNITY DEVELOPMENT DEPARTMENT
 PLANNING DIVISION
 CITY PERMIT CENTER
 425 NORTH EL DORADO STREET
 STOCKTON CALIFORNIA 95202-1997
 (209) 937-8266

APRIL 28 1997



City of Stockton

San Joaquin County

State

Comm Dev /Planning
 Ramirez w/MAP

Planning
 w/M

Cal Trans
 w/M

Comm Dev /Building
 Himes w/SM

Public Works
 w/M

Other Agencies

Housing & Redevel Dept
 Pinkerton w/SM

Pub Health Services/
 Env Health w/M

P G & E
 w/M
 w/SM M

Public Works/Engineering
 Sensibaugh w/M
 Meissner w/M

Flood Control
 w/M

P T & T
 w/M

Public Works/Op & Main
 Escobar w/M

Sheriff's Dept
 w/M

Continental Cable
 w/M

Public Works/Solid Waste
 Stephen Chen w/M

School District

SMART
 w/M

Stockton Unified
 w/M

U S Post Office
 w/M

Parks & Recreation
 Machado w/M

Lincoln Unified

LAFCO
 w/M

Fire Department
 Gillis w/M

Lodi Unified

COG/ALUC

Police Department
 Marconi w/M

Manteca Unified

Cal Water
 w/M

Municipal Utilities Dept
 Don King w/M

Other

MIS
 Edgecomb w/M

File w/M

Enclosed is a PRELIMINARY TENTATIVE MAP of AIRPORT GATEWAY CENTER (TM3-97). We would appreciate your reviewing this proposal and submitting any comments to this office by MAY 9, 1997.

Please answer and return the questionnaire on the back of this page as well as the map

If you have any questions regarding this proposal, please contact ASSISTANT PLANNER JENNY LIAW at (209) 937-8266

To Community Development Department/Planning, Division

From

Ben Hoag Director

Subject: Response to tentative map or proposal of _____

1 We have reviewed the tentative map or proposal and our comments are as follows

a map is acceptable

b map is not acceptable for reasons itemized below

c other comments

2 Recommendation

- S.W.-1. (STORM WATER)
Plan to be developed as early as possible
& forwarded to Bldg Div & PDW
- Proposed DEU SHOULD BE
DESIGNATED ON SITES / 60' yards.
- X.M. conditions SHOULD BE
enforced during DEVELOPMENT.

3 Person reviewing the map

Jim

phone

8521

T \PLANNING\DRC2\TM2NDPG

COMMUNITY DEVELOPMENT DEPARTMENT
 PLANNING DIVISION
 CITY PERMIT CENTER
 425 NORTH EL DORADO STREET
 STOCKTON CALIFORNIA 95202-1997
 (209) 937-8266

APRIL 28 1997

City of Stockton

San Joaquin County

State

___ Comm Dev /Planning
 Ramirez w/MAP

___ Planning
 w/M

___ Cal Trans
 w/M

___ Comm Dev /Building
 Himes w/SM

___ Public Works
 w/M

Other Agencies

___ Housing & Redevel Dept
 Pinkerton w/SM

___ Pub Health Services/
 Env Health w/M

___ P G & E
 w/M
 ___ w/SM M

___ Public Works/Engineering
 Sensibaugh w/M
 Meissner w/M

___ Flood Control
 w/M

___ P T & T
 w/M

___ Public Works/Op & Main
 Escobar w/M

___ Sheriff's Dept
 w/M

~~___ Continental Cable
 w/M~~

___ Public Works/Solid Waste
 Stephen Chen w/M

School District

___ SMART
 w/M

___ Parks & Recreation
 Machado w/M

___ Stockton Unified
 w/M

___ U S Post Office
 w/M

___ Fire Department
 Gillis w/M

___ Lincoln Unified

___ LAFCO
 w/M

___ Police Department
 Marconi w/M

___ Lodi Unified

___ COG/ALUC

___ Municipal Utilities Dept
 Don King w/M

___ Manteca Unified

___ Cal Water
 w/M

___ MIS
 Edgecomb w/M

___ Other

___ File w/M

Enclosed is a ~~PRELIMINARY TENTATIVE MAP OF AIRPORT GATEWAY CENTER (TM3-97)~~ We would appreciate your reviewing this proposal and submitting any comments to this office by MAY 9, 1997

Please answer and return the questionnaire on the back of this page as well as the map

If you have any questions regarding this proposal please contact ASSISTANT PLANNER JENNY LIAW at (209) 937-8266

DRC2\TMREF 97

2/15/97

5

To Community Development Department/Planning Division

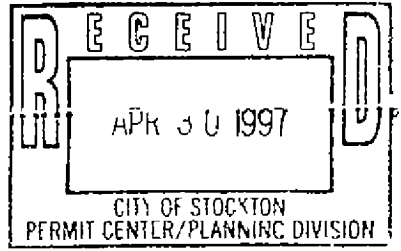
From: Continental Cablevision

Subject Response to tentative map or proposal of Autway

1 We have reviewed the tentative map or proposal and our comments are as follows

a map is acceptable

b map is not acceptable for reasons itemized below



c other comments

2 Recommendation

Supply joint trench map

3 Person reviewing the map [Signature] phone 473-4955

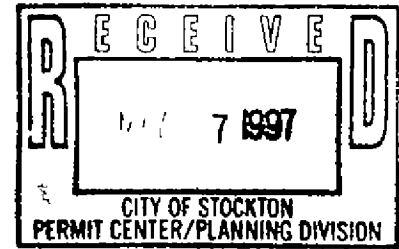
T \PLANNING\DRC2\TM2NDPG

Pacific Gas and Electric Company

Land Rights Office
P O Box 930
Stockton CA 95201

May 6 1997

Ms Jenny Liaw
Community Development Department
City of Stockton
425 North El Dorado Street
Stockton CA 95202-1997



**RE Tentative Map for Airport Gateway Center File # TM 3-97
97015st**

Dear Ms Liaw

PG&E has reviewed the proposal and has the following comment

Dedicate a 10 foot wide Public Utility Easement adjacent to all public and private streets for underground facilities and appurtenances

If you have any questions please contact me at (209) 942-1650

Sincerely

A handwritten signature in cursive script, appearing to read 'Michael Gunby'.

Michael Gunby
Land Agent

Planning

4411 - 150

Sam



July 7, 1997

RECEIVED

COUNTY OF SAN JOAQUIN
DEPARTMENT OF AVIATION

Dan DeAngelis
Airport Manager

JUL 8 1997

CITY OF STOCKTON
COMMUNITY DEVELOPMENT DEPT.]

Mr John Carlson Director
Community Development
City of Stockton
345 North El Dorado Street
Stockton CA 95202

Subject Airport Gateway Center

Dear John

In reference to the proposed detention pond for the anticipated Airport Gateway Center project the San Joaquin County Stockton Metropolitan Airport has no objection to the project as outlined in Preliminary Tentative Map Drawing No G-5231

You will note that the current Stockton Metropolitan Airport flight pattern is not on direct departure for turbo-jet aircraft but may possibly conflict with single and twin engine general aviation aircraft departures and arrivals relative to the anticipated location of the proposed detention pond

Consequently there is also no concern with the County of San Joaquin Stockton Metropolitan Airport in regard to the referenced time to drain It is our position however that mitigating measures should be taken to discourage wildlife habitat (ie birds seagulls ducks geese etc) in and around the Airport environs by denying food, water and roosting areas The following are examples of possible mitigating measures which may be useful in securing conformity to detract water fowl

Pond Coverings

- Fish line grid
- Netting
- Wire grid
- Vinyl shake screen

Thank you for the opportunity to comment on this mutually-beneficial industrial project If I can provide you with any further information or data relative to the proposed Airport Gateway Center venture, please do not hesitate to contact me

Sincerely

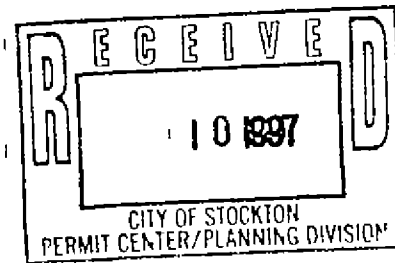
Dan DeAngelis
Dan DeAngelis
Airport Manager

DD dlw

c Siegfried Engineering Inc

MEMORANDUM

June 9 1997



TO Jenny Liaw, Assistant Planner
FROM Mark J Madison Senior Civil Engineer
SUBJECT AIRPORT GATEWAY CENTER (TM3-97)

We have reviewed the proposed subject tentative map and have several comments. In general, from a utility service standpoint, we find this tentative map acceptable subject to the following conditions:

- 1 The owners, developers, and/or successors in interest shall submit subdivision master water, storm, and sanitary sewer plans. These plans, and the facilities on them, shall conform to the Stockton Municipal Code and City of Stockton design standards and be acceptable to the Director of Municipal Utilities and approved by the City Engineer prior to the approval of any final map. No subsequent changes shall be made to these plans without the approval of the Director of Municipal Utilities and the City Engineer.
- 2 The owners, developers, and/or successors in interest shall design and install water system improvements in accordance with the 1985 Master Plan Update and any amendments or updates thereto. The owners, developers, and/or successors in interest shall provide an engineering analysis, acceptable to the Director of Municipal Utilities, that demonstrates that the water system improvements to be constructed in conjunction with this project are sufficient to meet the following conditions:
 - a With a given system pressure of 45 psi at the point of connection to the City water system, the water system improvements shall provide at least 40 psi pressure at any location during the period of peak hour demand, and
 - b With a given system pressure of 45 psi at the point of connection to the City water system, the water system improvements shall provide at least 20 psi pressure at any location during the period of Maximum Day demand combined with a fire flow of 2000 gpm out of any fire hydrant in the subdivision.
- 3 The owners, developers, and/or successors in interest shall provide legal access, to the City, for all public facilities to be owned, operated, and maintained by the City.

June 9, 1997
Jenny Liaw
Page 2

AIRPORT GATEWAY CENTER (TM3-97)

Please incorporate these attached conditions into the City's conditions for this tentative map
If you have any questions, please call me at x 8782

MORRIS L ALLEN
DIRECTOR OF MUNICIPAL UTILITIES

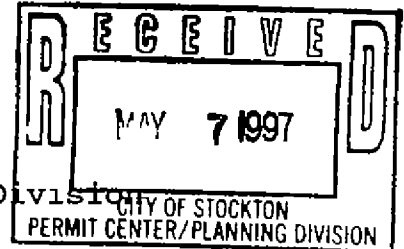


MARK J MADISON, P E
SENIOR CIVIL ENGINEER

MJM mm

cc Ken Moeckel, Senior Civil Engineer/Public Works

KE/SL



SM

To Community Development Department/Planning Division
From San Joaquin County Flood Control and Water Conservation District

Subject Response to tentative map or proposal of AIRPORT GATEWAY CENTER (TM3-97)

1 We have reviewed the tentative map or proposal and our comments are as follows

a map is acceptable x

b map is not acceptable for reasons itemized below

c other comments

2 Recommendation It is recommended that terminal outflow from the detention pond be designed to maintain existing runoff levels from the undeveloped condition of the area consistent with the San Joaquin County Flood Control and Water Conservation District Interim Flood Hazard Reduction Policy (Attached for reference) or a drainage study be conducted which defines the potential flooding impacts to downstream properties due to any increase in flow from this development

3 Person reviewing the map Dave Thompson Telephone 468-3060

SAN JOAQUIN COUNTY FLOOD CONTROL
AND WATER CONSERVATION DISTRICT

INTERIM FLOOD HAZARD REDUCTION POLICY

PURPOSE

The purpose of the Interim Flood Hazard Reduction Policy is to ensure that existing drainage facilities are not used beyond the capacity of the flood control facilities. The policy will be followed in the evaluation of drainage from all proposed new projects.

APPLICATION

The policy shall apply to all new and existing drainage facilities which discharge (1) directly to the San Joaquin River, (2) to a Delta channel, or (3) to any flood control channel or tributary to a flood control channel maintained by a zone of the San Joaquin County Flood Control District.

This policy does not apply to any non-regulated, agricultural drainage improvement. The agricultural developments that will come under review of this policy are listed in Chapter 9-605 (Agricultural Zones, Use Regulations) of the San Joaquin County Development Title. This policy does not apply to any new development which will discharge to an existing facility that discharges to one of the above mentioned receiving streams, provided there is no increase in size or design capacity of the existing discharge facility.

DURATION

The policy will be in effect until a plan has been developed to convey, within community established risk standards, the drainage discharge resulting from the development of lands in the County expected to exist in the year 2010.

POLICY

It is the policy of the San Joaquin County Flood Control District to require that anyone who proposes a new or increased discharge of storm water to any of the above mentioned receiving streams, perform hydrologic and hydraulic analyses to demonstrate the following:

For Discharge to Leveed Channels

The proposed discharge, in combination with the existing runoff, will not cause water levels to encroach into the minimum freeboard space specified by the Federal Emergency Management Agency (FEMA) regulations. For channels that are designed to pass floods larger than the FEMA 100-year base flood, the proposed new discharge will not cause encroachment into the design freeboard

OR

The proposed discharge to a leveed channel will not raise the existing maximum water surface elevation resulting from existing discharges and natural flows at any point in stream

For Discharge to Unleveed Channels

If an unleveed channel is not capable of conveying the maximum 100-year peak discharge with one foot of freeboard, when considering the development that is anticipated to exist in the year 2010, the proposed discharge will not increase the peak discharge in the channel

If an unleveed channel is capable of conveying the maximum 100-year peak discharge with one foot of freeboard, when considering the development that is anticipated to exist in the year 2010, the proposed discharge can be added to the channel provided that the one foot of freeboard is maintained

Hydraulic analyses shall demonstrate through the use of hydrologic and hydraulic analyses used in the development of the Community Flood Insurance Studies, that all property, both downstream and upstream of the discharge, will not be subject to a higher flood level as a result of the proposed drainage

BEFORE THE BOARD OF SUPERVISORS OF THE COUNTY OF SAN JOAQUIN
STATE OF CALIFORNIA

R-91-473

WHEREAS, the Board of Supervisors shall establish minimum requirements for the approval of development projects

NOW, THEREFORE, BE IT RESOLVED by this Board of Supervisors that it does hereby rescind Resolution R-91-258, and adopt the following policy statement regarding water supply for future developments

"Any General Plan Amendment approved by the County shall not result in increased demand upon the water supply available to San Joaquin County as of April 2, 1991 "

This policy shall apply to

- (1) Any amendments to the County General Plan 1995, except applications submitted prior to September 19, 1989, for projects less than twenty acres
- (2) Any changes to the Draft General Plan 2010, as it existed on February 1, 1991, and having Planning Commission motions of intent to recommend adoption by the Board of Supervisors, and
- (3) Any changes to the new General Plan 2010 after adoption by the Board of Supervisors

Documentation adequate to enable the Department of Public Works to determine conformance with this policy shall be provided as required

BE IT FURTHER RESOLVED that the following Implementation Plan be adopted

Implementation Plan

- 1 Documentation adequate to enable the Department of Public Works to determine conformance with this policy shall be provided including, but not limited to, the following
 - a The proposed annual water demand for any land use included in a General Plan Amendment will be defined

in detail at the time of submittal of the proposed General Plan Amendment to the County for consideration

- b A connection to a water source for an emergency operation or redundant supply will not be considered as an increased demand on the water supplies available to the County
- c The source(s) of the proposed water supply to be used to meet the water demand of the proposed project will be identified at the time of submittal of the proposal for a General Plan Amendment
- d If a proposed water supply for a General Plan Amendment is to be met by the retirement of a water demand for the water supply available to San Joaquin County as of April 2, 1991, documentation of the amount and purpose of use of the existing water demand proposed to be retired must be submitted. The amount of the existing water demand to be retired shall be documented by historic flow records or other supportable information of the existing water demand to be retired as described below

(1) The retirement of a non-agricultural water demand shall be documented by the historical average water use over the immediate preceding twenty years. Documentation may include actual water use records, information detailing the normal water uses for the processes involved, prorated share of service area water use, pumping records, or comparable recorded data for the same or similar demand

(2) The retirement of an agricultural use shall be documented by the historical average water use over the immediate preceding twenty years. Documentation may include pumping records, district water service records, or an estimation of applied water based on the crops or combination of crops grown

Record of crop shall include documentation such as pesticide permits, cannery records, Agricultural Stabilization and Conservation Service Records or other supportable documentation

e A water demand proposed to be retired shall be within the boundary of the proposed project

f A recordable agreement with the County shall be entered into which evidences that the retired water demand will remain retired for the duration of the proposed project included in the General Plan Amendment

2 For a General Plan Amendment (GPA) proposed for commercial or industrial land uses, the accompanying General Plan text, or other methods adopted concurrently with the GPA (such as a Development Agreement), shall specify the water use permitted. In lieu of this, the maximum water usage for the designation being sought will be assumed

PASSED AND ADOPTED NOV 26 1991, by the following vote of the Board of Supervisors, to wit

AYES WILHOIT COSTA SOLSA SHERS D-BEER

NOES NONE

ABSENT NONE

GEORGE L BARBER

ATTEST JORETTA J HAYDE
Clerk of the Board of
Supervisors of the County
of San Joaquin, State of
California

GEORGE L BARBER, Chairman of
the Board of Supervisors
County of San Joaquin
State of California

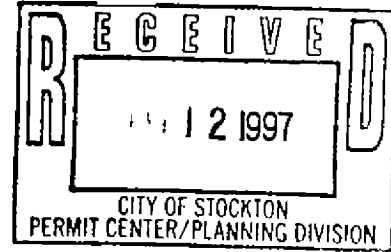
By CINDY DUBRUTZ
Deputy Clerk



2/1/97

5

MEMORANDUM



June 12, 1997

TO John Carlson Community Development Director
Attention Jenny Liaw, Assistant Planner

FROM Gregg S Meissner Senior Transportation Planner

SUBJECT **REVISED COMMENTS ON A PRELIMINARY TENTATIVE MAP FOR PANATTONI-CATLIN VENTURE (TM3-97) AIRPORT GATEWAY CENTER**

Public Works staff reviewed the subject preliminary tentative map. Based on their review, standard Public Works' conditions and conformance to the adopted Standard Plans and Specifications are recommended.

Further, the following information should be forwarded to the owners/developers for their incorporation into the tentative map submittal:

- 1 The right-of-way dedication including appropriate slope easements for the grade separated crossing of Tidewater Southern Railroad/Union Pacific Railroad/McKinley Avenue, for a minimum 134-foot wide, realigned Sperry Road shall be shown on this map.
- 2 The west right-of-way line of Performance Drive shall be located a minimum of 1600 feet, measured along the centerline of the future Sperry Road alignment, east of the east right-of-way line for Sperry Road, unless the project's engineer can demonstrate that a distance less than 1600 feet will be adequate to provide for an at grade intersection of Sperry Road and Performance Drive.
- 3 Emergency access as proposed may not be feasible due to the potential for the area south of existing Sperry Road to be a "fill" area for the grade-separated crossing.
- 4 The internal circulation appears to be inadequate to serve a project of this size and nature. Further, a cursory review, by KD Anderson Transportation Consultants of traffic projections revealed that an additional access point to Sperry Road between the proposed Performance Drive and Airport Way is needed to adequately serve the area bounded by Sperry Road, Airport Way, ~~South Little John Creek~~ **French Camp Slough** and Union Pacific Railroad.
- 5 The 50-foot wide access easement through the area of Lots 3 through Lots 19 is

3'

10

John Carlson
June 12, 1997
Page 2

REVISED COMMENTS ON A PRELIMINARY TENTATIVE MAP FOR PANATTONI-CATLIN VENTURE (TM3-97) AIRPORT GATEWAY CENTER

awkward and may result in the need for additional improvements such as wider street widths or signalization of intersections that would otherwise not be necessary

- 6 The 50-foot wide access easement along the north property line of Lot 1 is not acceptable
- 7 The Runway Drive cul-de-sac does not meet City standards (The maximum length of cul-de-sacs permitted in industrial zoned areas is 1000 feet)
- 8 Comply with all Public Utilities Commission requirements related to building setback and adequate sight distances for railroad crossings

The above information is a summary of necessary corrections needed for a complete tentative map and does not constitute Public Works conditions of approval. Tentative map conditions shall be developed when the project's environmental review has been completed and the map is formally submitted for review.

In addition, the attached checklist for Solid Waste issues should be forwarded to the Simpson-Moore, Inc. for their use in preparation of the environmental document for this project.



GREGG S. MEISSNER
SENIOR TRANSPORTATION PLANNER

GSM KT/HS

CITY OF STOCKTON
DEPARTMENT OF PUBLIC WORKS

SOLID WASTE ISSUES TO BE DISCUSSED IN EIR FOR DEVELOPMENT PROJECTS

I Waste Generation Analysis

Discussion of types and estimated quantities of solid waste and hazardous waste that will be generated

II Waste Storage

Discussion of onsite solid waste and hazardous waste storage methods prior to collection for diversion and/or disposal. Include discussion of types of storage containers to be used, location of storage areas on site plan, and accessibility to storage area for collection vehicles.

III Waste Diversion Analysis

A Discussion of types of solid waste and hazardous waste that will be diverted from disposal by means of recycling and reuse

B Discussion of processes that will be used to reduce the amount of solid waste and hazardous waste that would normally be generated

C Estimation of the annual quantity of solid waste and hazardous waste that will be diverted, per waste type, as identified in A and B above

D Discussion of market availability, per waste type, for diverted materials as identified in A and B above

IV Waste Collection

Discussion of methods that will be used to collect and transport solid waste, hazardous waste, and recyclable materials

V Waste disposal

Discussion of disposal facilities that will be used for disposal of solid waste and hazardous waste generated, including the discussion of impacts to the site lives of the facilities due to the increased waste quantities

Jan 96

From Victor Machado
To Kirk JLIAW
Date 5/9/97 5 16pm
Subject Airport Gateway Center TM3-97

Map is not acceptable

I went out to the site today What does the 82 EIR say about all of the existing Oak trees I see four (4) in the interior of the site and a whole bunch that border the site ?

This map (preliminary or not) should show all existing Oaks especially all existing Heritage Oaks

The Heritage Tree Ordinance requires a permit prior to removal or any type of harm to any Heritage Oak tree (16" dia or greater trunk measured at 2' above grade)

In the future I think any type of improvement plan/ building permit application should address the City Heritage tree ordinance I will E Mail John regarding getting this issue on the DRC agenda

Thanks

CC TGALLAGH



HENRY M. HIRATA
DIRECTOR

SA/FC/30 *Sum*
COUNTY OF SAN JOAQUIN

DEPARTMENT OF PUBLIC WORKS

P. O. BOX 1810 1810 EAST HAZELTON AVENUE
STOCKTON CALIFORNIA 95201 1810
209/468 3000
FAX 209/468 2999

THOMAS R. FLINN

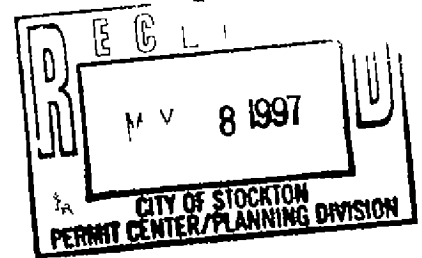
DEPUTY DIRECTOR

MANUEL LOPEZ

DEPUTY DIRECTOR

STEVEN WINKLER

DEPUTY DIRECTOR



May 7, 1997

City of Stockton
Community Development Department
Planning Division City Hall Annex
425 North El Dorado Street
Stockton, California 95202

Attention Ms Jenny Liaw

SUBJECT AIRPORT GATEWAY CENTER (TM 3 97)

Gentlemen

The San Joaquin County Department of Public Works has the following comments regarding the above subject

- 1 The Tentative Map is inconsistent with the draft Arch-Airport/Sperry Road Specific Road Plan
- 2 Dedication for ultimate rights-of way for Sperry Road and Airport Way frontage road, per Specific Road Plan, is needed
- 3 A Traffic Study shall be done to include Interstate 5 and State Highway Route 99
- 4 Stockton Metropolitan Airport shall be informed of the proposed subdivision

Thank you for the opportunity to review this matter. If you have any questions, please contact me at (209) 468-3024.

Very truly yours,

Paterno A. Dichupa
PATERNO A. DICHUPA
Engineering Assistant II

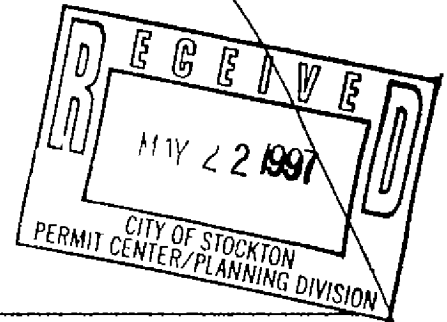
PAD:ti
PS-7E064 II

- c Dave Edrosolan, Associate Civil Engineer
- Tom Iwamiya, Senior Civil Engineer

SIMPSON MOORE, INC

Fax Transmittal

TO DAVID STAGNARO
FROM MICK KLASSON
PAGES 3, INCLUDING COVER
RE DUCK CREEK REZONING/GPA
DATE 2/11/97



HARD COPY WILL OR WILL NOT FOLLOW BY MAIL

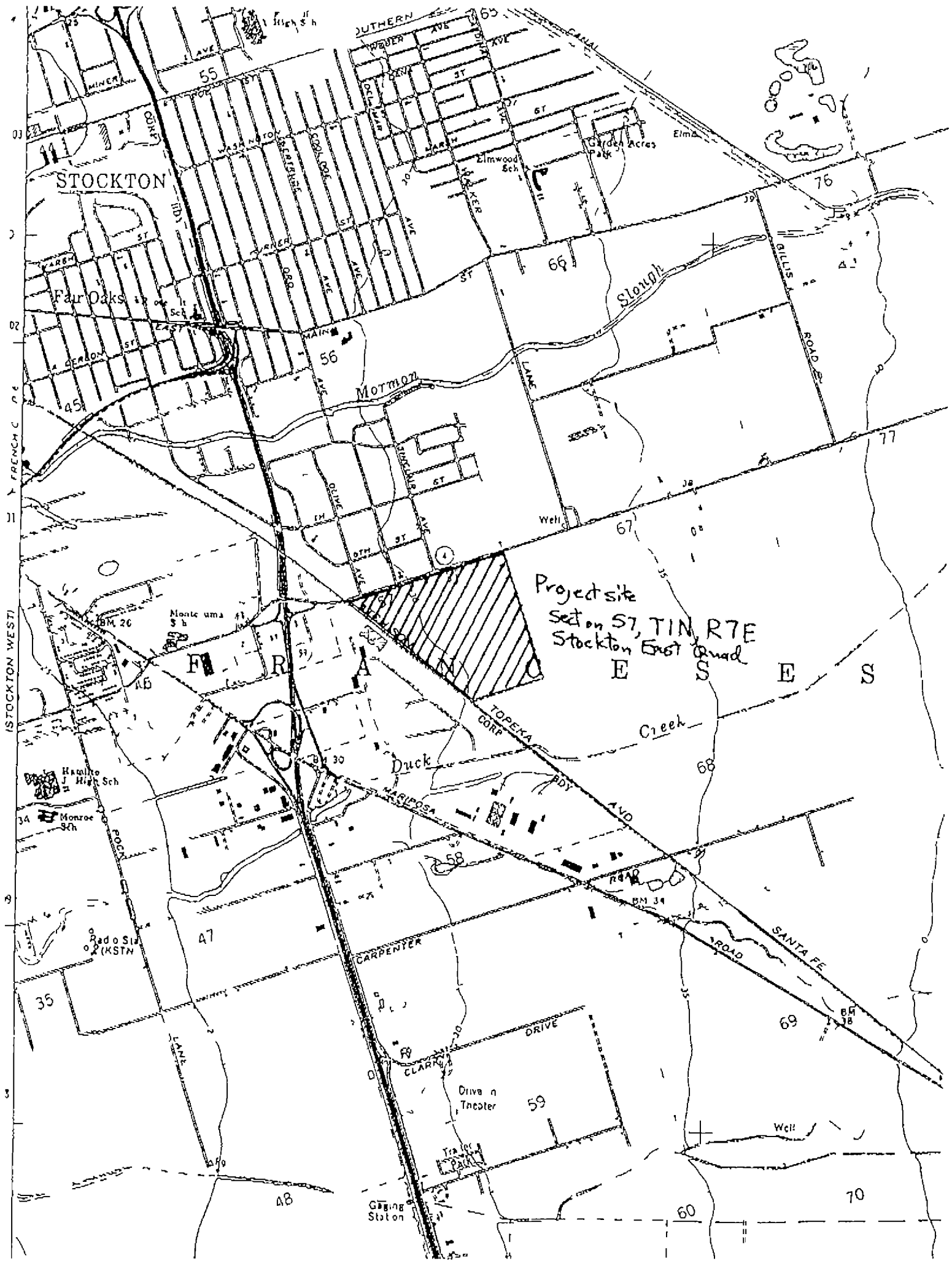
As a follow up to our phone conversations on May 13 and 19 I've enclosed a map showing the Duck Creek industrial project site. As we discussed I'd like to ask Vince Huey to prepare three graphics of this site: all portrait-style 8.5 X 11' maps: a project location map at about 1" = 2,000' and an existing zoning map and existing General Plan map at a closer scale (say 1" = 600' or thereabouts).

We will add labels and figure titles in our office.

I have also enclosed a site map for the Airport Gateway project which is on a similar time line and we would like to get a 1" = 2,000' project location map for.

I'd still like to chat with you or Mike regarding whether access restrictions on Farmington Road will be sought for the Duck Creek project and if a masonry wall is a desirable mitigation measure for industrial noise, visual impacts or land use compatibility.

Thanks for looking into these items. If you have any questions or cannot schedule the graphics at this time, please let me know.



Please Sign In

4/9/97

AIRPORT Gateway - TM + EIR

REED HOGAN	COS PW	937-8868
Sharon Parker	Econ Dev	937-8862
Gregg Meissner	Public Works	937-8270
Mike Niblock	COS Planning Div	937-8266
Bob Sivell	City Mgrs office	937-8530
Guy Petzold	City Attorney's Office	937-8333
Charlie Simpson	Simpson Moore	477-2833
Mick Klasson	Simpson Moore	477-2833
GREGORY O'LEARY	CB Commercial	476-2908
MICHAEL THAKKEM	APPLICANT council	4742800
RICHARD LAROUY	COS PLANNING DIVISION	937-8627
Let Corrales	Siegfried Engr.	943-2021
Stan Oshita	Siegfried Engr	943-2021
Jenny (ian)	planning	937-8266
KEN ANDERSON	KD ANDERSON T&S	916 786-5529
JULIE TOWNSEND	KD ANDERSON TRANSP ENGS	(916) 786-5529
KATHY TOMURA	COS Public Works	(209) 937-8690

BRIEF SUMMARY OF
ANTICIPATED MITIGATION MEASURES
Airport Gateway Project

Geology and Soils

- Building Code Seismic Requirements
- SWPPP (construction runoff)

Water Resources

- Storm Drainage Detention
- Detention Pond Discharge (to Slough) Permit
- Industrial Use NPDES General Permit including spill prevention and control

Biology

- Swanson s Hawk Foraging Fee
- Site-specific Impact Mitigation if any

Air Resources

- Construction Dust Controls (APCD Regulation 8)
- On-site Emissions, if any Authority to Construct
- Regional Impacts accounted for in General Plan Air Quality Fee
- CO hotspots accounted for in traffic mitigation not expected

Cultural Resources

- Construction Encounter Survey

Land Use

- Dentention Pond Siting
- Airport Compatibility Issues
- Land Use Conflicts

Traffic

-
-

Utilities

- Sewer trunk lines, hookup fees
- Water trunk lines, hookup fees surface water fees cumulative GW overdraft
- Storm drain trunks, detention facilities and terminal discharge location clean water fees

Services

- No Substantial impacts

Fiscal Impacts

- Beneficial Impacts

Aesthetic Impacts

- Industrial Screening and Outdoor Storage Requirements

Noise

- Noise Study for On-site Noise Generators
- Off-site Traffic Noise accounted for in General Plan

Safety and Hazmat

- Hazmat and Waste Permitting
- NPDES

NOTES.

THIS SUBDIVISION IS A RECONSTRUCTION OF BLOCK AND PARCELS OF BLOCKS AND OF BERRY WAY INC. THIS SUBDIVISION CONTAINS 37 ACRES. EASEMENTS ARE REFERRED TO BY DATE IN APPROVALS. THIS IS IN ACCORDANCE WITH THE CITY OF STOCKTON ORDINANCES.

DATE: 1987 JAN 10 AM AND 10:00 AM PM ME OF THE CITY OF STOCKTON. THE CITY OF STOCKTON IS THE OWNER OF THE AIRPORT GATEWAY CENTER. THE CITY OF STOCKTON IS THE OWNER OF THE AIRPORT GATEWAY CENTER. THE CITY OF STOCKTON IS THE OWNER OF THE AIRPORT GATEWAY CENTER.

THE SUBDIVISION WILL BE OPEN TO THE PUBLIC. THE NAME WILL BE CHANGED TO AIRPORT GATEWAY CENTER. THE CITY OF STOCKTON IS THE OWNER OF THE AIRPORT GATEWAY CENTER. THE CITY OF STOCKTON IS THE OWNER OF THE AIRPORT GATEWAY CENTER.

LEGEND

--- CHURCH
 --- EXISTING BOUNDARY LINE
 --- EXISTING EASEMENT
 --- EXISTING SANITARY SEWER
 --- EXISTING WATER MAIN
 --- EXISTING GAS MAIN
 --- EXISTING SANITARY SEWER
 --- PROPOSED CHURCH
 --- PROPOSED SANITARY SEWER
 --- PROPOSED WATER MAIN

OWNER

POSITION: CITY ENGINEER
 3630 AIR DIXON BLVD SUITE 30
 STOCKTON, CA 95204
 PHONE: (209) 938-4444
 ATTN: THE ENGINEER

SUBDIVIDER

NAME: CIVIL ENGINEER
 3630 AIR DIXON BLVD SUITE 30
 STOCKTON, CA 95204
 PHONE: (209) 938-4444
 ATTN: CIVIL ENGINEER

THIS MAP THIS _____ DAY OF _____ 1997

CITY ENGINEER

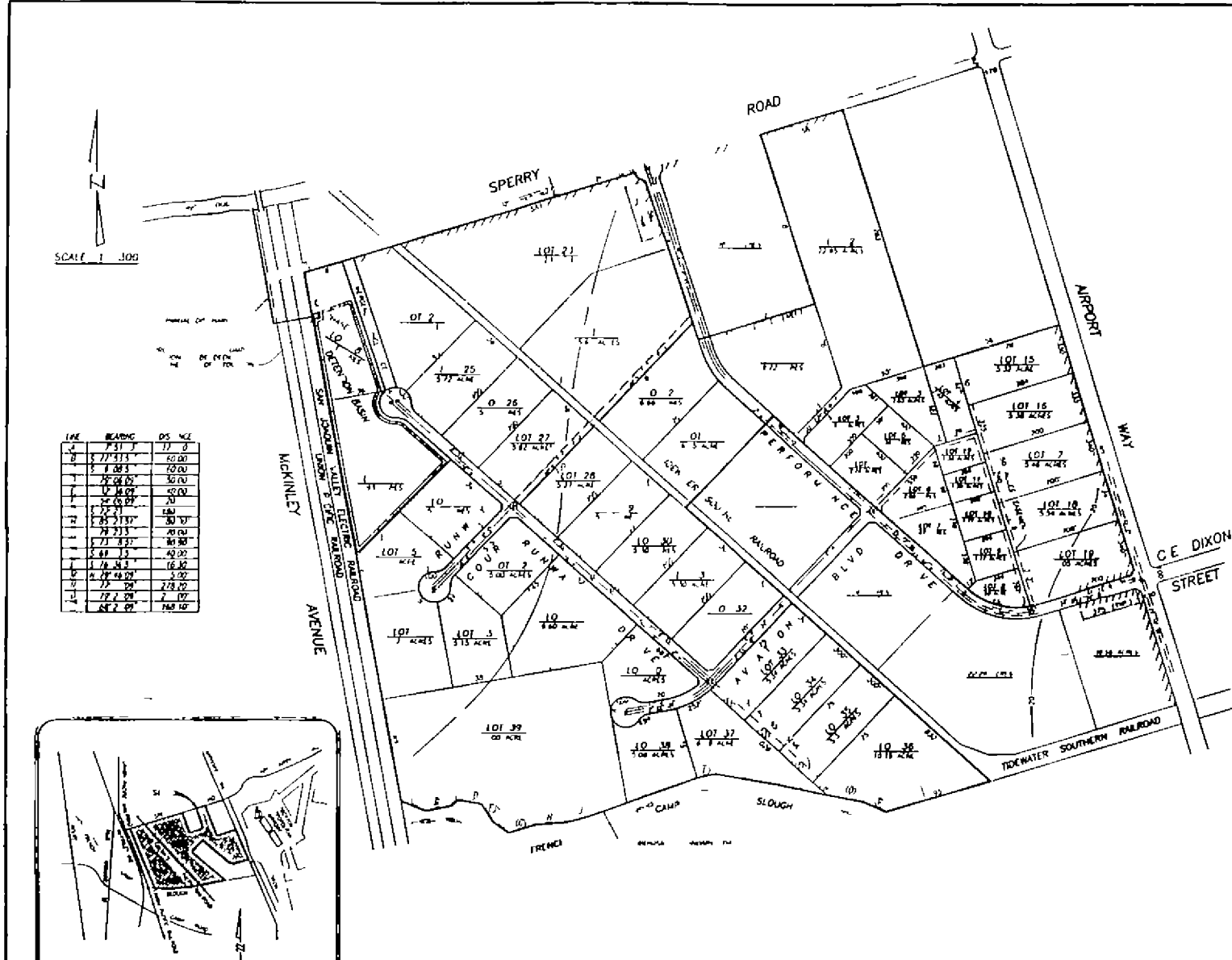
APPROVED BY THE CITY OF STOCKTON PLANNING COMMISSION THIS _____ DAY OF _____ 1997

CHURCH

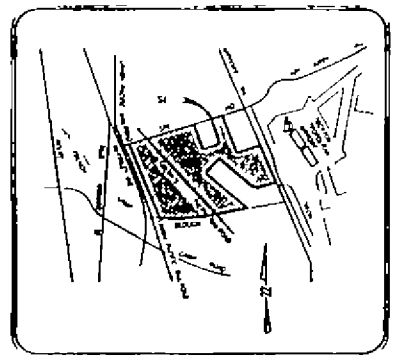
APPROVED BY THE CITY ENGINEER OF THE CITY OF STOCKTON THIS _____ DAY OF _____ 1997

CITY ENGINEER

TENTATIVE MAP
 TRACT NO. 8778
 SUBDIVISIONS OF SAN JOAQUIN COUNTY
 AIRPORT GATEWAY CENTER
 BEING A PORTION OF SECTION 75, C. WEBER GRANT
 CITY OF STOCKTON, SAN JOAQUIN COUNTY, CALIFORNIA
 JANUARY 1997



LINE	READING	DIS.	ACE.
1	77.113	77.0	
2	4.003	4.000	
3	75.043	75.000	
4	10.140	10.000	
5	22.000	22.000	
6	25.313	25.000	
7	29.213	29.000	
8	33.113	33.000	
9	37.013	37.000	
10	40.913	41.000	
11	44.813	45.000	
12	48.713	49.000	
13	52.613	53.000	
14	56.513	57.000	
15	60.413	61.000	
16	64.313	65.000	
17	68.213	69.000	
18	72.113	73.000	
19	76.013	77.000	
20	79.913	81.000	
21	83.813	85.000	
22	87.713	89.000	
23	91.613	93.000	
24	95.513	97.000	
25	99.413	101.000	
26	103.313	105.000	
27	107.213	109.000	
28	111.113	113.000	
29	115.013	117.000	
30	118.913	121.000	
31	122.813	125.000	
32	126.713	129.000	
33	130.613	133.000	
34	134.513	137.000	
35	138.413	141.000	
36	142.313	145.000	
37	146.213	149.000	
38	150.113	153.000	
39	154.013	157.000	



VICINITY MAP
 NOT TO SCALE
 (20 D SHEET NO 35)

SIEGFRIED ENGINEERING, Inc

Civil Engineering and Surveying Structural Engineering PE

4444 Broadway Ave. Stockton, CA 95204-1001 Phone (209) 938-4444

SE logo

TENTATIVE MAP

AIRPORT GATEWAY CENTER
 STOCKTON CALIFORNIA

1

**PROJECTED PROCESSING SCHEDULE FOR ENVIRONMENTAL IMPACT REPORT (EIR)
REQUIRING PLANNING COMMISSION AND/OR CITY COUNCIL CERTIFICATION**

Project Title _____

	PROCEDURAL STEPS	TYPICAL PROCESSING TIME	OPTIMAL SCHEDULE (DATES) ²	ACTUAL COMPLETION DATE
1	Staff/Applicants/Consultants Hold Scoping Meeting	1		
2	Consultants Prepare/Submit Adm Draft Notice of Preparation (NOP)/Initial Study (IS) (15 copies); Applications Submitted	14-21		
3	Staff Review Adm Draft NOP/IS and Applications	10-14		
4	Consultants Prepare/Submit Revised Adm Draft NOP/IS (?) Copies	4-7		
5	Complete Staff Review	4-7		
6	Consultants Revise/Submit (?) Copies of NOP/IS, CDD Staff Deem Applications Complete	3-7		
7	Staff Prepare Transmittal Forms/Distribute NOP/IS, Formal 30-Day Comment Period (includes transmittal time)	35		
8	Consultants Prepare/Submit Adm Draft EIR (preparation initiated with NOP) (15 Copies)	14-28		
9	Complete Staff Review	21-28		
10	Consultants Prepare/Submit Revised Adm Draft EIR (?) Copies	14-21		
11	Complete Staff Review	14-21		
12	Consultants Revise/Submit (?) Copies of Draft EIR	4-7		
13	Staff Prepare/Distribute Transmittal Forms, Notice of Completion (NOC)/Notice of Availability (NOA), Draft EIR, Formal 45-Day Comment Period or 30-Day Shortened Review	35-50		
14	Consultants Complete/Submit Adm Final EIR and Findings/Monitoring Program ¹ (15 Copies)	14-21		
15	Complete Staff Review	14-21		
16	Consultants Prepare/Submit Revised Adm Final EIR & Findings/Monitoring Program (15 Copies)	7-10		
17	Complete Staff Review	7-10		
18	Consultants Submit Revised Final EIR & Findings/Monitoring Program (?) Copies	4-7		
19	CDD Staff Schedules Planning Commission (PC) Meeting, Transmit Draft and Final EIR/Findings/Monitoring Program to PC	1-4		
20	CDD Staff Provide Notice of Public Hearing, Prepare/Transmit Staff Report(s) to PC, Transmit Final EIR/Hearing Notice to Agencies Submitting Formal Comments on Draft EIR	14-18		
21	PC Conduct Hearing/Certify and/or Recommend EIR Certification and Adoption of Findings/Monitoring Program, PC Approves/Recommends Approval of Applications	1		
22	CDD Staff File Notice of Determination (NOD) and Dept of Fish and Game (DFG) Fees with County Clerk/OPR for any Final PC Actions	1		

23	CDD Staff Prepare/Transmit PC Recommendations to City Council (CC) via City Manager (CM) Agenda Report, Concurrent 10-Day Appeal Period on PC Actions	11		
24	CM Requests CC to Set Hearing Date; CC Schedules Hearing, City Clerk Provides Notice of CC Hearing	20-27		
25	CC Considers and Certifies Final EIR/Adopts Findings-Monitoring Program, Conducts Hearing/Approves Applications	1-8		
26	CDD Staff File NOD/DFG Fees with County Clerk/OPR for Final CC Actions	1		

TOTAL ESTIMATED PROCESSING TIME = 221-318 Days for PC Approvals Only
(Excluding Steps 1 and 2) 254-365 Days for PC and CC Approvals

- Notes
- 1 The Adm FEIR and Adm Findings/Monitoring Program may be submitted and reviewed concurrently but must be separate documents
 - 2 This represents the projected scheduling for an EIR, but is by no means a commitment by the City or consultants to meet this schedule There are numerous factors which may make this schedule infeasible

4/9/97

P1

~~Bridgeport~~ ~~Tran~~

Airport gateway project

EIR (2-83) → supplement to EIR.

Traffic study - infrastructure, Arch-Sperry extensions

Greg → Sperry will be considered

start traffic analysis, external

(cal) ~~freeway~~ interchange Arch-Sperry to

connect French Camp

↓ project study, no internal analysis

no acre - site is large reserve for

a single street

Temporary emergency access ~~at~~ at

Sperry Road

~~PUC leasing crossing~~
~~PUC at-grade~~

PUC leasing crossing
at-grade

easier way, trade crossing from the city

No crossing → ask new crossing (time,

administrative hearing), no development

at the west side unless obtaining railroad

crossing.

No access along Sperry road

* preliminary review to other agencies

* preliminary DRC meeting

2. utility - MUD three lot dim adjustments, (first second third)
 terminal discharge for the third site

master plan for the entire subdivision site.

① maintain CEQA compliance with city standard.

② Redevelopment agreement.

supplement to EIR (6 months)

NO control for Railroad.

accepted master ^{infrastructure} storm drainage plan prior to issuance of Building permit.

utility master plan to PW.

Approval of master infrastructure plan by the city.

master storm drainage plan will be similar to EIR 2-83

3 Biological survey - Airport ~~Area~~ quality.
 to water related issues infrastructure for storm drainage project description

~~AT&T~~ master plan for storm drainage

Scope

P 3

- ① swam lands biological No impact --
- ② real geographic same
- ③ water resource
- ④ ^{storm} discharge, to fresh camp along
- ⑤ nesting habitats
- ⑥ air resources to run new model
- ⑦ traffic study.
- ⑧ cultural resource (No)
- ⑨ airport
- ⑩ noise (on-site)
noise (traffic)
- ⑪ storm damage.
- ⑫ Amount of industrial land (acres)
- ⑬ background for project description.

**CITY OF STOCKTON****OFFICE OF THE CITY MANAGER**

CITY HALL
425 N. EL DORADO STREET
STOCKTON, CA 95202-1887
(209) 944-8212
FAX (209) 463-1550

March 26, 1993

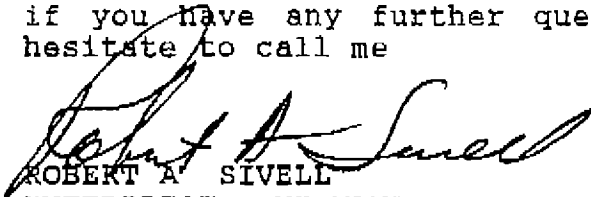
Mr. Tim Gagnier
Panattoni-Catlin Venture
1851 Heritage Lane, Ste. 260
Sacramento, CA 95815

AIRPORT GATEWAY INDUSTRIAL PARK

Pursuant to your request this morning, I have reviewed the Smith's preliminary layout for their configuration at the Airport Gateway Industrial Park with the City's Deputy Director of Planning and with staff of the Public Works Department.

As we have previously indicated, a lot line merger and lot line adjustments to accommodate this conceptual layout does not appear to present any problems.

We look forward to working with you on this project. Accordingly, if you have any further questions or concerns please do not hesitate to call me.


ROBERT A. SIVELL
ENTERPRISE ZONE MANAGER

RAS tf

cc Lyn Krieger, Deputy City Manager
Community Development Department/Planning
Public Works Department

**CITY OF STOCKTON****OFFICE OF THE CITY MANAGER**

CITY HALL
425 N EL DORADO STREET
STOCKTON, CA 95202-1887
(209)944-8212
FAX (209) 463-1850

September 10, 1992

Mr. Robert N Brost
Equimark Realty Corp.
c/o Mr Jim Stanley
CB Commercial Group, Inc.
555 Capitol Mall, Suite 100
Sacramento, CA 95814

CITY OF STOCKTON FEE INFORMATION AND A 20-YEAR PROPERTY TAX
PROJECTION FOR THE PROPOSED WAREHOUSE/DISTRIBUTION CENTER

We appreciated the time you spent with us last week discussing the development of your project in the City of Stockton. Attached is the follow-up information we promised to provide you. The fee calculations provided are only estimates; actual fees will be dependant on project specifics.

To date we have made the following agreements

- 1 The City will recognize the existing lot lines and use lot mergers to create the lot size you need
- 2 No additional environmental impact report will be required.
- 3 The City agrees to the fire exiting distance which you requested (400 feet) with mitigating measures (please see attachment).
4. Your project will be considered as a fast-track permit.

We look forward to working with you and believe, given our experience, that we can effectively deal with your project needs

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If you have any questions, please do not hesitate to call Sharon Parker or me at (209) 944-8862.

LYN KRIEGER
DEPUTY CITY MANAGER
ECONOMIC DEVELOPMENT/CENTRAL STOCKTON

LK:tf

attachments

cc: Dwane Milnes, City Manager
Sharon Parker, Economic Development Analyst
CB Commercial
Economic Review Committee

**CITY OF STOCKTON**CITY HALL
STOCKTON, CA 95202COMMUNITY DEVELOPMENT DEPARTMENT
(209) 944-8266

March 20, 1984

Stockton Airport Industrial, Ltd
c/o TMI
P O Box 2500
Newport Beach, CA 92660APPROVED TENTATIVE MAP - STOCKTON AIRPORT INDUSTRIAL, LTD
(AIRPORT GATEWAY CENTER) (AMENDED TM29-83)

At its regular meeting of March 8, 1984, the City Planning Commission approved your request to subdivide 447 acres at the southwest corner of Sperry Road and Airport Way, across from the Stockton Metropolitan Airport (Airport Gateway Center) (Amended TM29-83)

This approval is subject to the following conditions

- 1 Comply with applicable State, County and City codes and regulations and adopted standards
- 2 Soils reports shall be required prior to any submission of plans for construction
- 3 Street names shall be approved by the Community Development Director
- 4 A complete master storm water drainage system, including a detention basin, shall be provided by the owner/developer as described in the engineering report. On-site storm water drains and the detention basin shall be sized to provide adequate capacity for the tributary area. Provisions to serve off-site parcels, such as enlargement of the detention basin, shall be at the expense of the owners/developers of the off-site parcels

Stockton Someplace Special!

STOCKTON AIRPORT INDUSTRIAL, L.L.C.

March 20, 1984

Page 2

The owners shall be responsible for maintenance of the detention basin. When further development of the off-site properties within the tributary area occurs, the City shall require the owners/developers of these parcels to pay an equitable share of the cost of maintaining the drainage system, including the detention basin. The boundaries of the tributary area have been refined, based on data supplied by the applicant, and have been field reviewed by the Department of Public Works staff and are shown in the addendum to EIR 3-82.

The owner/developer shall provide and have approved any interim storm drain plan for phased development. Said approval shall precede approval of the final map.

- 5 If any industries store, transport or use toxic substances on the subdivision site, sufficient design measures shall be incorporated to keep spills of such materials from entering French Camp Slough or the subdivision drainage system. Such design measures shall be approved by the City during the design review process and by the Water Quality Control Board before a Waste Discharge Permit is granted.
- 6 The owner/developer shall obtain permits and shall comply with all requirements of all government agencies with jurisdiction over French Camp Slough including, but not limited to, dedication of right-of-way, access easements, fencing, outfall structure and necessary improvement of French Camp Slough.
- 7 All building pads shall be raised to conform to flood insurance requirements and City Flood Plain Management Ordinance.
- 8 The owner/developer shall participate in an Area of Benefit for South Industrial Sanitary Sewer Trunk Line.
- 9 The available capacity for this development in the 27 inch Southern Industrial Sanitary Sewer Trunk Line is very limited. A new sanitary sewer line may be necessary in the future when this area is fully developed. Any sanitary sewer connections to this line must be approved by the Municipal Utilities Department.
- 10 The area of the subject map shall be annexed to the City of Stockton prior to approval of the final map.

Stockton Airpo, Industrial, ~~Industrial~~
 March 20, 1984
 Page 3

11. Traffic signals shall be required at the following locations and owner/developer participation shall be as indicated:

- a Airport Way/Arch Sperry Road 25%
- b Airport Way/C E Dixon-Aviation Way 50%
- c Sperry Road/Hughes Street (required only when Sperry Road is extended westerly of site) 100%
- d Sperry Road/Aviation Way 75%
- e Sperry Road/Belding Street 75%
- f Aviation Way/Lindberg Street 100%
- g Aviation Way/Belding 100%

These traffic signals shall be installed when warranted or at the discretion of the Director of Public Works

12 The owner/developer shall be responsible for all necessary on-site improvements, with the exception of those improvements to be funded through assessment districts or Area of Benefit, which will spread the cost equitably to benefiting owners. In addition, the owner/developer shall contribute to the off-site improvements specified below

- a The owner/developer shall provide three southbound lanes on Airport Way along the tract frontage. A minimum of a 62 foot half section along the tract frontage shall be dedicated to the City of Stockton. Flaring of intersections will be required to accommodate projected traffic volumes, an additional 7 foot dedication will be required for intersection flares. Flaring of the north leg of the Airport Way/Sperry Road intersection will also be necessary and shall be provided by the owner/developer, the City shall attempt to recover a proportionate share of the costs from future developments
- b The owner/developer shall construct full improvements on Sperry Road including four lanes, a raised median with turning lanes (dual left-turn lanes at Airport Way), and intersection flares as required. A 67 foot half section and 134 foot full cross section along the proposed alignment of Sperry Road shall be dedicated to the City of Stockton. Additional dedication will be required for intersection flares. Construction of additional lanes shall be funded through an assessment district or Area of Benefit

Stockton Airport Industrial, ~~1984~~

March 20, 1984

Page 4

If the owner/developer elects to finance construction of the grade separation crossing of the Tidewater Southern Railroad prior to the formation of an assessment district, all costs above the landowner's/developer's assessment shall be credited fully toward equitable participation in any assessment district or Area of Benefit formed to finance improvements to the Sperry Road/Arch Airport Road corridor

The proposed grade separation structure over the Tidewater Southern Railroad shall have a 100 foot full cross section and will contain six lanes, an 8 foot median and sidewalks

- c The owner/subdivider shall participate equitably in any assessment district or Area of Benefit formed to construct Sperry Road or Industrial Drive westerly to Interstate 5/French Camp Turnpike Road including railroad overcrossings, waterway bridges, and Interstate 5 interchange improvements
 - d The owner/subdivider shall participate equitably in any assessment district or Area of Benefit formed to realign and/or widen Arch-Airport Road easterly of Airport Way to provide a 6 lane cross section
 - e The owner/subdivider shall participate equitably in any assessment district or Area of Benefit formed to widen or reconstruct the Arch-Airport Road overcrossing at the interchange with State Route 99
- 13 The owner/developer shall include and adopt a statement in the Conditions, Covenants and Restrictions (CC&Rs) stating that all owners/developers of the subdivision lots must participate in the implementation of a Transportation Systems Management (TSM) Program when requested to do so by the City of Stockton Possible TSM measures include appointing a transportation coordinator to organize car and van pools, implementing flex-time or staggered work shifts, providing preferential parking to car-pooling vehicles, and other similar measures
- 14 The owner/developer shall dedicate access rights to the City of Stockton at the following locations along the west side of Airport Way, except at the Lindberg
- 108

Stockton Airport Industrial, Ltd.
March 20, 1984
Page 6

2 yrs after the Annexation is effective - AUG 31, 1984

Enclosed is a copy of the approved tentative map which will expire on March 8, 1986. Any request for an extension shall be made in writing, filed with this office and accompanied by the appropriate fee at least thirty (30) days prior to expiration of the tentative map and clearly stating the reasons for requesting the extension. In granting an extension, new conditions may be imposed and existing conditions may be revised.

If you have any questions, please contact Mr Sam Mah of the Community Development Department, Planning Division, at (209) 944-8266

John Carlson
JOHN CARLSON, SECRETARY
CITY PLANNING COMMISSION

JC kmg

Enclosure

cc Randall O'Dell w/enclosure
1581 Cummins Drive, Suite 123
Modesto, CA 95351

Luis Manzano and Associates w/enclosure
31320 Via Colinas, Suite 104
West Lake Village, CA 91352

Steve Herum w/enclosure
P O Drawer 20
Stockton, CA 95201

Robert P Flick w/enclosure
2049 Century Park East
Los Angeles, CA 90067

John Erickson w/enclosure
4560 Enchanted Way
Redding, CA 96001

Planning Associates w/enclosure
P O Box 1527
Redding, CA 96009

Public Works w/sepia

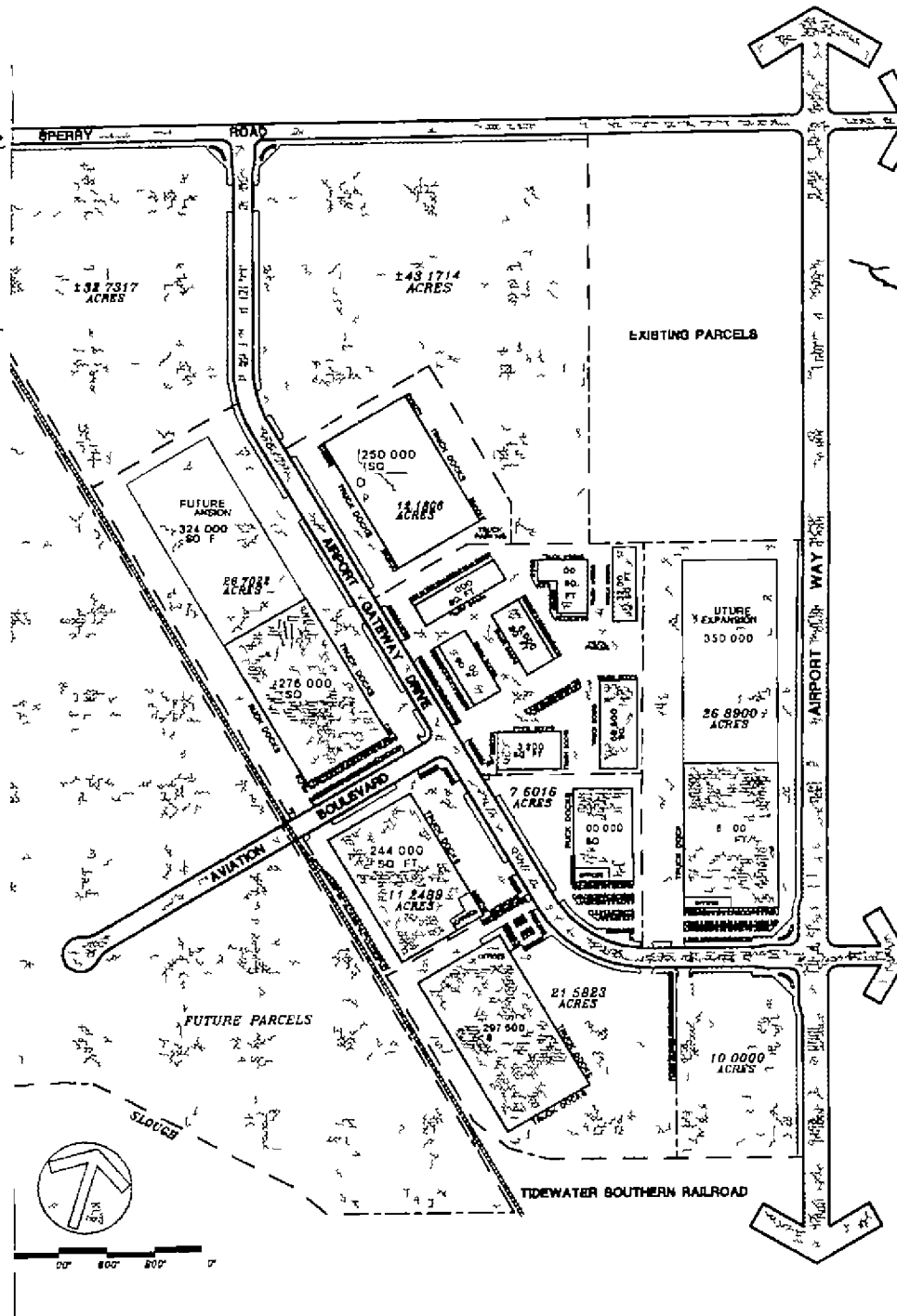
104

Notes from 10/1/96 ERC Meeting

Panattoni wanted to create an additional parcel via lot line adjustment to allow a second industrial use (yellow on map). They were reminded that the deal was for one user only on NW corner of Airport Gateway Drive and Aviation Blvd until a TM and EIR are completed for the entire site.

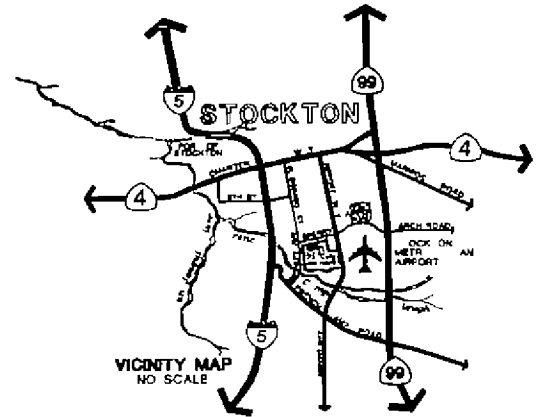
Paul wants Master Utility Plans and a permanent storm drain basin. I pointed out that no commitment should be made on "permanent master plans for improvements/utilities until a TM and EIR are completed. Facilities should be "interim" for the first development. I noted that the location and final design of a basin and other infrastructure will require a TM and public disclosure via a EIR in order to consider input from Airport and other interested parties -- not to mention other environmental issues.

Panattoni/Seasbaugh to submit new building/site improvement plans for different user at NW corner of Airport Gateway/Aviation Blvd - so existing plans are null and void. Sivell/Seasbaugh committed to "fast tracking" plans to be submitted in one or two weeks.



A Joint Project By

Panattoni-Catlin Venture



AIRPORT GATEWAY CENTER

FOR FURTHER INFORMATION CONTACT

GREGORY O'LEARY



CB COMMERCIAL
REAL ESTATE GROUP INC.

BROKERAGE AND MANAGEMENT
BROKERAGE SERVICES

1776 MARCH LANE
SUITE 170
STOCKTON CA 95207

TEL (209) 476-2908
FAX (209) 476-2960

LOCAL KNOWLEDGE WORLDWIDE

STOCKTON AIRPORT INDUSTRIAL, LTD AND CITY PLANNING
COMMISSION INITIATED PREZONING REQUESTS (Z-12-83)

On Monday evening, a public hearing will be held to consider the report of the Planning Commission regarding the Stockton Airport Industrial, Ltd , and Planning Commission initiated requests for prezoning to E-P, Enterprise Performance District, A-1, Agricultural District, and M-2, Heavy Industrial District, of approximately 516 acres at the southwest corner of Sperry Road and Airport Way, across from the Stockton Metropolitan Airport

RECOMMENDATION

The Planning Commission recommends that the City Council approve the prezoning requests for the reasons stated in the Commission's report (Exhibit I) to the Council dated March 28, 1984

SUMMARY SHEET

Case No Z-12-83

Permit Prezoning

Applicant Stockton Airport Industrial, Ltd and City Planning
Commission Initiated

Location

Request Approximately 516 acres at the southwest corner of Sperry
Road and Airport Way

Environmental Status The Planning Commission is recommending
certification of EIR3-82 for this project

RECOMMENDATION

Based on the following findings, the City Planning Commission voted unanimously to recommend approval of the request of Stockton Airport Industrial, Ltd for prezoning of 401 84 acres to E-P north of French Camp Slough and 30 37 acres to A-1 south of the slough, and approval of the Planning Commission initiated prezoning of 37 acres to M-2 at the southwest corner of Sperry Road and Airport Way and the 4 3 acre abandoned sewage treatment plant site to E-P north of the slough and west of Airport Way

- 1 The proposal conforms to the City of Stockton General Plan designation and policies for the subject site
- 2 With respect to the proposed prezoning north of French Camp Slough and excluding the 37 acres at the southwest corner of Airport Way and Sperry Road, the E-P, Enterprise Performance District, would be a more appropriate zone than M-2 zoning for the subject site as it provides setback, aesthetic and sign controls, and other performance criteria regulating the future development of the site and its compatibility with the Stockton Metropolitan Airport and the nearby existing and proposed industrial parks
- 3 With the exception of the conversion of prime agricultural land, all potentially adverse environmental effects of subsequent development of the area will be mitigated to an acceptable level through project design and/or by enforceable conditions of approval on the associated tentative subdivision map and on any subsequent Use Permits and construction permits for specific projects within the project area
- 4 The benefits of the proposed project outweigh the unavoidable conversion of prime agricultural land to urban use as supported by EIR3-82 and by the following overriding considerations
 - a The proposed E-P zoning combined with the M-2 will benefit the immediate area
 - b It will have a positive economic impact on the entire area

- c The proposed E-P zone is consistent with the Airport Land Use Plan and the Stockton Metropolitan Airport Overlay Zones
- d If the entire property were to be developed in the County, it could have a negative effect if developed with problemated uses generally associated with the M-2 zones
- e The project could be developed in the County since it is presently in conformance with their General Plan designation for the site. This would result in overlapping and duplicative jurisdictional situations. Since the development is in need of urban services and municipal facilities, it is logical that the area be developed in the City and served by municipal government and services.
- f All urban services and facilities are available and can readily be extended to the project site



CITY OF STOCKTON

CITY HALL
STOCKTON CA 95202
(209) 944-8266
COMMUNITY DEVELOPMENT DEPARTMENT

March 28, 1984

Honorable City Council
City of Stockton, California

STOCKTON AIRPORT INDUSTRIAL, LTD , AND CITY PLANNING COMMISSION
INITIATED PREZONING REQUESTS (Z-12-83)

At its regular meeting of March 8, 1984, the City Planning Commission considered the Stockton Airport Industrial, Ltd , and Planning Commission initiated requests for rezoning to E-P, Enterprise Performance District, A-1, Agricultural District, and M-2, Heavy Industrial District, of approximately 516 acres at the southwest corner of Sperry Road and Airport Way, across from the Stockton Metropolitan Airport

Stockton Airport Industrial, Ltd is requesting rezoning of 401.84 acres to E-P on the west side of Airport Way north of French Camp Slough. An additional 30.37 acres east of the Western Pacific Railroad line, south of the Slough, is proposed to be rezoned to A-1. Both of these sites are currently zoned GA-20, General Agricultural District, in the county.

At its regular meeting of June 9, 1983, the City Planning Commission initiated rezoning of the remaining parcels within a proposed 516 acre annexation area at the southwest corner of Airport Way and Sperry Road. Specifically, the Commission initiated rezoning to M-2, Heavy Industrial District, for a 37 acre area currently zoned M-2 in the county, and to E-P zoning for a 4.3 acre abandoned sewage treatment plant site, currently zoned GA-20 and I-PA, Interim Protected Agriculture, in the county, west of Airport Way and northeast of French Camp Slough.

The General Plan designates the project area for Industrial uses north of French Camp Slough and for Open Space/Agriculture south of the slough. The County's General Plan also designates the site as Industrial and Open Space/Agriculture.

An environmental impact report (EIR3-82) has been prepared for the project. The Planning Commission certified that the Final EIR is adequate and that they had reviewed and considered it prior to

EXHIBIT I

'Stockton Someplace Special!'

approval of the related tentative map (amended TM29-83) for this project and recommended that the City Council certify said Final EIR prior to taking action on the subject rezoning request and prior to any related discretionary actions. Also, the City Council must adopt a statement of overriding considerations with findings supporting conversion of the site to urban use if the proposed rezoning is to be approved (see finding No 4 below)

The project involves rezoning and annexation of approximately 516 acres. Of this area, 447± acres will be subdivided to form the 84-lot Airport Gateway Center. The subdivision will include 41 lots totaling 166± acres to be used for professional and corporate offices, 40 lots totaling 176± acres to be designated for airport-dependent high technology industrial uses such as assembly of electronic components, two lots totaling 10± acres for a retention pond, and one 30± acre lot south of French Camp Slough which will remain in agricultural use. Total buildout of the subdivision is expected in ten years, phasing is not presently being contemplated. Before development can occur, a California Land Conservation (Williamson Act) contract must be terminated for the 401 acres proposed for development.

As background information, the City Council has authorized the filing with the Local Agency Formation Commission (LAFCO) of the annexation request by Stockton Airport Industrial, Ltd for the approximately 516 acres at the southwest corner of Airport Way and Sperry Road. At its meeting on January 20, 1984, LAFCO considered the application and the City's Resolution No 30313 (January 3, 1984) requesting that processing of the application proceed. LAFCO determined that rezoning would resolve any questions concerning future zoning, as discussed in the EIR, and would provide a procedure in which the City Council would certify the Final EIR. Therefore, LAFCO continued the hearing pending City rezoning of the proposed annexation area.

A representative for Stockton Airport Industrial, Ltd appeared and described the proposed Airport Gateway Center project. It was emphasized that all of the potentially adverse environmental effects of the project will be mitigated to an acceptable level with the exception of the conversion of prime agricultural land. However, while he had no quarrel with the technical designation of the site as being prime agricultural land, he contended that the site cannot be economically farmed as a practical matter. He noted that the project would be consistent with the goals and policies of the City's General Plan and would provide a logical and attractive environment for the placement of high tech industries across from the Stockton Metropolitan Airport. Although it would be possible for the project to develop in the County, it was felt that it would be mutually beneficial to the applicant and the City--particularly since the City would be obligated to extend municipal services to the site irrespective of

whether it develops in the city or the county. In closing, he reiterated the project's numerous beneficial impacts including the attraction of high tech industries and creation of many jobs, and recommended that the EIR be certified and that the rezoning and related discretionary approvals be granted subject to the noted overriding considerations offered in the EIR Summary.

The president of TMI, the general partner of Stockton Airport Industrial, Ltd, appeared and described various similar projects which TMI has initiated in other parts of California. It was noted that the large size, shape, and the proximity to the airport and freeways make the subject site more suitable and desirable than other sites in the Stockton area. It was further noted that the E-P zoning will provide inherent mitigation measures due to the required setback, aesthetic and sign controls, and other performance criteria regulating the future development of the site and its compatibility with the airport and other surrounding land uses.

Various other project proponents reiterated the potential positive economic effects which the project will have on the Stockton area and the suitability of the site for high tech industrial development. No one appeared in opposition to the proposed rezoning requests, however, two of the property owners at the southwest corner of Airport Way and Sperry Road appeared to express their desire to be rezoned M-2 rather than E-P in accordance with their present County zoning designation.

In their deliberation, some Commissioners expressed concern that the proposal did not include a specific project for consideration and over the speculative nature of the request. Nonetheless, the Commission noted that the proposed rezoning is consistent with the City General Plan and that the proposed land uses will be compatible with surrounding zoning and land uses. The Commission felt that the E-P and M-2 zoning north of French Camp Slough and the subsequent land uses which they will attract will have a positive economic effect on the entire area and will represent a logical and timely extension of City zoning and municipal services adjacent to the airport and other existing and approved industrial sites.

After deliberation and certification of the related Final EIR (EIR3-82) for the project, the Commission voted unanimously to recommend approval of the request of Stockton Airport Industrial, Ltd for rezoning of 401.84 acres to E-P north of French Camp Slough, 30.37 acres to A-1 south of the slough, approval of the Planning Commission initiated rezoning of 37 acres to M-2 at the southwest corner of Sperry Road and Airport Way, and the 4.3 acre abandoned sewage treatment plant site to E-P north of the slough and west of Airport Way, based on the following findings:

- 1 The proposal conforms to the City of Stockton General Plan designation and policies for the subject site
- 2 With respect to the proposed rezoning north of French Camp Slough and excluding the 37 acres at the southwest corner of Airport Way and Sperry Road, the E-P, Enterprise Performance District, would be a more appropriate zone than M-2 zoning for the subject site as it provides setback, aesthetic and sign controls, and other performance criteria regulating the future development of the site and its compatibility with the Stockton Metropolitan Airport and the nearby existing and proposed industrial parks
- 3 With the exception of the conversion of prime agricultural land, all potentially adverse environmental effects of subsequent development of the area will be mitigated to an acceptable level through project design and/or by enforceable conditions of approval on the associated tentative subdivision map and on any subsequent Use Permits and construction permits for specific projects within the project area
- 4 The benefits of the proposed project outweigh the unavoidable conversion of prime agricultural land to urban use as supported by EIR3-82 and by the following overriding considerations
 - a The proposed E-P zoning combined with the M-2 will benefit the immediate area
 - b It will have a positive economic impact on the entire area
 - c The proposed E-P zone is consistent with the Airport Land Use Plan and the Stockton Metropolitan Airport Overlay Zones
 - d If the entire property were to be developed in the County, it could have a negative effect if developed with problemed uses generally associated with the M-2 zones
 - e The project could be developed in the County since it is presently in conformance with their General Plan designation for the site This would result in overlapping and duplicative jurisdictional situations Since the development is in need of urban services and municipal facilities, it is logical that the area be developed in the City and served by municipal government and services
 - f All urban services and facilities are available and can readily be extended to the project site

Honorable City C 11
March 28, 1984
Page 5

Notification Notice in the Stockton Record ten (10) days prior to the public hearing Owners of record as shown on the last equalized tax roll and addresses within 300 feet of the site

Five votes of the City Council are necessary to approve this rezoning



JOHN CARLSON, SECRETARY
CITY PLANNING COMMISSION

JC pb

Attachments

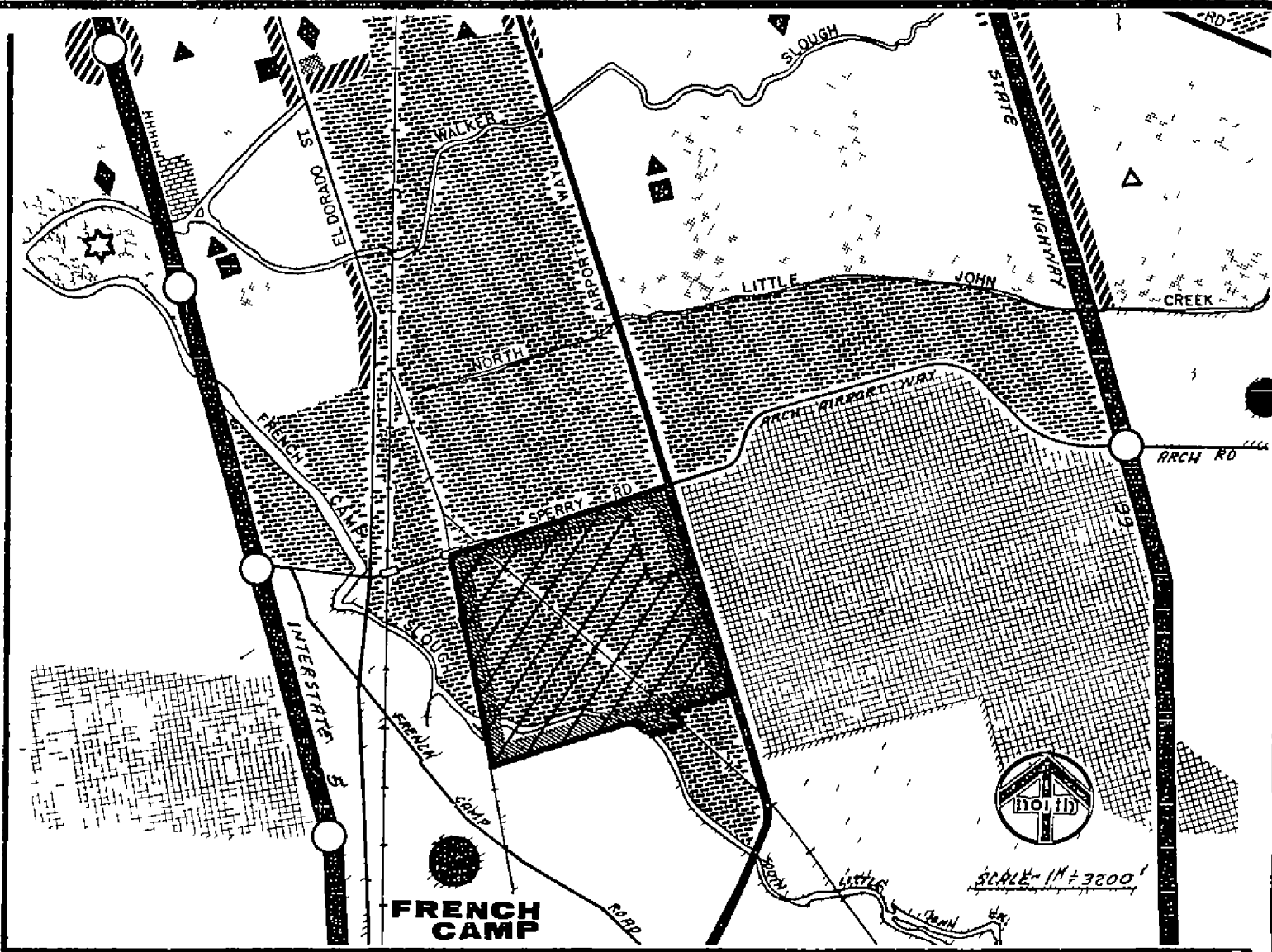
cc City Manager w/attachments
City Attorney w/attachments

LEGEND

LAND USE	
LOW DENSITY RESIDENTIAL	PERFORMANCE INDUSTRIAL
HIGH DENSITY RESIDENTIAL	INDUSTRIAL
ADMINISTRATIVE PROFESSIONAL	INSTITUTIONAL
LIMITED COMMERCIAL	PARKS AND RECREATION
COMMERCIAL	OPEN SPACE & AGRICULTURE

CIRCULATION	
MINOR ARTERIAL	—————
MAJOR ARTERIAL	—————
FREEWAY	—————
INTERCHANGE PARTIAL & FULL	—————
RAILROAD	—————
RAILROAD GRADE SEPARATION	—————

SCHOOLS AND PARKS	
ELEMENTARY SCHOOL	□
MIDDLE SCHOOL	□
SENIOR HIGH SCHOOL	□
COLLEGE OR UNIVERSITY	□
NEIGHBORHOOD PARK	◆
COMMUNITY PARK	◆
REGIONAL PARK	★
SATELLITE COMMUNITY	●



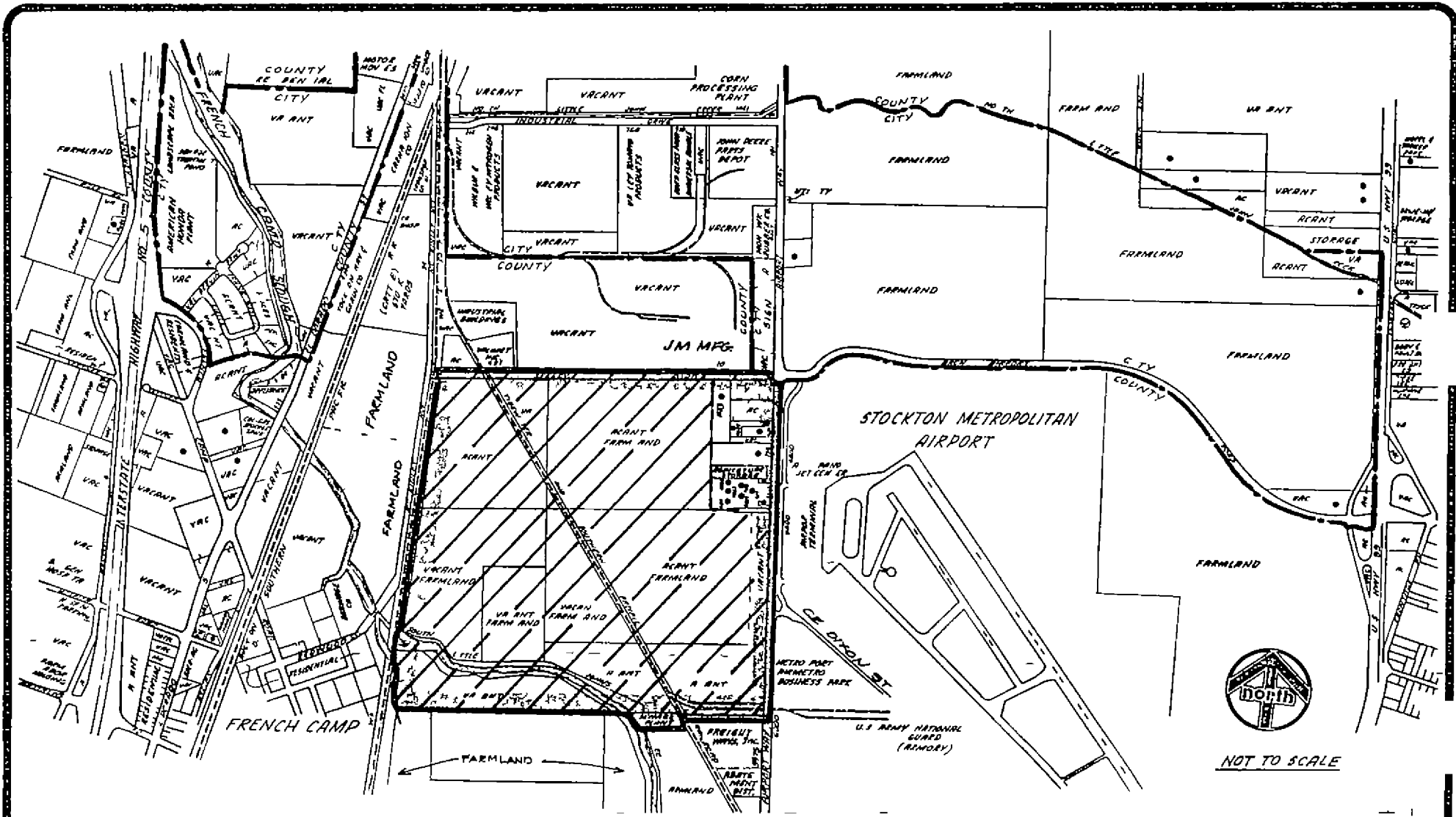
2-12-83
and
T.M 29-83

GENERAL PLAN MAP
STOCKTON AIRPORT INDUSTRIAL, LTD
and CITY of STOCKTON - EIR 3-82

LEGEND	
	PROPOSED ANNEXATION AREA
	PROPOSED SUBDIVISION

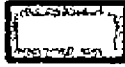


STOCKTON CITY PLANNING COMMISSION

PH DATE 3-8-84

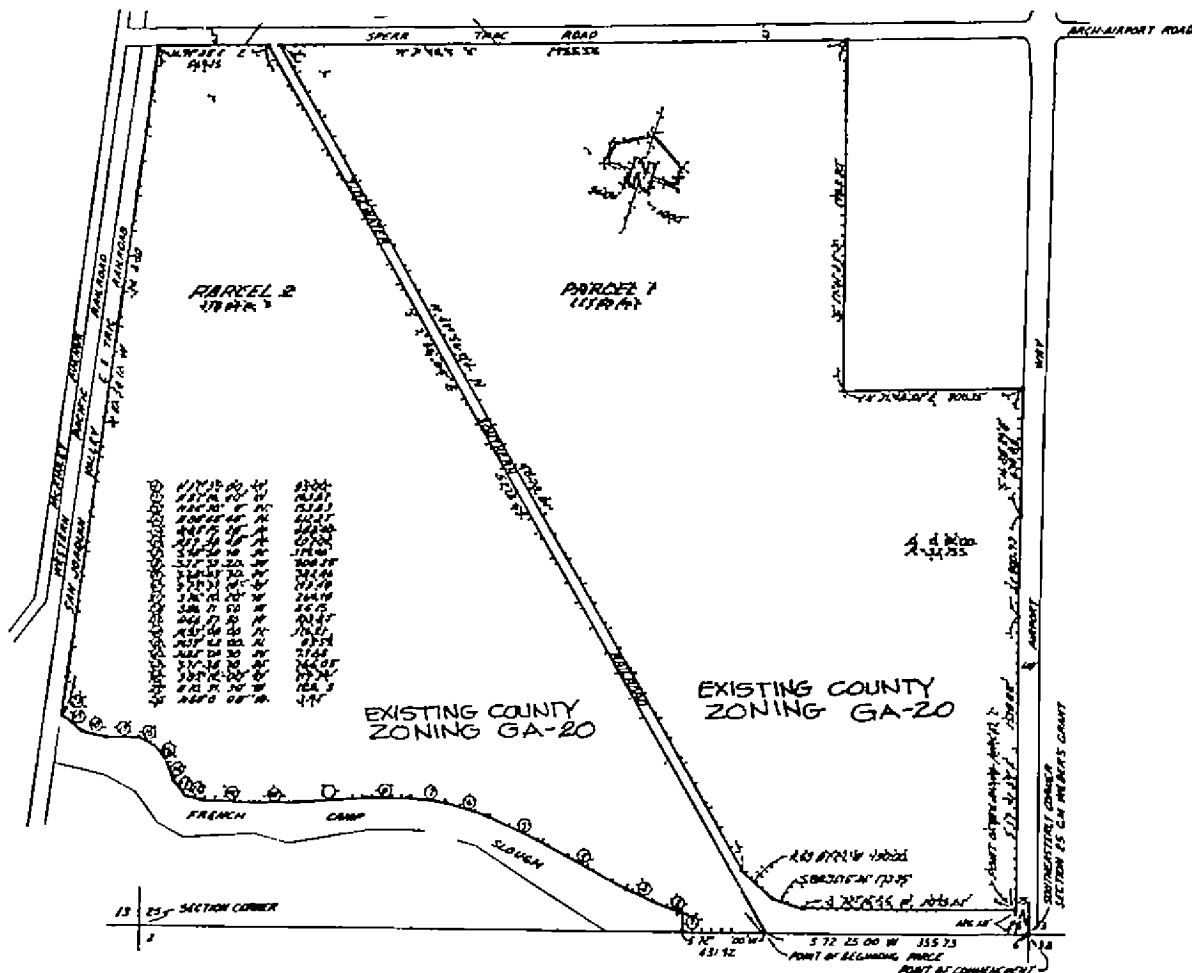


7-12-83 and TM 29-83 LAND USE MAP
STOCKTON AIRPORT INDUSTRIAL, LTD.
and CITY of STOCKTON - EIR 3-82

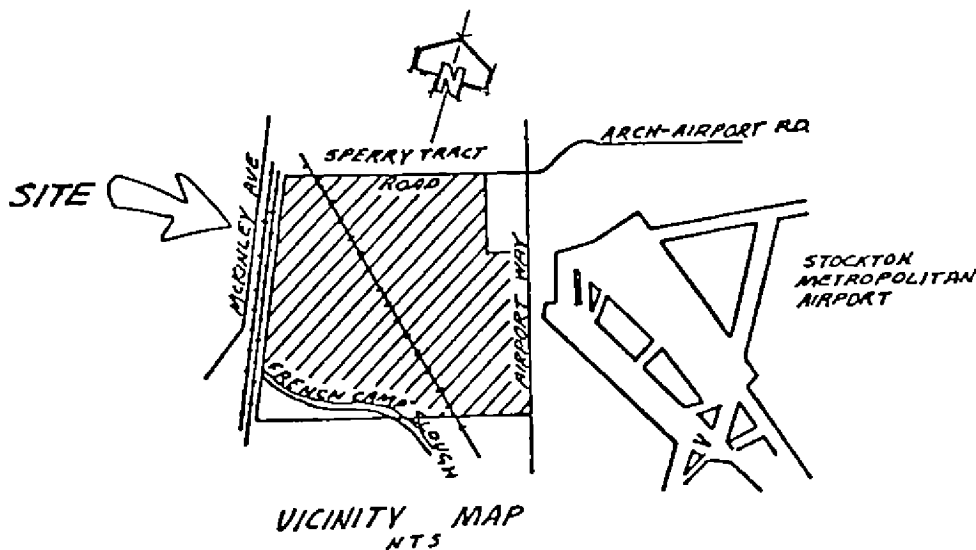
LEGEND

	PROPOSED ANNEXATION AREA (516 Ac)
	PROPOSED SUBDIVISION (447 Ac)
	SINGLE FAMILY UNITS

STOCKTON CITY PLANNING COMMISSION PH DATE 3-8-84



PROPOSED PREZONING TO EP



STOCKTON AIRPORT INDUSTRIAL LTD

Z-12-83 PH DATE 3-8-84

STOCKTON CITY PLANNING COMMISSION

PROPOSED PREZONING TO M-2

SPERRY TRACT RD

ARCH-AIRPORT RD

N 71° 48' 43" E 880.56
 Point of Beginning
 R = 38.00
 Δ = 87° 37' 22"
 L = 58.11

S 20° 33' 55" E 197.22

927.47

S 18° 58' 39" E

37.00 Acres

S 71° 48' 15" W 450.00

S 16° 50' 39" E 150.00

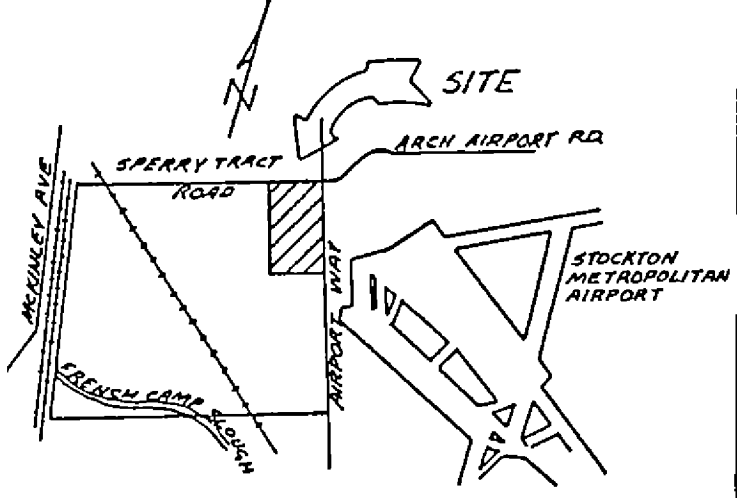
N 71° 48' 15" E 450.00

S 71° 48' 01" W 906.15

EXISTING COUNTY ZONING M-2
 N 17° 41' 43" W 1763.30

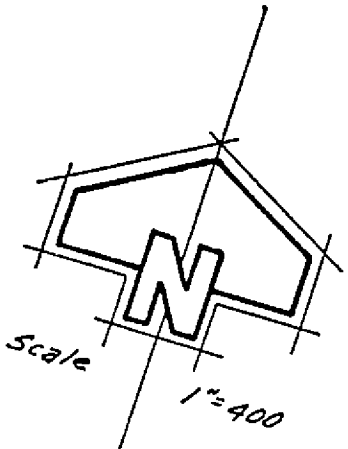
AIRPORT WAY

Northeasterly Corner
 Section 25 C M Weber's Grant
 Point of Commencement



CITY PLANNING
 COMMISSION INITIATED

Z-12-83 PH DATE 3-8-84



STOCKTON CITY PLANNING COMMISSION

STAFF REPORT

Item H-1 PUBLIC HEARING - Rezoning
Case No Z-12-83, Stockton Airport Industrial, Ltd , and City
Planning Commission Initiated

Data Stockton Airport Industrial, Ltd is requesting rezoning of 401 84 acres to E-P, Enterprise Performance District, on the west side of Airport Way, north of French Camp Slough. An additional 30 37 acres east of the Western Pacific Railroad line, south of the Slough, is proposed to be rezoned to A-1, Agricultural District. Both of these sites are currently zoned GA-20, General Agricultural District, in San Joaquin County.

At its regular meeting of June 9, 1983, the City Planning Commission initiated rezoning of the remaining parcels within a proposed 516 acre annexation area at the southwest corner of Airport Way and Sperry Road. Specifically, the Commission initiated rezoning to M-2, Heavy Industrial District, for a 37 acre area, currently zoned M-2 in the County, and to E-P zoning for a 4 3 acre abandoned sewage treatment plant site, currently zoned GA-20 and I-PA, Interim Protected Agriculture, in the County, west of Airport Way and northeast of French Camp Slough (Zoning Map pages 71 and 74). See attached exhibits.

This item is directly related to EIR 3-82, Item G-1a, and to the Airport Gateway Center tentative map case Amended TM29-83, Item I-1, on this same agenda. The property and the surrounding land uses have been previously described in the EIR staff report.

General Plan The General Plan designates the project area for Industrial uses north of French Camp Slough and for Open Space/Agriculture south of the slough. The rezoning site is currently under the jurisdiction of San Joaquin County. The County's General Plan also designates the site as Industrial and Open Space/Agriculture.

Environmental Clearance An environmental impact report (EIR 3-82) has been prepared for the project. The Planning Commission must review and consider the Final EIR and make a recommendation to the City Council as to its adequacy prior to taking action on this rezoning case. Also, the Commission must adopt a statement of overriding considerations with findings supporting conversion of the site to urban use if the proposed rezoning is to be approved (see finding No 4 below).

Discussion The project involves rezoning and annexation of approximately 516 acres. Of this area, 447± acres will be subdivided to form the 84-lot Airport Gateway Center. The subdivision will include 41 lots totaling 166± acres to be used for professional and corporate offices, 40 lots totaling 176± acres to be designated for airport-dependent high technology industrial use such as assembly of electronic components, two lots totaling 10± acres for a retention pond, and one 30± acre lot south of French Camp Slough which will remain in agricultural use. Total build-out of the subdivision is expected in ten years, phasing is not presently being contemplated. Before development can occur, a California Land Conservation (Williamson Act) contract must be terminated for the 401 acres proposed for development.

As background information, the City Council has authorized the filing with the Local Agency Formation Commission (LAFCO) of the annexation request by Stockton Airport Industrial, Ltd for the approximately 516 acres at the southwest corner of Airport Way and Sperry Road. At its meeting on January 20, 1984, LAFCO considered the application and the City's Resolution No. 30313 (January 3, 1984) requesting that processing of the application proceed at that meeting. LAFCO determined that rezoning would resolve any questions concerning future zoning, as discussed in the EIR, and would provide a procedure in which the City Council would certify the Final EIR. Therefore, LAFCO continued the hearing pending City rezoning of the proposed annexation area.

The Planning Commission has authorized the consideration of rezoning for the areas within the annexation area which are not owned by Stockton Airport Industrial, Ltd in order to insure consistency and compatibility with the subsequent development of an industrial/office park which is proposed by Stockton Airport Industrial, Ltd. The Commission has authorized consideration of rezoning to M-2 for the 37 acres at the northeast corner of the annexation area in order to allow for the option of recommending M-2 zoning or a more restrictive zone, such as E-P, for the area.

It is staff's opinion that, because of the setback, landscaping, aesthetic, and sign controls required, E-P zoning is more compatible than M-2 zoning for the entire annexation area north of French Camp Slough. It should also be noted that both the County Planning Department and the Manager of the Stockton Metropolitan Airport have indicated that E-P zoning would be more compatible with the airport, as a major entrance to Stockton and San Joaquin County, than the potentially more visually offensive uses which would be permitted if said areas were zoned M-2, Heavy Industrial District. Nonetheless,

several letters have been received from property owners at the southwest corner of Airport Way and Sperry Road protesting the annexation of the area if the City rezoned the existing County M-2 area to E-P zoning (see attached letters)

Recommendation Approval of rezoning to E-P for the entire annexation area north of French Camp Slough and to A-1 for the remaining area south of the slough based on the following findings

- 1 The proposal conforms to the City of Stockton General Plan designation and policies for the subject site
- 2 With respect to the Planning Commission initiated rezoning, the E-P, Enterprise-Performance District, would be a more appropriate zone than M-2 zoning for the subject site as it provides setback, aesthetic, and sign controls, and other performance criteria regulating the future development of the site and its compatibility with the Stockton Metropolitan Airport and the nearby existing and proposed industrial parks
- 3 With the exception of the conversion of prime agricultural land, all potentially adverse environmental effects of subsequent development of the area will be mitigated to an acceptable level through project design and/or by enforceable conditions of approval on the associated tentative subdivision map and on any subsequent Use Permits and construction permits for specific projects within the project area
- 4 The benefits of the proposed project outweigh the unavoidable conversion of prime agricultural land to urban use as supported by EIR 3-82 and by the following overriding considerations
 - a An existing need for the proposed type of land use in the Stockton area has been demonstrated
 - b As the site is isolated from the parcels under Williamson Act contracts, development of the site is not expected to encourage other land owners to terminate such contracts for their parcels. Additionally, the isolation of the subject property from other contracted lands seems to indicate that no nearby parcels are being considered for long term agricultural use

- c The project site is located within the Stockton Sphere of Influence and is consistent with both the City of Stockton and San Joaquin County General Plans, the Airport Land Use Plan, and the Stockton Metropolitan Airport Overlay Zone
- d The project could be developed in the County since it is presently in conformance with their General Plan designation for the site This would result in overlapping and duplicative jurisdictional situations Since the development is in need of urban services and municipal facilities, it is logical that the area be developed in the City and served by municipal government and services
- e All urban services and facilities are available and can readily be extended to the project site
- f Upon full development, the subdivision could provide about 12,000 jobs, which will significantly benefit the economy of the City of Stockton and San Joaquin County

February 29, 1984

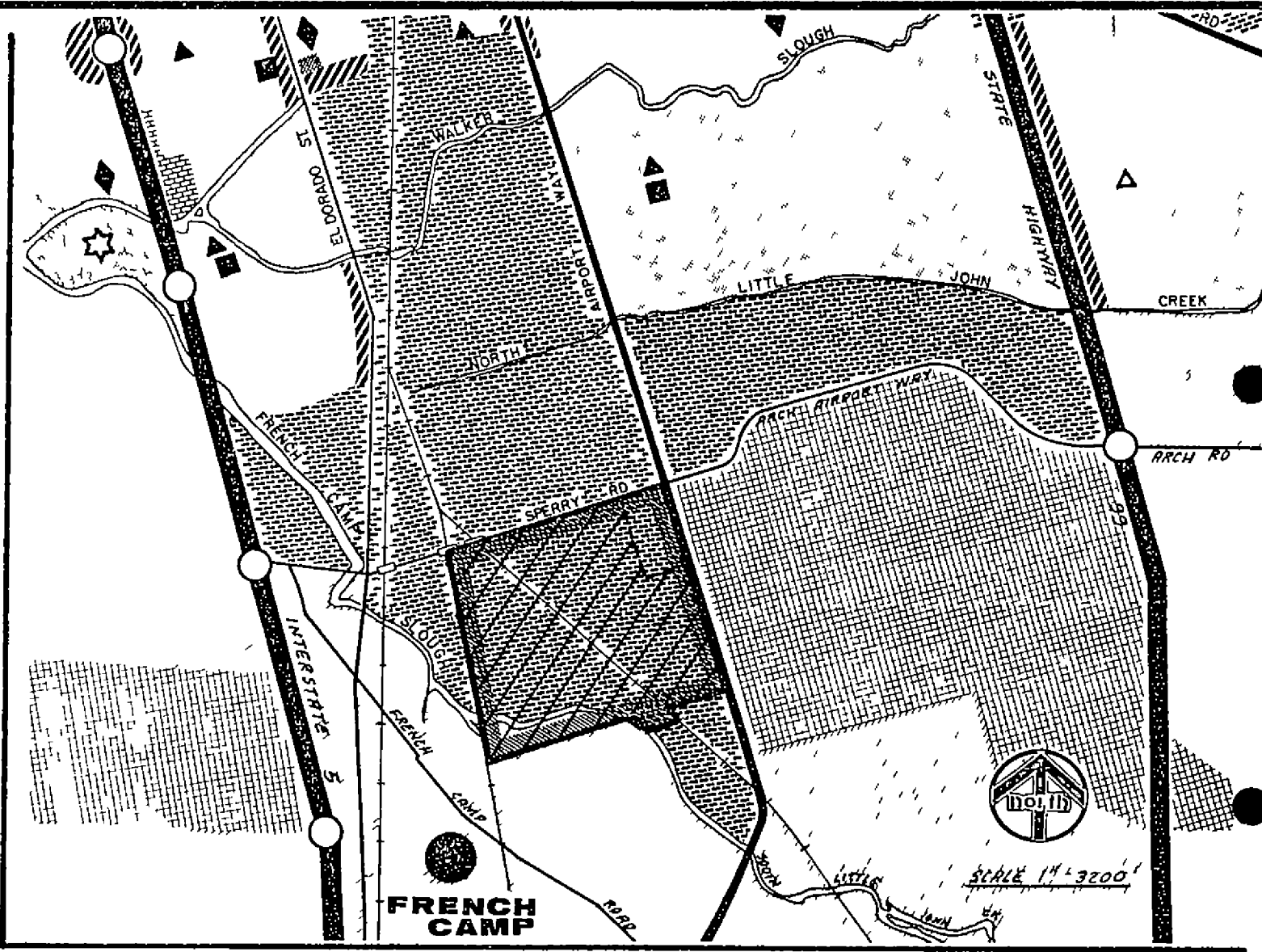
Note Staff reports are prepared well in advance of the Planning Commission consideration of the proposal and reflect the staff's view based on the best available information at the time the report was formulated Evidence submitted during the course of the public hearing may require a reevaluation of the staff's position

LEGEND

LAND USE	
LOW DENSITY RESIDENTIAL	PERFORMANCE INDUSTRIAL
HIGH DENSITY RESIDENTIAL	INDUSTRIAL
ADMINISTRATIVE PROFESSIONAL	INSTITUTIONAL
LIMITED COMMERCIAL	PARKS AND RECREATION
COMMERCIAL	OPEN SPACES & AGRICULTURE

CIRCULATION	
MINOR ARTERIAL	
MAJOR ARTERIAL	
FREEWAY	
INTERCHANGE PARTIAL & FULL	
RAILROAD	
RAILROAD GRADE SEPARATION	

SCHOOLS AND PARKS	
ELEMENTARY SCHOOL	
MIDDLE SCHOOL	
SENIOR HIGH SCHOOL	
COLLEGE OR UNIVERSITY	
NEIGHBORHOOD PARK	
COMMUNITY PARK	
REGIONAL PARK	
SATELLITE COMMUNITY	



GENERAL PLAN MAP

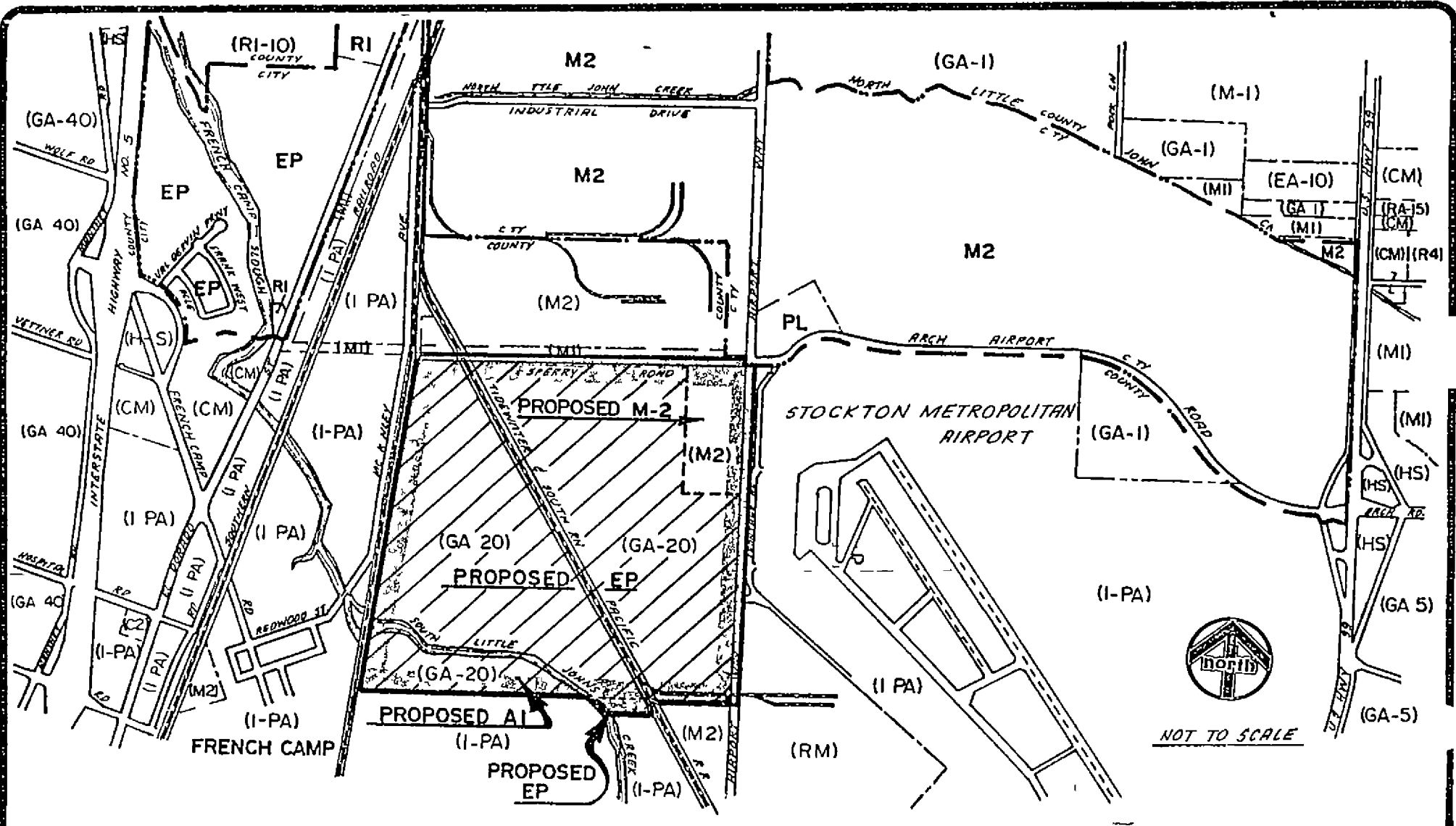
LEGEND

<i>STOCKTON AIRPORT INDUSTRIAL, LTD</i>		PROPOSED ANNEXATION AREA
<i>and CITY of STOCKTON - EIR 3-82</i>		PROPOSED SUBDIVISION

2-12-83
and
TM 29-83

STOCKTON CITY PLANNING COMMISSION

PH DATE 3-8-84



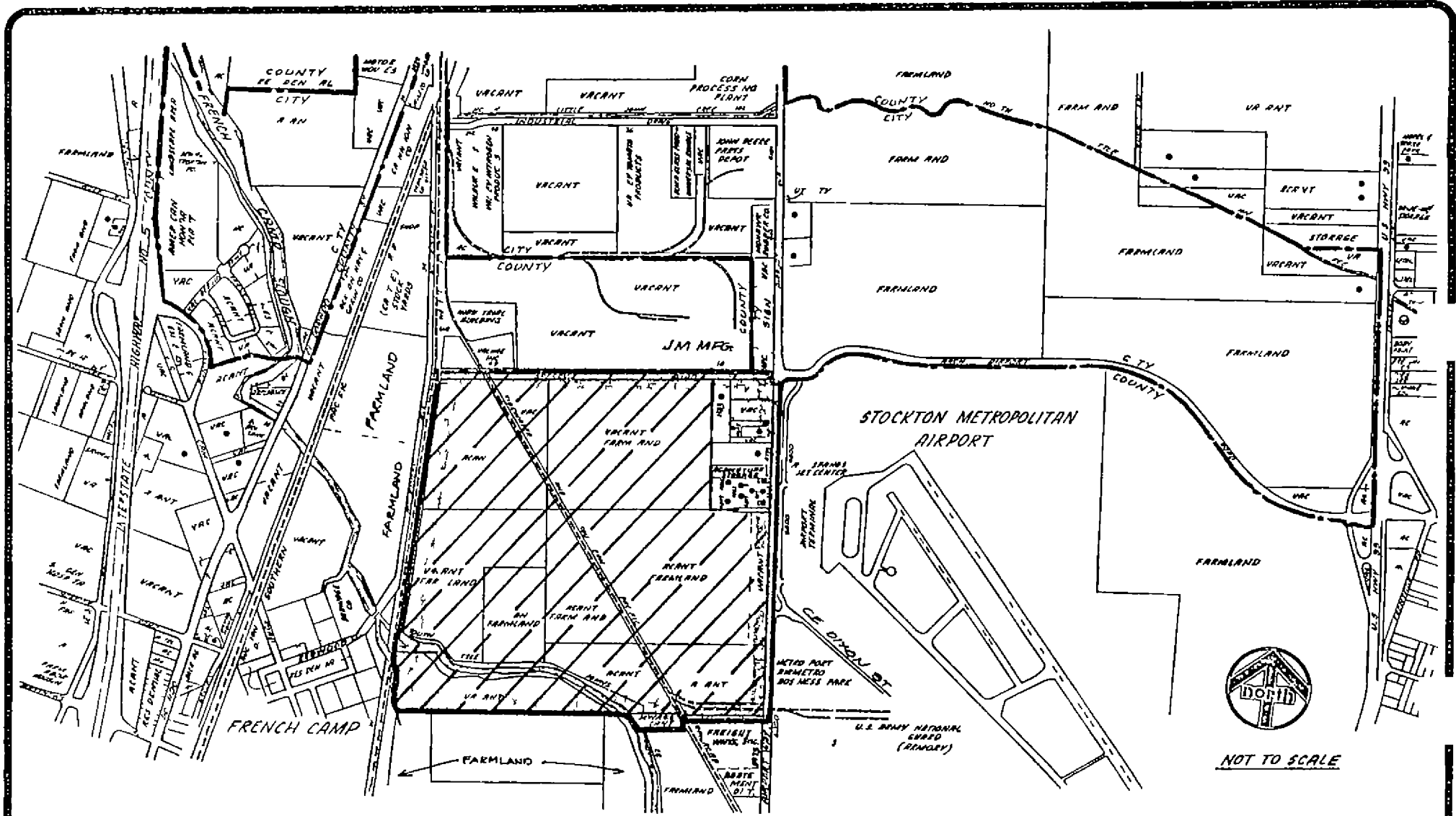
2-12-83 and TM 29-83
STOCKTON AIRPORT INDUSTRIAL, LTD.
 and **CITY of STOCKTON - EIR 3-82**

ZONING MAP

M2
 (M2)

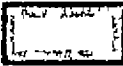


LEGEND

PROPOSED ANNEXATION AREA (516 Acres)
 PROPOSED SUBDIVISION (447 Acres)
 CITY ZONING
 COUNTY ZONING



Z-12-83 and TM 29-83 LAND USE MAP
STOCKTON AIRPORT INDUSTRIAL, LTD.
and CITY of STOCKTON - EIR 3-82

LEGEND

	PROPOSED ANNEXATION AREA (516 Ac)
	PROPOSED SUBDIVISION (447 Ac)
	SINGLE FAMILY UNITS

RECEIVED

APR 27 1983

April 25, 1983

**CITY CLERK
CITY OF STOCKTON**

CITY MANAGER	<u>1 ✓</u>
CITY ATTORNEY	<u>1 ✓</u>
CITY COUNCIL	<u>9 ✓</u>
COM. DEV	<u>1 ✓</u>
PUB. WORKS	_____
FINANCE	_____

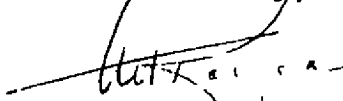
City of Stockton
Community Development Department
Stockton, California

Re Annexation A-82-2

Gentlemen

I submitted a letter to you on March 31, 1983 protesting the annexation and rezoning of my property located on Airport Way south of Sperry Road. In that regard, I respectfully request that I be notified prior to any public or private meetings that may effect any decision related to zoning or annexation of mine or the surrounding properties

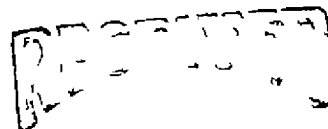
Yours truly,



Art Korock

cc Planning Department
Planning Comm
City Council

1180 Country Club
Stockton Ca
95204



RECEIVED
APR 27 1983

April 11, 1983

Gentlemen

It is with regret that I must inform you of my opposition to the annexation and down zoning of mine and my neighbors property I cannot understand how your planning staff could possibly recommend that the entire parcel under consideration be designated for other than heavy industrial use Would you please reconsider the zoning aspect of the proposal before proceeding any further with the annexation

Sincerely yours,

Parvado Soler
4651 S Airport Way

RECEIVED

APP 19 1983

CITY OF ST. CATHARINES
COMMUNITY DEVELOPMENT DEPT.
PLANNING DIVISION

April 11, 1983

Gentlemen

It is with regret that I must inform you of my opposition to the annexation and down zoning of mine and my neighbors property I cannot understand how your planning staff could possibly recommend that the entire parcel under consideration be designated for other than heavy industrial use Would you please reconsider the zoning aspect of the proposal before proceeding any further with the annexation

John V. Balla
Sincerely yours,

file = April 17 1983

RECEIVED

APP 19 1983

CITY OF STROVTON
CITY ENGINEER DEPT.
PLANNING DIVISION

April 11, 1983

Dear Sirs

I have been informed that the city wants to down zone the entire area under consideration for annexation to included my property, therefore, I am opposed to annexation. The county has designated the entire area for heavy industrial use which is consistent with the zoning of the surrounding businesses such as Valmet and Johns-Mansville

Very truly yours,

Eusebio Cabalar
Sara R Cabalar

RECEIVED

APR 13 1983

CITY OF STOCKTON
COMMUNITY DEVELOPMENT DEPT.
PLANNING DIVISION

307
RECEIVED

APR 11 1983

CITY OF STOCKTON
COMMUNITY DEVELOPMENT DEPT.
PLANNING DIVISION

April 6, 1983

Gentlemen

I am writing this letter to formally protest the proposed down zoning of my property from M-2 to Enterprise Performance Zone. It has been my understanding that the city would allow me to keep my present zoning if I did not oppose annexation, but apparently your position has changed since the entire area including my property is now designated for down zoning to Enterprise Performance which is not the best land use for my property or the area surrounding it.

Sincerely,

Clarence A. Brooks
Lily E. Brooks

24600 N. Highway 101
Stockton, CA

March 31, 1983

City of Stockton
Community Development Dept
Stockton, CA

RE Annexation - A-82-2

It is with regret that I must submit my formal protest to the above reference annexation. Upon receipt of the initial proposal (see attached), I contacted the Community Development Dept and expressed my concerns in regard to my property being down zoned. I was informed that my existing zoning of H-2 would not be changed under the proposal.

However, it is now the Planning Department's intent to prezone all of the annexed land to Enterprise Performance District, which is not acceptable for my present or future use and is not compatible with the existing land use in the area. It is also not in conformance with the County general plan which specifically shows this area for heavy industrial use.



Art Koice
1130 Count Club Blvd
Stockton, CA 95204

AK 20

cc Planning Division
Planning Commission ✓
City Council

RECEIVED
APR 6 1983
CITY OF STOCKTON
COMMUNITY DEVELOPMENT DEPT.
PLANNING DIVISION



CITY OF STOCKTON

CITY HALL
STOCKTON CA 95202

COMMUNITY DEVELOPMENT DEPARTMENT

(209) 944-8266

March 20, 1984

Stockton Airport Industrial, Ltd
c/o TMI
P O Box 2500
Newport Beach, CA 92660

APPROVED TENTATIVE MAP - STOCKTON AIRPORT INDUSTRIAL, LTD
(AIRPORT GATEWAY CENTER) (AMENDED TM29-83)

At its regular meeting of March 8, 1984, the City Planning Commission approved your request to subdivide 447 acres at the southwest corner of Sperry Road and Airport Way, across from the Stockton Metropolitan Airport (Airport Gateway Center) (Amended TM29-83)

This approval is subject to the following conditions

- 1 Comply with applicable State, County and City codes and regulations and adopted standards
- 2 Soils reports shall be required prior to any submission of plans for construction
- 3 Street names shall be approved by the Community Development Director
- 4 A complete master storm water drainage system, including a detention basin, shall be provided by the owner/developer as described in the engineering report. On-site storm water drains and the detention basin shall be sized to provide adequate capacity for the tributary area. Provisions to serve off-site parcels, such as enlargement of the detention basin, shall be at the expense of the owners/developers of the off-site parcels

'Stockton Someplace Special'

The owners shall be responsible for maintenance of the detention basin. When further development of the off-site properties within the tributary area occurs, the City shall require the owners/developers of these parcels to pay an equitable share of the cost of maintaining the drainage system, including the detention basin. The boundaries of the tributary area have been refined, based on data supplied by the applicant, and have been field reviewed by the Department of Public Works staff and are shown in the addendum to EIR 3-82.

The owner/developer shall provide and have approved any interim storm drain plan for phased development. Said approval shall precede approval of the final map.

- 5 If any industries store, transport or use toxic substances on the subdivision site, sufficient design measures shall be incorporated to keep spills of such materials from entering French Camp Slough or the subdivision drainage system. Such design measures shall be approved by the City during the design review process and by the Water Quality Control Board before a Waste Discharge Permit is granted.
- 6 The owner/developer shall obtain permits and shall comply with all requirements of all government agencies with jurisdiction over French Camp Slough including, but not limited to, dedication of right-of-way, access easements, fencing, outfall structure and necessary improvement of French Camp Slough.
- 7 All building pads shall be raised to conform to flood insurance requirements and City Flood Plain Management Ordinance.
- 8 The owner/developer shall participate in an Area of Benefit for South Industrial Sanitary Sewer Trunk Line.
- 9 The available capacity for this development in the 27 inch Southern Industrial Sanitary Sewer Trunk Line is very limited. A new sanitary sewer line may be necessary in the future when this area is fully developed. Any sanitary sewer connections to this line must be approved by the Municipal Utilities Department.
- 10 The area of the subject map shall be annexed to the City of Stockton prior to approval of the final map.

11 Traffic signals shall be required at the following locations and owner/developer participation shall be as indicated

a	Airport Way/Arch Sperry Road	25%
b	Airport Way/C E Dixon-Aviation Way	50%
c	Sperry Road/Hughes Street (required only when Sperry Road is extended westerly of site)	100%
d	Sperry Road/Aviation Way	75%
e	Sperry Road/Belding Street	75%
f	Aviation Way/Lindberg Street	100%
g	Aviation Way/Belding	100%

These traffic signals shall be installed when warranted or at the discretion of the Director of Public Works

12 The owner/developer shall be responsible for all necessary on-site improvements, with the exception of those improvements to be funded through assessment districts or Area of Benefit, which will spread the cost equitably to benefiting owners. In addition, the owner/developer shall contribute to the off-site improvements specified below

- a The owner/developer shall provide three southbound lanes on Airport Way along the tract frontage. A minimum of a 62 foot half section along the tract frontage shall be dedicated to the City of Stockton. Flaring of intersections will be required to accommodate projected traffic volumes, an additional 7 foot dedication will be required for intersection flares. Flaring of the north leg of the Airport Way/Sperry Road intersection will also be necessary and shall be provided by the owner/developer, the City shall attempt to recover a proportionate share of the costs from future developments
- b The owner/developer shall construct full improvements on Sperry Road including four lanes, a raised median with turning lanes (dual left-turn lanes at Airport Way), and intersection flares as required. A 67 foot half section and 134 foot full cross section along the proposed alignment of Sperry Road shall be dedicated to the City of Stockton. Additional dedication will be required for intersection flares. Construction of additional lanes shall be funded through an assessment district or Area of Benefit

If the owner/developer elects to finance construction of the grade separation crossing of the Tidewater Southern Railroad prior to the formation of an assessment district, all costs above the landowner's/developer's assessment shall be credited fully toward equitable participation in any assessment district or Area of Benefit formed to finance improvements to the Sperry Road/Arch Airport Road corridor

The proposed grade separation structure over the Tidewater Southern Railroad shall have a 100 foot full cross section and will contain six lanes, an 8 foot median and sidewalks

- c The owner/subdivider shall participate equitably in any assessment district or Area of Benefit formed to construct Sperry Road or Industrial Drive westerly to Interstate 5/French Camp Turnpike Road including railroad overcrossings, waterway bridges, and Interstate 5 interchange improvements
 - d The owner/subdivider shall participate equitably in any assessment district or Area of Benefit formed to realign and/or widen Arch-Airport Road easterly of Airport Way to provide a 6 lane cross section
 - e The owner/subdivider shall participate equitably in any assessment district or Area of Benefit formed to widen or reconstruct the Arch-Airport Road overcrossing at the interchange with State Route 99
- 13 The owner/developer shall include and adopt a statement in the Conditions, Covenants and Restrictions (CC&Rs) stating that all owners/developers of the subdivision lots must participate in the implementation of a Transportation Systems Management (TSM) Program when requested to do so by the City of Stockton Possible TSM measures include appointing a transportation coordinator to organize car and van pools, implementing flex-time or staggered work shifts, providing preferential parking to car-pooling vehicles, and other similar measures
- 14 The owner/developer shall dedicate access rights to the City of Stockton at the following locations along the west side of Airport Way, except at the Lindberg

Street and Aviation Way intersections, along the north and south sides of Sperry Road, except at the Belding Street, Aviation Way and Hughes Street intersections, along Belding Street, Aviation Way and Lindberg Street and Hughes Street within 100 feet of Airport Way and Sperry Road, and along Runway Street and Old Sperry Road within 200 feet of the centerline of the Tidewater Southern Railroad right-of-way Direct lot access in these areas shall be prohibited

- 15 The owner/developer shall be responsible for the construction of crossing protection to PUC standards at the at-grade crossing of the Tidewater Southern Railroad by Runway Street No building shall be located within a sight triangle extending 400 feet down the centerline of the track from each side of the street and 100 feet down the street from each side of the railroad crossing
- 16 All requirements of the Public Utilities Commission pertaining to building setbacks along railroad spur lines shall be the responsibility of the owner/developer
- 17 Before a detailed site plan is developed, the applicable railroad company shall be consulted regarding specific requirements for spur line development
- 18 Until the westerly extension of Sperry Road to Interstate 5 is constructed, the owner/developer shall retain the option of extending Hughes Street to the existing Sperry Road alignment and improving the at-grade crossing of the Tidewater Southern Railroad
- 19 Lindberg Street shall be limited to right-turn access only to Airport Way The existing median opening shall be closed per the Stockton Metropolitan Airport Specific Plan Amendments for Airport Way
- 20 All mitigation measures listed in the Airport Gateway Center Final Environmental Impact Report (EIR 3-82, SCH#82120217) within the purview of the City of Stockton (see, Sections IV 4, 7, 8, 11, 16, 17, and 18 of this EIR) are adopted by this reference as conditions of approval for the tentative subdivision map Implementation shall be the responsibility of the owner/developer unless otherwise assigned in the mitigation measures

Stockton Airport Industrial, Ltd.
March 20, 1984
Page 6

*2 yrs after the Commission is effective -
AUG 31, 1984*

Enclosed is a copy of the approved tentative map which will expire on March 8, 1986. Any request for an extension shall be made in writing, filed with this office and accompanied by the appropriate fee at least thirty (30) days prior to expiration of the tentative map and clearly stating the reasons for requesting the extension. In granting an extension, new conditions may be imposed and existing conditions may be revised.

If you have any questions, please contact Mr. Sam Mah of the Community Development Department, Planning Division, at (209) 944-8266.

John Carlson

JOHN CARLSON, SECRETARY
CITY PLANNING COMMISSION

JC kmg

Enclosure

cc Randall O'Dell w/enclosure
1581 Cummins Drive, Suite 123
Modesto, CA 95351

Luis Manzano and Associates w/enclosure
31320 Via Colinas, Suite 104
West Lake Village, CA 91352

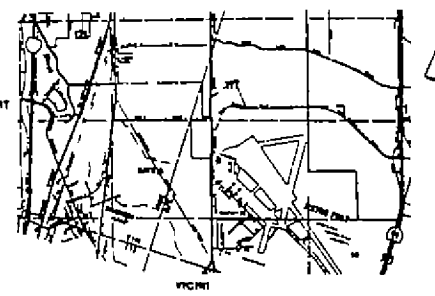
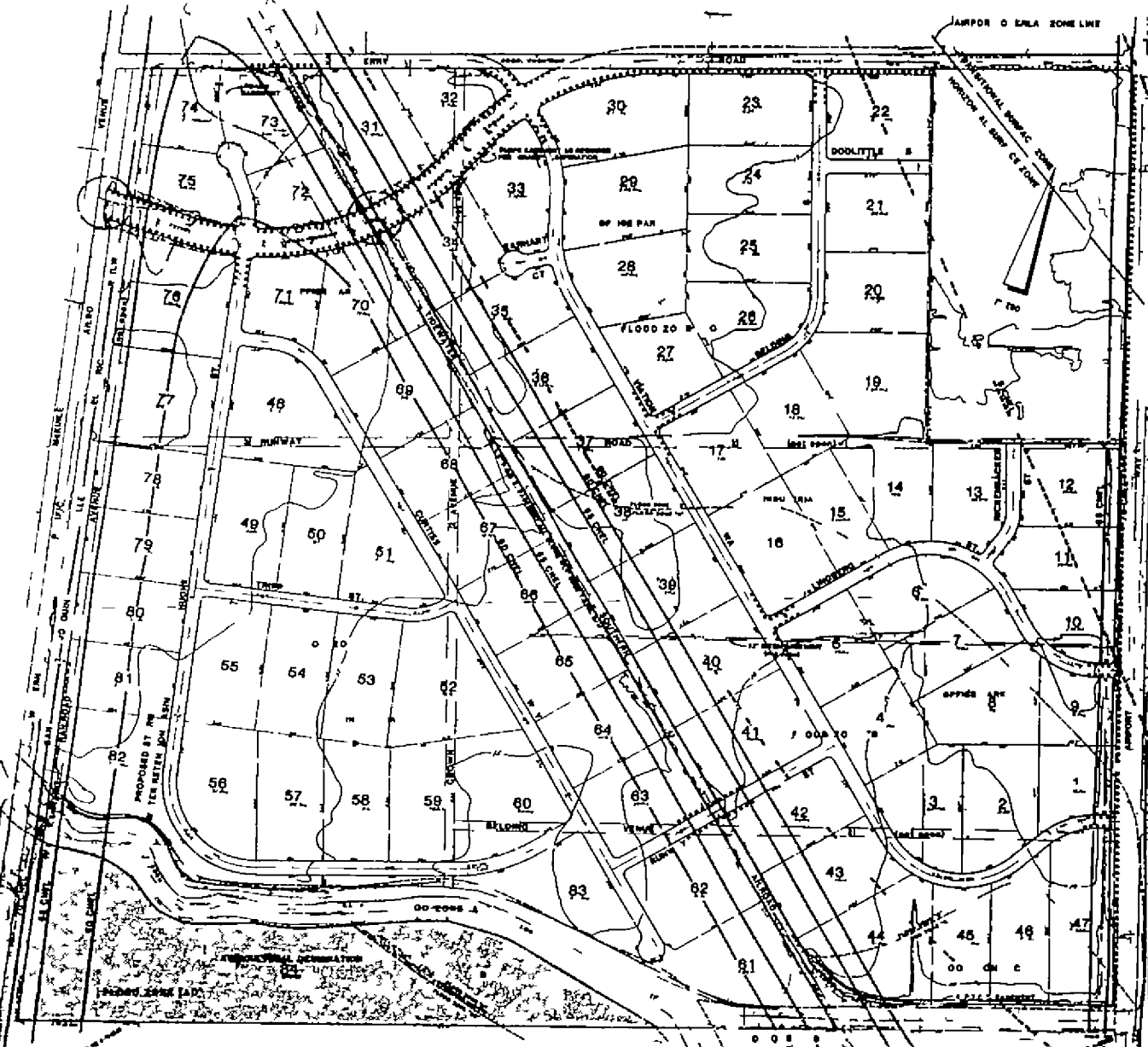
Steve Herum w/enclosure
P O Drawer 20
Stockton, CA 95201

Robert P. Flick w/enclosure
2049 Century Park East
Los Angeles, CA 90067

John Erickson w/enclosure
4560 Enchanted Way
Redding, CA 96001

Planning Associates w/enclosure
P O Box 1527
Redding, CA 96009

Public Works w/sepia



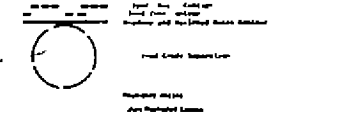
MAP OF TENT TYPE
 TRACT No 1838
AIRPORT GATEWAY CENTE
 A BUSINESS PARK

3 00 ON AIRPORT INDUS RI L. O.
 NO
 TEACHERS MANAGEMENT INVE MENT CORPORATION
 GENERAL PARTNER
 DEM IN C ION 28 AND FOR ION OF SECTION OF
 H. WE GR NT SAN JOAQUIN COUNTY CALIFORNIA

PERTINENT DATA

Area	28.1	72.5
Permit No.	28.1	72.5
Other Data	28.1	72.5
...

LEGEND



SIGNATURES

LAND PLANNING

WE HANZANG & ASSOCIATES
 28 VIA COLINAS SUITE 104
 WESTLAK VILLAGE, CA 91362
 (1 31) 3 3
CIVIL ENGINEERING
 JOHNSON-TOLLGAT ENGINEERS
 5 08 M E D B 6 U I E 11
 MOORE, O.C.A. 95228
 (09) 886

MINUTES OF THE
CITY PLANNING COMMISSION

A regular meeting of the City Planning Commission was held on Thursday, March 8 1984 at 7 30 p m in the Council Chambers, second floor City Hall with Chairman Mike Gold presiding

PRESENT Chairman Mike Gold Vice-Chairwoman Edie Lane
Planning Commissioners Stella Alonzo Frank
Cusumano Wiley Henderson Kathryn Mecca Jeanette
Michaels James Thibodeaux and Floyd Weaver
Community Development Director John Carlson Deputy
Director Bob Ching Deputy City Attorney Janet
Gladfelter and Associate Planners Sam Mah and
Michael Niblock

ABSENT None

Consent Items

Commissioner Henderson noted for the record that page 4 of the February 23 1984 Planning Commission minutes regarding the motion to approve the tentative map request of Cal West Properties Inc (TM2-84) should have been carried 8 to 0 with Commissioner Mecca stepping down rather than being unanimously approved 9 to 0

MOTION It was duly moved (Michaels) seconded (Henderson) and unanimously carried 9 to 0 to approve the minutes of the meeting of February 23 1984 as amended

Chairman Gold read the statement relative to the disclosure of campaign contributions and the policy regarding applications that are pending before the Commission into the record

Environmental Impact Report Stockton Airport Industrial, Ltd, and City of Stockton - Annexation and rezoning of 516 acres including subdivision of and cancellation of the California Land Conservation Contract on 447 acres, for the proposed development of an industrial/office park at the southwest corner of Sperry Road and Airport Way across from the Stockton Metropolitan Airport (EIR 3-82) and

Public Hearing To consider the request of Stockton Airport Industrial, Ltd and Planning Commission initiated rezoning to E-P, Enterprise Performance District A-1 Agricultural District and M-2 Heavy Industrial District of approximately 516 acres at the southwest corner of Sperry Road and Airport Way across from the Stockton Metropolitan Airport (Z-12-83) and

Public Hearing To consider the request of ^{Stockton Airport} Stockton Airport Industrial Ltd to subdivide 447 acres at the southwest corner of Sperry Road and Airport Way

across from the Stockton Metropolitan Airport
(Airport Gateway Center) (Amended TM 29-83) etc.

The public hearings were declared opened and the Affidavits of Mailing and Publication were filed

Associate Planner Michael Niblock described the subject requests and the surrounding land uses

Commissioner Michaels indicated that she was contacted on this matter

Community Development Director John Carlson made reference to a letter dated March 5 1984 addressed to the Mayor and members of the City Council with a copy to the City Planning Commission from Kenneth Blackman City Manager of Santa Rosa California, expressing his support of Teachers Management and Investment Corporation (TMI)

Thomas Shephard 509 West Weber Avenue appeared on behalf of the Stockton Airport Industrial Ltd and general partner TMI He explained that it is the intention of Stockton Airport Industrial Ltd to create within this project area an environment for high tech industry The annexation the zoning of the surrounding properties the improvements to be made in the event the project is approved were discussed at some length The staff report states that all potentially adverse environmental effects will be mitigated to an acceptable level through project design and/or by enforceable conditions of approval on the associated tentative subdivision map and on any subsequent Use Permits and construction permits for specific projects within the project area with the exception of the conversion of prime agricultural and He questions whether this property is prime agricultural land He read into the record a letter from an agricultural land advocate expressing his opinion of this property as being questionable farm land

The development of high tech industries would also reduce the high unemployment in the city particularly amongst the semi-skilled and unskilled laborers Mr Shephard also pointed out that Stockton lacks sites that are truly competitive on a national scale for high tech industry The proximity to the airport and freeways add to the desirability of such a project Other cities such as Fresno San Diego San Jose and Newport Beach have this type of an industry in close proximity to airports This project would also be very much in keeping with the goals and policies of the General Plan Mr Shephard stated that there is a need for additional industrial land of the type proposed Another significant aspect of this project is that it would provide the keystone to the widening of Sperry Road and the extension of Sperry Road will ultimately be used as a connector serving the entire southern industrial area from Highway 99 to Interstate 5 The developer is also prepared to be part of a benefit district that will ultimately pay for the total cost of constructing the four lane connection and provisions will ultimately be made for six lanes on Sperry Road

Although it would be possible to develop this parcel in the County it would not be as desirable for the applicant

or the City because of the total benefits that could be derived

A slide presentation was shown of the proposed site. The project will develop over a ten year period and is anticipated to produce approximately 12 500 jobs when the development is completed.

Mr Shephard also made reference to a letter dated March 7 1984 addressed to the Planning Commission and City Council from Gary G Gillmore attesting that during one of his three terms as mayor of Santa Clara, TMI had demonstrated that they are responsible people and can be counted on to follow through with their commitments.

Robert Fitzpatrick president of TMI, explained that they are the common general partner for the many limited partnerships that they form in California. All of their investors are California educators. No properties are bought or funded out of this state. TMI has been in existence for 17 years and has had approximately 30 000 educators invest in real estate and approximately another 10 000 in other products. All of their projects have been successful. Their investments are primarily in industrial buildings, mini storage projects, shopping centers, office projects, a winery and a hotel.

Mr Fitzpatrick explained that his job is to acquire all the properties and to see to it that they profit from this investment. This is the reason why educators invest. In the process they have an obligation of seeing to it that the projects that they acquire and operate as income properties are representative of the area. Marketing and location are what makes a project successful. He explained that TMI is the catalyst that creates the projects and causes them to happen which means that once the package is put together it is up to TMI to know the type of developer the project fits. He explained the methodology used to complete an entire project and why they are consistent in liking airport locations. He also feels that they can carry out their commitment. If the annexation is approved and the plan is approved then TMI can bring in developers that will implement their plan and will involve themselves in the acquisition and ownership of some of the buildings.

In response to Commissioner Thibodeaux's question regarding the type of high tech industry TMI hopes to attract, Mr Fitzpatrick explained that at present there is a trend that they are going to follow. Many firms are looking to relocate because of affordable housing and land costs. TMI attracts the developers and it is the developers who have the contacts with these various industries.

Commissioner Alonzo was concerned that so far the proponent's presentation has been speculative and that nothing specific has been stated. She asked how the job projections could be reached if there is no idea as to the type of specific businesses that will be located in such a facility. Mr Fitzpatrick explained that the EIR addresses this issue and that the job projections are estimates based on usage of the land.

In response to Commissioner Thibodeaux's question TMI prefers to have this project built within the city because that has been their standard practice. It has been found that if the city stands behind a project that is in a favored location there would be added support however if this project is not approved within the city then they will develop in the county. The developers prefer to be within the city.

Chairman Gold pointed out that some of the surrounding properties wish to remain M-2 rather than go to an E-P zoning. Due to the vast differences in these zones relative to the types of uses that each allows what does TMI propose to make these areas more compatible with each other? Mr. Fitzpatrick explained that perhaps the M-2 zoned properties will follow the same pattern once they see how successful the continuity of the TMI project can be.

In closing Mr. Shephard added that based on TMI's track record and interest that they attract in other communities TMI will provide the opportunity for Stockton to compete for this type of industry that is important for Stockton.

There was further discussion regarding the size shape and proximity to the airport and freeways which makes this particular site more suitable and desirable versus other sites around Stockton.

Dialogue ensued relative to why this parcel was put under the Land Conservation Contract in view of the fact that this land is difficult to farm. The benefit assessment district was further discussed.

(A short recess was taken at 9 08 p m)

(The meeting was resumed at 9 24 p m)

Colleen Riggs 4600 North Pershing Avenue appeared on behalf of the Chamber of Commerce and expressed her strong support of the project because of the number of jobs that this project will create and the additional revenue to the City that will be generated. Once the momentum is started this project is destined to succeed inside or outside of the city. She feels that the City would have everything to gain and much to lose by not including it within its jurisdiction.

Frank Johnson 715 Newhall Court spoke on behalf of the company that he is employed with who owns a nearby parcel of land. His company has owned their property for approximately seven years and he also spoke of the time and efforts that were made in attending public hearings to have this area rezoned to M-2. They are not opposed to annexation but they are certainly opposed to any down zoning. It is vital to their operation that they remain in close proximity to the Stockton Metropolitan Airport and remain in M-2 zoning.

William Dorsey of the Building and Trades Council appeared in strong support of the project because it will reduce Stockton's unemployment rate. He feels that possibly there is a surplus of agricultural land in this area. He also

spoke about the revenues to be generated to the City in the form of utility taxes alone

Commissioner Thibodeaux was concerned whether this piece of property will be more beneficial in terms of employment and economic impacts if it remains zoned M-2 in the county or whether it is rezoned to E-P in the city

John Cyr realtor 840 North El Dorado Street, also appeared in strong support of this project One of Stockton's primary problems is the lack of jobs San Joaquin County has historically been an agricultural-related economy but in recent years the valley has been able to attract a large number of non agricultural-related industries As he sees it the future of Stockton and San Joaquin County is growing industrially and not agriculturally As a practicing realtor he feels that agriculture has been stagnant due to the number of inquiries that he receives regarding the availability of industrial land He has also witnessed some of the projects that TMI has been involved with The Commission should realize the full potential of this area in the future as a major transportation hub and communication center for the northern part of the state as evidenced by the almost 800 trucking firms represented in this area This City now has the opportunity to develop the southern part of town with industrial zoned land that will attract many fine firms which would also assist the airport which may in turn bring back the kind of air service that Stockton needs

Kevin Shay appeared on behalf of American Honda Parts Distribution Center and was in support of the project because he feels that a very positive economic impact can be expected from this project

Curt Cummings, 7117 South Field Way educator and TMI investor spoke favorably of TMI's track record and feels that this community needs to broaden its economic base

Dave Smith 1105 North El Dorado Street presented a letter to the Commission from Elder Gunter President of the Greater Stockton Chamber of Commerce supporting this proposal

Clinton Brooks 716 South Airport Way and Esther Brooks 4863 Airport Way have no objection to the project but are adamantly opposed to their property being rezoned from County M-2 to City E-P

Mr Johnson stated that he failed to mention earlier that the property owners of the presently M-2 zoned land held a meeting regarding this issue The outcome of that particular meeting was unanimous in that these property owners do not have any objection to the annexation but would be opposed to having their properties rezoned to E-P

No one else desired to be heard and the public hearings were closed

MOTION It was duly moved (Weaver) seconded (Michaels) and unanimously carried 9 to 0 that the Commission certify the Environmental Impact Report (EIR3-82) with the finding that

the Environmental Impact Report complies with the environmental standards and regulations as established by the California Environmental Quality Act and the City of Stockton guidelines and that the Commission has reviewed and considered the final EIR prior to any related project approval

MOTION It was duly moved (Thibodeaux) seconded (Henderson) and unanimously carried 9 to 0 to approve the request of Stockton Industrial Ltd and Planning Commission initiated request to prezone to E-P for the annexation area north of French Camp Slough excluding 37 acres at the northeast portion which is to be M-2 and to A-1 for the area south of the slough based on the following findings

- 1 The proposal conforms to the City of Stockton General Plan designation and policies for the subject site
- 2 With respect to the proposed rezoning north of French Camp Slough and excluding the 37 acres at the southwest corner of Airport Way and Sperry Road the E-P Enterprise-Performance District would be a more appropriate zone than M-2 zoning for the subject site as it provides setback aesthetic sign controls and other performance criteria regulating the future development of the site and its compatibility with the Stockton Metropolitan Airport and the nearby existing and proposed industrial parks
- 3 With the exception of the conversion of prime agricultural land all potentially adverse environmental effects of subsequent development of the area will be mitigated to an acceptable level through project design and/or by enforceable conditions of approval on the associated tentative subdivision map and on any subsequent Use Permits and construction permits for specific projects within the project area
- 4 The benefits of the proposed project outweigh the unavoidable conversion of prime agricultural land to urban use as supported by EIR 3-82 and by the following overriding considerations
 - a The proposed E-P zoning combined with the M-2 will benefit the immediate area
 - b It will have a positive economic impact on the entire area
 - c The proposed E-P zone is consistent with the Airport Land Use Plan and the Stockton Metropolitan Airport Overlay Zones
 - d If the entire property were to be developed in the County it could have a negative effect if developed with problemed uses generally associated with the M-2 zones

- e The project could be developed in the County since it is presently in conformance with their General Plan designation for the site. This would result in overlapping and duplicative jurisdictional situations. Since the development is in need of urban services and municipal facilities it is logical that the area be developed in the City and served by municipal government and services.
- f All urban services and facilities are available and can readily be extended to the project site.

Commissioner Weaver and Chairman Gold supported the motion because of the positive impacts that it will have on the economy of the City of Stockton upon full development. Chairman Gold further added that in weighing the situation it was a question of whether it would be more beneficial for the City to have this area remain as farmland and provide, at best a few seasonal jobs or have this area be zoned for high tech industrial uses where thousands of full time jobs could be created year round.

Under most circumstances Commissioner Alonzo would not have supported the proposal because there was nothing specific presented. She favored the motion because of the E-P zoning designation which is one of the most restrictive zones in the code and that it requires setbacks, aesthetic qualities for the buildings, provisions for landscaping and noise reduction and is unlike the M-2 zone.

Commissioner Cusumano had mixed emotions about this proposal because of the interpretation of farmland and what may not be productive for one farmer may be to the next. He supports the issue of growth and would rather see it happen within the City than in the County.

~~NOTION~~ It was duly moved (Alonzo) seconded (Michaels) and unanimously carried 9 to 0 to approve the tentative map request of Stockton Airport Industrial Ltd based on the findings and subject to the conditions as listed in the staff report with the following amendment to Finding No 3-f.

- 3-f Upon full development the subdivision could provide jobs which will significantly benefit the economy of the City of Stockton and San Joaquin County.

Commissioner Thibodeaux stated for the record that the conditions that have been placed on the developer are over-whelming. The cost to the developer will be immense and would lessen the cost to the City.

Public Hearing To consider the request of Guarantee Savings a Federal Savings and Loan Association a Corporation to subdivide 4.4 acres at the southwest corner of Tam O'Shanter Drive and Hammer Lane (TM6-84)

6

- 23 League of California Cities Legislative Bulletin dated August 17 1984 #29-84 (Agenda Report No 43 dated August 27, 1984) filed/information only

The roll call was as follows

AYES	Councilmembers Clayton Coale Fass Herbert Oliva Parkinson Sousa Stebbins and Mayor Ronk
NOES	None
ABSENT	None

The motion to adopt the foregoing consent items was carried unanimously

PUBLIC HEARINGS AND PROTESTS

- 1 Continued hearing to consider the Stockton Airport Industrial Limited Reorganization (A-82-2) including annexation to the City of Stockton and detachment from certain fire districts and the California Land Conservation Contract No 72-C1-523

At this time the City Clerk announced that this was the time and place set for the continued public hearing to consider the Stockton Airport Industrial Limited Reorganization (A-82-2) including annexation to the City of Stockton and detachment from certain fire districts and the California Land Conservation Contract No 72-C1-523

Mayor Ronk declared the public hearing open at the hour of 7 12 p m

Certificate of Posting Notice of Continued Public Hearing filed

Deputy Director of the Community Development Department/Planning Bob Ching stated that at the July 30, 1984 Council meeting numerous questions arose which required response by staff members who were not present concerning the Environmental Impact Report and thus the matter was continued to the August 27 meeting

For background information Mr Ching reviewed the history of this particular project as it related to the environmental clearances He stated that both the Planning Commission and the City Council adopted a finding that the Environmental Impact Report (EIR) 3-82 complies with the environmental rules and regulations as established by the California Environmental Quality Act and the City of Stockton Guidelines and that they had reviewed and considered the final EIR prior to any related project approval Since the Environmental Impact Report 3-82 concluded that the conversion of the site to urban use would constitute a significant environmental impact which cannot be mitigated however the Planning Commission and the City Council adopted a statement of overriding considerations in conjunction with the approval of the rezoning The Environmental Impact Report is found to be adequate for the annexation and all that remains is that a statement of overriding consideration must be adopted prior to the approval of the annexation In addition at that meeting City Council members also questioned the role of the Environmental Impact Report consultants who had worked in conjunction with the City Planning Department to prepare the Environmental Impact Report

In conclusion Mr Ching stated that Gerry Scott of the Local Agency Formation Commission as well as a representative from the Planning Associations Consulting firm who worked with the staff on the EIR were in the audience and are prepared to answer any questions from the Council

Due to the fact that Mr Dario Marengo had the floor at the time the hearing was continued Mr Marengo was asked to come forward and complete his presentation/comments to the City Council

Mr Marengo stated that the bone of contention still is the statement of overriding consideration and questions that were raised by Councilmember Fass concerning the Environmental Impact Report He was under the understanding that a member of the Community Development Department who authored the study on which the statement of overriding consideration hangs was to be present to answer questions which were previously raised and he was disappointed that he was not in attendance The statement of overriding consideration bases its whole context on the report of the overall economic development for the City of Stockton He reviewed his presentation that he made to the City Council on July 30, 1984 and he contended that the statement of overriding consideration was in error and he gave his explanation for his opinion He questioned the figures and formula used by the staff in preparing the EIR as well as the classification of the land previously identified as only adequate for industrial development to an excellent category He again pointed out the development of the industrial park is purely speculative and there are no indications for specific industrial development apparent or evident

Appearing in opposition was John Eilers 21355 Walnut Drive, representing the San Joaquin County Farm Bureau He stated that they opposed the annexation because it is "leaping-frogging" and the boundary lines are not truly contiguous to the existing City limit lines They opposed the annexation because it is the taking of agricultural land without due justification The statement of overriding consideration indicates that this kind of industrial park is necessary for high-tech industry, however, there is a number of vacant industrial land that lies vacant and undeveloped It appears that the City is trying to prepare a full portfolio of industrial land at different prices at the expense of agricultural land There is also the responsibility that the City may have toward those industrial parks that have been established and lying vacant It seems that the County is at this particular time "jobs-at-any-price-industrial-site-hysteria To take prime ag land out of circulation that someone may possibly want is poor planning He stated that the statement of overriding considerations does not adequately cover the situation and recommended that the City Council deny the annexation until the Johns Mansfield property to the north is also included thereby making the property fully contiguous to the City limit lines Mr Eilers answered questions from the Council

Councilmember Sousa raised the question of whether or not it has ever been established as to what constitutes adequate amount of industrial land and what is considered over and above that surplus

Mr Ching stated that at this time there is no formula to indicate what is the proper amount and what is considered surplus industrial lands but it has a lot to do with timing

Mr Sousa countered with the fact that the amount of industrial land should be associated with the amount of acre feet of water that is available and a project he has devoted seven years to develop additional waters Mr Ching stated that the answer is not contained in the Economic Development Plan but they did do some preliminary work after the last meeting on re-evaluating the lands that are designated industrial on the General Plan and they found that the yearly absorption rate has increased in the past several years

Councilmember Fass inquired about the statement of overriding considerations and how the staff was able to arrive at those conclusions Mr Ching explained that this was based on the testimony prior to the time the Environmental Impact Report was considered and adopted by both the Planning Commission and the City Council Mr Ching also explained the criteria that they used for evaluating industrial lands and classifying them as excellent or good or adequate the infrastructure contained within the property and the ability to serve those properties which are not no serviced lack of flooding or flooding potential proximity to railroads and highways, sufficient parcel size (those containing 40 000 square feet or less were considered insufficient size) Railroad ownership played a negative factor

Mr Ching responded to a question that was raised concerning the change in designation of the subject parcel from "only adequate" for industrial development to an excellent rating recently He stated the change in designation had a great deal to do with the early study of the parcel when utilities were not available and now the placement of utilities on the site changed the designation to excellent

Mr Tom Shephard, attorney for TMI appeared and responded to the objections raised by the opponents He introduced Mr Patrick McCarty a Stockton consultant on farming technology who explained the quality of the soil in the proposed site He explained that five clients had turned the leases down because they felt the soil quality was inadequate to support the crops that they planned to farm

In response to the Mayor's call for communications from proponents or opponents, the City Clerk announced that no new written communications had been filed

There being no other person present in the audience desiring to address the Council and after a full and fair opportunity had been given to all persons present, Mayor Ronk declared the public hearing closed at 8 40 p m and the following Resolution was presented for adoption

Resolution No 49 989 ordering the Stockton Airport Industrial Ltd Reorganization (A-82-1) including annexation to the City of Stockton and detachment from the French Camp-McKinley Rural Fire Protection District and the Montezuma Fire Protection District and notice of intent not to succeed to California Land Conservation Contract No 72-C1-523

MOTION It was moved by Vice Mayor Parkinson and seconded by Councilmember Oliva to adopt the foregoing resolution with the statement of overriding considerations

Councilmembers Stebbins and Fass felt that the statement of overriding considerations were not adequate to mitigate the conversion of the site to urban use which would constitute a significant environmental impact

The motion to adopt the resolution was carried by a vote of 6-3 with Councilmembers Fass Sousa and Stebbins dissenting

Vice Mayor Parkinson left at 8 45 p m

ORDINANCES AND RESOLUTIONS (FINAL ADOPTION)

- 1 Amendment to the Stockton Municipal Code--cultural preservation designations

Ordinance No 3807-C S amending Chapter 16 Part VII of the Stockton Municipal Code to except certain alterations from certificate of appropriateness requirements and to provide procedures for the amendment or rescission of designations

MOTION It was moved by Councilmember Fass and seconded by Councilmember Sousa to finally adopt the above ordinance

The motion was carried by a vote of 7-1 with Councilmember Clayton dissenting and Vice Mayor Parkinson absent

Vice Mayor Parkinson returned at 8 50 p m

ORDINANCES AND RESOLUTIONS (INTRODUCTION AND ADOPTION)

- 1 San Joaquin Strategic Plan

At this time Councilmember Fass stepped down due to the possibility of a conflict in interest

The following resolutions were introduced

- a) Resolution authorizing the appropriation of \$43 750 from the Unrestricted Fund Balance of the General Fund to Other Non-Departmental Budget Account No 10-1940-250; and
- b) Resolution authorizing the execution of an agreement between the City and the San Joaquin County Strategic Plan, an Unincorporated Association under which Association agrees to prepare a strategic plan encompassing the City of Stockton for the sum of \$43,570

MOTION It was moved by Councilmember Coale and seconded by Councilmember Clayton that the foregoing resolutions be introduced

The motion was carried by a vote of 7-1 with Councilmember Stebbins dissenting and Councilmember Fass having stepped down

MINUTES
CITY COUNCIL
VOL 112

CITY HALL
CITY COUNCIL CHAMBERS
STOCKTON CALIFORNIA
JULY 30 1984

The City Council of the City of Stockton met on the above date at the hour of 6 30 p m in special session with Mayor Ronk presiding called for the purpose of an executive session to discuss negotiations

ROLL CALL

Present Councilmembers Clayton Coale Herbert Oliva
Parkinson Stebbins and Mayor Ronk

Absent Councilmembers Fass and Sousa

(Councilmembers Fass and Sousa arrived at 6 50 p m)

The City Council adjourned to executive session at 6 30 p m

Councilmembers Fass and Sousa arrived at 6 50 p m and entered the executive session

The City Council returned from executive session at 7 31 p m and the special meeting was adjourned

* * * * *

The City Council of the City of Stockton met on the above date at the hour of 7 36 p m in regular session with Mayor Ronk presiding

Councilmember Oliva gave the invocation

Pledge of Allegiance to the flag

ROLL CALL

Present Councilmembers Clayton Coale Fass Herbert Oliva
Parkinson Sousa Stebbins and Mayor Ronk

Absent None

PROCLAMATIONS AND COMMENDATIONS

Ms Janet Bonner of the Friends of the Stockton Public Library presented \$1 200 to the City's Buy a Book Fund which was raised in a recent fundraiser The check was accepted by Ursula Meier Director of the Stockton Public Library and Mayor Randy Ronk

CONSENT ITEMS

It was moved by Vice Mayor Parkinson and seconded by Councilmember Coale that the following listed consent items (21 items) be approved

- 1 Approval of minutes
The minutes of the regular meeting held July 23 1984

RECEIVED

AUG 15 1984

CITY OF STOCKTON
COMMUNITY DEVELOPMENT DEPT.
PLANNING DIVISION

City Manager Cezar stated that starr has some concerns and problems that they wish to address and it is requested that the public hearing be continue to the meeting of August 13 1984

MOTION It was moved by Vice Mayor Parkinson and seconded by Councilmember Clayton and unanimously carried to continue the public hearing to the meeting of August 13 1984

4 Public Hearing - Stockton Airport Industrial Ltd Reorganization (A-82-2) including annexation to the City of Stockton and detachment from the French Camp-McKinley Rural County Fire Protection District the Montezuma Fire Protection District and the California Land Conservation Contract No 72-C1-523

At this time the City Clerk announced that this was the time and place set for the public hearing to consider public comment/testimony on the Stockton Airport Industrial Ltd Reorganization (A-82-2) Including Annexation to the City of Stockton and Detachment from the French Camp-McKinley Rural County Fire Protection District the Montezuma Fire Protection District and the California Land Conservation Contract No 72-C1-523

Mayor Ronk declared the public hearing open at the hour of 8 11 p m

Affidavits of Publication Posting and Mailing in the manner prescribed by law are on file in the office of the City Clerk

Mr Gunter Konold of the Community Department outlined the agenda report dated July 30 1984 and filed He stated that the subject property contains some 480 acres which lies generally west of the Stockton Metropolitan Airport and is generally bounded by Airport Way, Sperry Road, the Western Pacific Railroad French Camp Slough and Sperry Road He stated that the Local Agency Formation Commission at its hearing of June 15 1984 approved the proposal north of French Camp Slough and authorized the City of Stockton to proceed in accordance with the applicable provisions of the District Reorganization Act This calls for the holding of a protest hearing in view of the fact that less than 100% of the property owners in the subject area have requested annexation to the City of Stockton

Mr Konold stated that the subject property has been under the California Land Conservation Act of 1965 which is commonly known as the Williamson Act and under these provisions there are basically three options concerning the separation from the California Land Conservation contract They are as follows

- 1) The City Council can order the annexation and state the intent not to succeed to the conditions of the County under the Land Conservation Contract This option was established when the Council protested the execution of the contract on September 20 1982 for that portion located within one mile of the City limit line With this option the contract would become null and void upon annexation and without any cancellation fee being assessed to the property owner which is estimated to be approximately \$900 000 and paid to the State of California

- 2) The City Council could order the annexation and state its intent to succeed to the rights, duties and powers of the County under the Land Conservation Contract. And under this action the jurisdiction of the contract would transfer from the County to the City with the contract provisions intact. The property owners would then have the option to either file a notice of non-renewal or proceed on a previously initiated petition for cancellation in order to terminate the contract for the property.
- 3) The last possibility would be for the City Council not to approve the annexation and for the developer to carry on his plans in the County.

Mr. Konold stated that as far as environmental clearances are concerned the City has already certified the Environmental Impact Report at the time of the rezoning request earlier this year but it must adopt a statement of overriding consideration in conjunction with the annexation approval with findings supporting the conversion of the site to urban use. The Council therefore is being requested this evening to consider (1) the annexation, (2) the Land Conservation Contract and (3) the Statement of Overriding Consideration in conjunction with the annexation.

Mr. Tom Shepherd, Attorney, of 509 W. Weber Avenue appeared in behalf of the Stockton Airport Industrial Ltd. and its general partner, Teachers Management Institute, of which Robert Fitzgerald, who is president, is also present. He recited the number of times this has been before the Council and explained the reasons why the City should not succeed to the conditions of the County under the Land Conservation Contract. He pointed out that this project which they are trying to promote would not be as competitive nationally if the developers were required to pay the \$900,000 assessment to the State of California particularly in view of the off-site costs which come to a maximum of \$3.8 million.

Dr. Dario Marengo of 605 N. Madison Street appeared in opposition to the proposal. He stated that at the present time there are over 8,000 acres of industrial zoned land of which 60% lies vacant and in the immediate area there is well over 1,000 acres of land zoned industrial which is vacant. In the South Stockton area there is over 2,800 acres of vacant land zoned industrial which clearly indicates that Stockton is over-stocked with vacant land zoned industrial. The proponents for this zoning have stated that one of the reasons for land to be rezoned to industrial is to provide for employment, however throughout the industrial parks there appears to be only one or two developments while the rest is vacant. This is a situation which is very apparent throughout the nation indicating there is some 20% more industrial zoned land than is needed. He stated that the promise of high-tech industries coming to Stockton should not be the reason for approving the annexation as previous statements of this kind have not been necessarily realized as reflected in the vacant lands within the industrial parks. He stated that there are many problems with the development of this land which relate to drainage and sewer and the railroad bisects the subject parcel, thus the development of this land would not be cheap. This is merely a speculative matter. He stated that concrete is an irreversible process. This property should remain agricultural as agricultural is not an irreversible process. He also pointed

out that the removal of the property from the French Camp Fire District would have a damaging effect on the fire district as the number of industrial parks detaching from the district increase. He questioned the removal of the amount of agricultural land in the last 20 years from production and he pointed out the amount of employment farming of those properties could have produced. Despite the number of agricultural lands that have been removed from production in the last 20 years Stockton still remains an agricultural city. He wondered what the effect of employment would have been if those lands were not removed from production.

Mr. Bill Dorsey of the Buildings Trades Council previously stated that there is too much agricultural lands in Stockton which prompted a study by the San Joaquin County Planning Department. Dr. Marengo recited the reverse of Dorsey's statement was true.

In reference to the annexation boundaries Mr. Marengo pointed out that the City once felt that the inclusion of the John-Mansville property should be included in the annexation to provide a logical and systematic growth pattern; however, this time the staff did not include the requirement of the John-Mansville property. In his opinion, this omission would be considered leap-frogging and contrary to good planning principals. He referred to the fact that the City deliberately omitted the John-Mansville property and forced the property owners at the northeast corner of the subject parcel to be included in the annexation in order to provide the contiguous contact required for annexation. He added that the John-Mansville property was omitted because they were opposed to the proposal. He stated that the boundaries were gerrymandered in order to effect the annexation and consequently the boundaries are illogical and are contrary to policies of the State Government Code and the Local Agency Formation Commission.

Dr. Marengo said that in essence this is urban sprawl and pointed out that in an election held several years ago the electorate clearly indicated their preference to the contrary. He spoke about the designation of the subject property as prime agricultural soil which should be preserved for agricultural use and he referred to sections of the Stockton General Plan which documents this policy.

In reference to the Environmental Impact Report, Dr. Marengo pointed out a statement that was a gross error. The benefits of the proposed project outweigh the unavoidable conversion of agricultural land to urban use as supported by the EIR. He pointed out other errors contained within the EIR: (1) The site location will draw high-tech industry. He stated that the fact that it is close to the airport is a factor but not a major one. He pointed out that high-tech is attracted to Sacramento in view of the fact that it offers industry a cut rate on the electrical and water. (2) He referred to Page h-14 of the EIR. He stated that the whole statement of overriding consideration uses the overall economic plan for the City of Stockton. The statement: Using the average of absorption of 84 acres a year which was experienced in Stockton from 1975 to 1980 there is a 47 year supply of industrial land. He pointed out that the use of the year 1975-1980 were not a typical period in Stockton and using different years for a longer duration would alter the number of years the supply of industrial land could be absorbed. The statement continues: However, the study that only 1,252 acres of land were rated

excellent for industrial development this land which is the most likely to be developed could be absorbed in 14.8 years

Dr. Marengo stated that in order to make the overriding statement that the industrial land must be absorbed within 10 years. This leads you to believe industrial vacant land vacated rated good or adequate in this study may not be used and that TMI's property rated as excellent industrial land will be more developable. He stated that furthermore the use of a criteria developed by or used by the City of Redding further reduces the amount of excellent developable industrial land to absorption to 7.4 years, and therefore make the statement of overriding considerations. He stated that this was an incredible manipulation of figures. He stated that he would like an explanation from the staff of the Community Development Department as to how these figures were arrived at.

He discussed the Land Conservation Contract and explained the reasons why he felt that the contract provisions should remain intact and that the property owner should pay the State of California the assessments due which is estimated to be some \$900,000.

In answer to an inquiry from the Council Community Development Director Carlson stated that the staff did prepare that portion of the document and presented it to the Planning Associates for their inclusion in the environmental impact report. However due to the fact that they were not prepared to answer any questions on the environmental impact report document the staff members responsible for the preparation are not present this evening to answer questions.

MOTION It was moved by Councilmember Fass and seconded by Councilmember Sousa to continue the public hearing to the meeting of August 27 and to have the appropriate staff members present to answer questions about the environmental impact report.

The motion was carried by a vote of 5 to 4 with Councilmembers Coale, Herbert, Oliva and Vice Mayor Parkinson dissenting.

The Council recessed at 9:10 p.m. and reconvened at 9:40 p.m.

5 Public Hearing - Delinquent Garbage Bills (Stockton Scavenger Association Service Area)

At this time the City Clerk announced that this was the time and place set for the public hearing for the purpose of receiving public comment/testimony prior to ordering that delinquent garbage bills together with penalties thereon be assessed against the real property and collected on the tax roll (Stockton Scavenger Association Service Area).

Mayor Ronk declared the public hearing open at 9:41 p.m.

Affidavit of Publication in the manner prescribed by law is on file in the office of the City Clerk.

City Manager Cezar reviewed the agenda report entitled "Public Hearing/Delinquent Garbage Charges (Stockton Scavenger Association Service Area) dated July 30, 1984. Agenda Report No. 37 filed.

For City Manager's Use

Routing

Agenda Coordinator _____
Budget Control _____
Asst City Mgr _____
City Manager _____

TO City Manager (attn: Agenda Coordinator)

FROM John Collier, Community Development Director

TRANSMITTAL OF MATERIAL FOR CITY MANAGER'S AGENDA REPORT

Transmitted for the City Manager's Agenda Report for the City Council meeting of August 27, 1984 is the following item(s)

1. Aerial photograph of the proposed public utility site located on the east side of the city, near the intersection of the city street and the highway. The site is approximately 100 feet wide and 200 feet long. The aerial photograph shows the site is currently vacant and is surrounded by residential and commercial property. The proposed utility site is shown in red on the aerial photograph. The aerial photograph is attached to this transmittal report.

Please include the material in the following section of the City Manager's Agenda Report

- _____ Environment Assessment
- _____ Public Hearing/Protest
setting date _____ hearing date _____
- _____ Ordinances and Resolutions (Introduction and Adoption)
- _____ Committee Report
- _____ Staff Report
- _____ Legislation
- _____ Written Communication
- _____ Correspondence
- _____ City Manager's Newsletter
- _____ City Manager's Activity Report

Additional comments and/or instructions

Department Director _____ Date _____

Attachments
cc (1tv Attorney w/attachments (pink copy)

SUPPLEMENTAL INFORMATION FOR THE CONTINUED PUBLIC HEARING ON THE PROPOSED STOCKTON AIRPORT INDUSTRIAL, LTD REORGANIZATION (A-82-2) INCLUDING ANNEXATION TO THE CITY OF STOCKTON AND DETACHMENT FROM THE FRENCH CAMP-MCKINLEY RURAL COUNTY FIRE PROTECTION DISTRICT AND THE MONTEZUMA FIRE PROTECTION DISTRICT, AND CALIFORNIA LAND CONSERVATION CONTRACT NO 72-C1-523

On Monday evening, a continued public hearing will be held to determine the extent of protests relative to the above annexation, reorganization and detachment

DISCUSSION

This report is intended only to supplement the staff report submitted for the July 30, 1984 City Council meeting. Except as noted, the findings and recommendations of staff remain the same as earlier reported. The original report follows this supplement and the Council is referred to said report for specific description, analysis, conclusions, and recommendations related to the subject request.

At the July 30, 1984 Council meeting, numerous questions arose which required response by staff members who were not present and, therefore, the matter was continued to the August 27, 1984 meeting. The appropriate staff members have since been apprised of the situation and will be present at that meeting to respond to any related questions. In addition, the consultants who prepared the related environmental impact report (EIR 3-82) for the City will be in attendance to respond to any substantive questions on said EIR.

The purpose of this report is to clarify several issues related to (1) the preparation and certification of EIR 3-82, (2) the role of the EIR consultants, and (3) the requirement for and wording of a "Statement of Overriding Considerations"

Preparation and Certification of EIR 3-82

Environmental impact report (EIR 3-82) was prepared in accordance with the California Environmental Quality Act Guidelines (CEQA) and City of Stockton Guidelines for Implementation of CEQA. The purpose of the report was to inform public decision makers and the general public of the environmental effects of annexation and rezoning of 516 acres, including the proposed

SUPPLEMENTAL INFORMATION FOR THE CONTINUED PUBLIC HEARING ON
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447 acre Airport Gateway Center industrial/office park subdivision. The concerns of public agencies with permit authority for various aspects of this project were obtained wherever possible and were addressed in the Draft EIR. Any additional environmental concerns that arose during review of the Draft EIR were addressed in the Final EIR.

As background, the Planning Division of the Community Development Department determined that an EIR would be required when the project was originally proposed in mid-1982. In November 1982, a Notice of Preparation/Expanded Initial Study was prepared and circulated. The Draft EIR was completed and distributed for review in June 1983. The Final EIR was completed in November 1983.

After reviewing and considering Final EIR 3-82 and hearing related public testimony, the City Planning Commission, at its regular meeting on March 8, 1984, certified EIR 3-82 in conjunction with the approval of Amended TM29-83 and recommended that the City Council certify said EIR prior to approval of the related rezoning and any subsequent discretionary actions. The City Council also reviewed and considered the Final EIR and heard related public testimony at its regular meeting on April 23, 1984 and certified EIR 3-82 in conjunction with the approval of the related rezoning request and prior to any subsequent discretionary actions which may be taken by the Council. Both the Planning Commission and City Council adopted a finding that EIR 3-82 complies with the environmental standards and regulations as established by the California Environmental Quality Act and the City of Stockton Guidelines and that they had reviewed and considered the Final EIR prior to any related project approval.

EIR 3-82 concluded that since the conversion of the site to urban use would constitute a significant environmental impact which cannot be mitigated, however, the Planning Commission adopted a Statement of Overriding Considerations in conjunction with the approval of the related tentative subdivision map and with their recommendation for approval of the rezoning.

SUPPLEMENTAL INFORMATION FOR THE CONTINUED PUBLIC HEARING ON THE PROPOSED STOCKTON AIRPORT INDUSTRIAL, LTD REORGANIZATION (A-82-2) INCLUDING ANNEXATION TO THE CITY OF STOCKTON AND DETACHMENT FROM THE FRENCH CAMP-MCKINLEY RURAL COUNTY FIRE PROTECTION DISTRICT AND THE MONTEZUMA FIRE PROTECTION DISTRICT, AND CALIFORNIA LAND CONSERVATION CONTRACT NO 72-C1-523
(Page 3)

request The City Council subsequently adopted the same Statement of Overriding Considerations as recommended by the Planning Commission in conjunction with the approval of the rezoning

Pursuant to the State and City Guidelines for Implementation of CEQA, the City of Stockton and all other public agencies having discretionary authority over any aspect of the project, must consider the City's certified Final EIR prior to approval of any related discretionary actions Since EIR 3-82 was previously considered and found to be adequate with respect to the approval of the tentative map, rezoning, and any subsequent required discretionary approvals, including the proposed ordering of the annexation and the nonsuccession to the California Land Conservation Contract, no further review of the EIR is required by the Council unless the project is altered Nonetheless, adoption of a Statement of Overriding Considerations is required if the Council decides to approve the annexation and nonsuccession to the Land Conservation Contract

Role of the EIR Consultants

Pursuant to the City of Stockton Guidelines for the Implementation of CEQA, the Planning Division of the Community Development Department required the applicant to provide all necessary environmental documentation for the proposed industrial/office park project The applicant, therefore, retained a bona fide environmental consulting firm, a traffic engineer, and a civil engineer to work with the City Planning staff in the preparation of the necessary environmental documents It was the consultants' role to provide all technical information and studies deemed appropriate by the Planning Division and to coordinate their work with various City Departments Although the consultants billed the applicant for their services, they worked directly with and for the City of Stockton and their

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work was reviewed and modified, as necessary, by the Planning staff prior to acceptance and processing as official City documents. The consultants acted as support staff and all of their work was analyzed by the Planning Division for compliance to City standards and to insure objectivity. Therefore, the environmental documents for the project were ultimately processed as official City documents by the Planning Division and certified accordingly by the Planning Commission and City Council.

Requirement and Wording of a Statement of Overriding Considerations

The California Environmental Quality Act (CEQA) Guidelines provide under Section 15093 that

- (a) CEQA requires the decision maker to balance the benefits of a proposed project against its unavoidable environmental risks in determining whether to approve the project. If the benefits of a proposed project outweigh the unavoidable adverse environmental effects, the adverse environmental effects may be considered "acceptable"
- (b) Where the decision of the public agency allows the occurrence of significant effects which are identified in the final EIR but are not at least substantially mitigated, the agency shall state in writing the specific reasons to support its action based on the final EIR and/or other information in the record. This statement may be necessary if the agency also makes a finding under Section 15091(a) (2) or (a) (3)
- (c) If an agency makes a Statement of Overriding Considerations, the statement should be included in the record of the project approval and should be mentioned in the Notice of Determination

As discussed in EIR 3-82, the project site is located on prime (Class II) agricultural land. Conversion of the site to urban

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use will constitute a significant environmental impact which cannot be mitigated. Therefore, adoption of a Statement of Overriding Considerations was required in conjunction with the Planning Commission's approval of the tentative map and in connection with the City Council's approval of the rezoning for the subject project. As noted above, a similar statement is also required as part of the findings if the subject request for annexation and nonsuccession to the Land Conservation Contract is approved.

It should be noted that the findings and Statements of Overriding Considerations suggested by the staff, although supported by substantial evidence in the record (included in EIR 3-82 and in the public testimony presented at the previous Planning Commission and City Council hearings on this project), are simply that--suggestions. If the Council feels that there is insufficient evidence to support any or all of the suggested findings and overriding considerations or is dissatisfied with their wording, the Council may devise alternate finding(s) and overriding consideration(s) in conjunction with the approval of the request for annexation and nonsuccession to the Land Conservation Contract. In any event, the Council must adopt finding(s) and overriding consideration(s) in some form if the project is to be approved.

Recommendation

The City Council is referred to the original staff report, which follows, for the staff recommendations.

City Manager's Use
Routing
Agenda Coordinator
Budget Control
Asst City Mgr
City Manager

TO City Manager (attn Agenda Coordinator)
FROM John Carlson, Community Development Director

TRANSMITTAL OF MATERIAL FOR CITY MANAGER'S AGENDA REPORT

Transmitted for the City Manager's Agenda Report for the City Council meeting of July 30, 1984 is the following item(s)

Protest hearing on Stockton Airpprt Industrial Ltd (A-82-2) annexation

Please include the material in the following section of the City Manager's Agenda Report

- Environment Assessment
- Public Hearing/Protest
setting date _____ hearing date July 30, 1984
- Ordinances and Resolutions (Introduction and Adoption)
- Committee Report
- Staff Report
- Legislation
- Written Communication
- Correspondence
- City Manager's Newsletter
- City Manager's Activity Report

Additional comments and/or instructions

John Carlson
Department Director

Date 7/24/84

Attachments
cc, (itv Attorney w/attachments (pink copy))

45

STOCKTON AIRPORT INDUSTRIAL LTD REORGANIZATION (A-82-2)
INCLUDING ANNEXATION TO THE CITY OF STOCKTON AND DETACHMENT
FROM THE FRENCH CAMP-MCKINLEY RURAL COUNTY FIRE PROTECTION
DISTRICT AND THE MONTEZUMA FIRE PROTECTION DISTRICT, AND
CALIFORNIA LAND CONSERVATION CONTRACT NO 72-C1-523

On Monday evening, a public hearing will be held to determine the extent of protests relative to the above annexation, reorganization and detachment

DISCUSSION

Background

The above reorganization/annexation was initiated on the basis of a request by Teachers Management and Investment Corporation (TMI), General Partner of Stockton Airport Industrial Ltd. The territory is located directly west of the Stockton Metropolitan Airport and is generally bounded by Airport Way, Sperry Road, the Western Pacific Railroad, French Camp Slough, and a line approximately one mile south of Sperry Road.

The Local Agency Formation Commission (LAFCO) at its hearing of June 15, 1984, approved the proposal north of French Camp Slough and authorized the City of Stockton to proceed in accordance with the applicable provisions of the District Reorganization Act. This requires the holding of a protest hearing on the matter because less than 100 percent of the property owners in the subject area have requested annexation to the City of Stockton.

Specific information regarding this annexation is as follows

Area	480± acres
Land Use	An industrial/agricultural storage building, 12 single-family dwellings, several outbuildings, vacant parcels, crops, an abandoned sewage plant
Proposed Zoning	E-P, M-2
Population	31 (estimate)
Property Value (1983-84)	\$520,445 (land) \$359,167 (improvements)

STOCKTON AIRPORT INDUSTRIAL LTD REORGANIZATION (A-82-2)
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The annexation would generate the following estimated annual revenues to the City

Property Tax	\$3,200
State Subventions	\$1,436
Sales Tax	\$0
Business License Tax	\$0
Utility Tax	\$1,080

The amounts reflect current development, changes in land uses and elimination of the Land Conservation Contract will affect these revenues accordingly

Further, the property currently owned by Stockton Airport Industrial Ltd was entered into a California Land Conservation Contract on May 16, 1972 between the Board of Supervisors and the property owners at that time. The City Council on September 20, 1982 protested the execution of that contract for land located within one mile of the existing boundaries of the City of Stockton. LAFCO upheld the City's protest on October 19, 1982. Section 51243 of the Government Code provides that, if the City states its intent not to succeed to the rights, duties and powers of the County under such a Land Conservation Contract, the contract becomes null and void on the date of annexation on that portion of the land that is being annexed.

Environmental Clearance

The City Council at its meeting of April 23, 1984, reviewed and considered the final environmental impact report (EIR 3-82) and, after hearing related public input, certified EIR 3-82 in conjunction with the approval of the related rezoning request and for any subsequent discretionary action which may be taken by the Council, including the consideration of ordering the annexation and the succession to the California Land Conservation Contract. The City Council at that meeting adopted a finding that EIR 3-82 complies with the environmental standards and regulations as established by the California Environmental Quality Act and the City of Stockton Guidelines and that they had reviewed and considered the final EIR prior to any related project approval.

STOCKTON AIRPORT INDUSTRIAL LTD REORGANIZATION (A-82-2)
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In conjunction with the approval of the annexation proposal, the Council must also adopt a statement of overriding considerations with findings supporting conversion of the site to urban use

Current Situation

A Annexation

Section 56438 of the California Government Code requires that the City Council determine the extent of possible protests. Since less than 12¹ registered voters reside in the subject area, the annexation will be processed under the "uninhabited territory" provision of the code.

The City Council, as a result of the protest hearing, has to make one of two findings:

1. If it finds that written protests represent a majority of the assessed value of land within the subject area, then the proposed annexation has to be abandoned, or
2. If it finds that there is less than a 50 percent protest, the Council may order the annexation, reorganization and detachment.

Should the City Council order the annexation, we anticipate a final completion of the matter within two weeks. City services will commence on the effective date and our City departments and other affected agencies will have been notified prior to that date. Notice of the jurisdictional change, City emergency telephone numbers and other information will be routinely mailed to property owners, residents, and businesses in the annexed area. With the approval of the annexation, a simultaneous detachment from the above-noted special districts will be effectuated.

B Land Conservation Contract

Section 51240, et seq., of the California Government Code established procedures under the California Land Conservation

STOCKTON AIRPORT INDUSTRIAL LTD REORGANIZATION (A-82-2)
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Act of 1965, commonly referred to as the Williamson Act
Under these provisions, there are, basically, three options
regarding the Land Conservation Contract related to this
property, as follows

- 1 City Council orders the annexation and states the
intent not to succeed to the conditions of the County
under the Land Conservation Contract The Council
established the basis for this option by protesting
the execution of the contract on September 20, 1982
for that portion located within one mile of the city
limit line Under this option, the contract would
become null and void upon annexation, without any
cancellation fee being assessed to the property owner
(estimated to be approximately \$900,000 and paid to
the State of California)
- 2 City Council orders the annexation and states its
intent to succeed to the rights, duties and powers
of the County under the Land Conservation Contract
Under this action, the jurisdiction of the contract
would transfer from the County to the City with the
contract provisions intact

The property owners, at this point, have the option
to either file a Notice of Nonrenewal or proceed on
a previously initiated Petition For Cancellation in
order to terminate the contract for the property,
as follows

- a Filing a Notice of Nonrenewal will phase out the
Land Conservation Contract over the ensuing ten
year period During that time, the contract
restrictions remain in full force and effect and
the assessment on the property rises until it
reaches full market value level at the end of
ten years Thus, property taxes on the land
would increase steadily over a ten year period,
but the use of the land would have to remain
agricultural during the entire term
- b Filing a Petition For Cancellation under a one-
time provision by the State legislature The
property owner, in May of 1982, filed with the
County a Petition For Cancellation of the above

STOCKTON AIRPORT INDUSTRIAL LTD REORGANIZATION (A-82-2)
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contract in case the City failed to approve the annexation or decided to succeed to the conditions of the contract. If the Council approves the cancellation of the contract, the property owner will be assessed a cancellation fee equal to 12-1/2 percent of the full value of the property. It is estimated in discussions with the Assessor's office that, under this option, the cancellation fee to the owner will be approximately \$900,000 for the portion north of the French Camp Slough right-of-way and within the one mile radius of the City limits. This fee, as previously indicated, would not go into the County's or City's treasury but would be deposited in the general fund of the State of California.

- 3 A third possibility would be for the City Council not to approve the annexation and thus require the property owner to pursue the Notice of Nonrenewal or Petition For Cancellation in the County.

Conclusion

Regarding the annexation, it seems a foregone conclusion that the property will be developed industrially, whether in the City or in the County. The site is designated for industrial use on the General Plan of both jurisdictions. The City will be required to provide sewer service in either case. Only through annexation will the City also have the benefit of the development and the corresponding tax revenues it will generate.

Regarding the Land Conservation Contract, it appears logical that the contract should be terminated and the property developed according to its potential, as expressed in the General Plan designation and the rezoning to E-P. It has been standard practice by the City Council to terminate Land Conservation Contracts simultaneously with annexation. This has been determined to be procedurally the most prudent course of action. The same holds true in this case and the City would benefit by the County not succeeding to the contract provision, in that

- 1 Assisting and facilitating the development will aid the local economy and job picture, and create tax revenue.

STOCKTON AIRPORT INDUSTRIAL LTD , REORGANIZATION (A-82-2)
INCLUDING ANNEXATION TO THE CITY OF STOCKTON AND DETACHMENT
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- 2 The development will bring about major public improvements at a minimal or no cost to the City, and the costs to the developer are estimated to exceed the approximately \$900,000 cancellation fees due to the City of Stockton's development standards versus County standards
- 3 The property will be developed under performance standards and represents an attractive catalyst toward the development of the airport environs
- 4 Airport-oriented development tends toward making the Stockton Metropolitan Airport a more cost-effective attraction for major airlines
- 5 Making the property owner go through a cumbersome cancellation proceeding would tend to delay the development and above-noted benefits to the City
- 6 Assessing the property owner the cancellation fee could impair the implementation of the project since the cost would be passed on to the ultimate user. If the project should become economically not viable, the City would not gain the public improvements, both on site and off site
- 7 The cancellation fee would go to the State general fund, not the City or County treasury
- 8 Development in the County would be to less restrictive standards and the City would still be required to provide sewer service yet receive no tax revenues

Notifications

- 1 Legal advertising in the Stockton Record at least 15 days prior to the hearing date,
- 2 Posting a copy of the Notice of Public Hearing near the Council Chambers at least 15 days prior to the hearing date, and
- 3 Mailing a copy of the Notice of Public Hearing to each landowner in the subject territory, the County, and all affected districts

STOCKTON AIRPORT INDUSTRIAL LTD REORGANIZATION (A-82-2)
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Maps of the annexation/reorganization territory (Exhibit A)
and of the area under the Land Conservation Contract (Exhibit B)
are attached

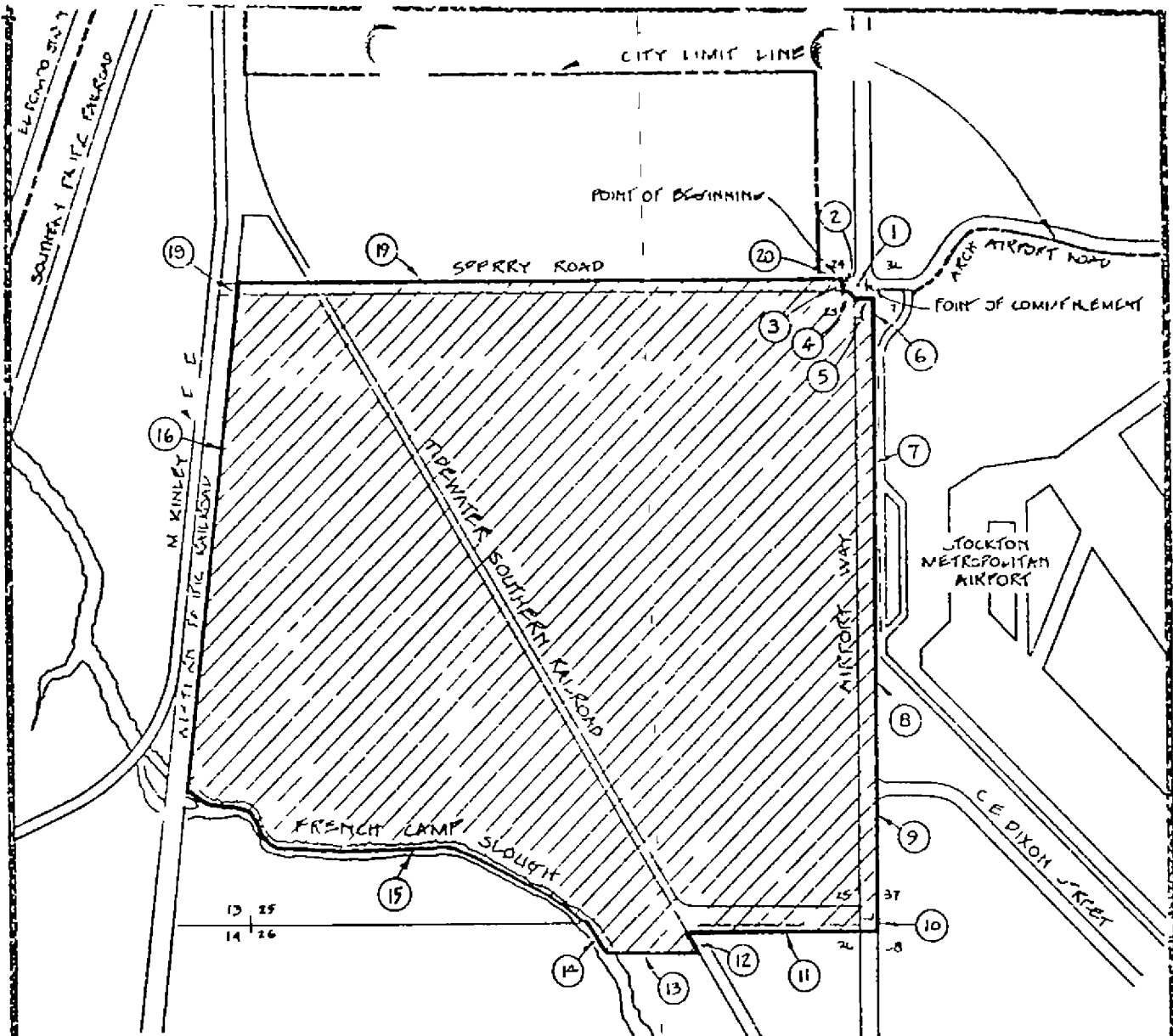
RECOMMENDATION

It is recommended that the City Council, in the absence of a majority protest, take the following action Order the annexation/reorganization and state the intent not to succeed to the provisions of the Land Conservation Contract on the subject property, based on the following findings and statements of overriding considerations

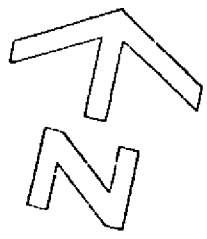
- 1 The proposal conforms to the City of Stockton General Plan designation and policies for the subject site
- 2 With the exception of the conversion of prime agricultural land, all potentially adverse environmental effects of subsequent development of the area will be mitigated to an acceptable level through project design and/or by enforceable conditions of approval on the associated tentative subdivision map and on any subsequent Use Permits and construction permits for specific projects within the project area
- 3 The benefits of the proposed project outweigh the unavoidable conversion of prime agricultural land to urban use as supported by EIR 3-82 and by the following overriding considerations
 - a An existing need for the proposed type of land use in the Stockton area has been demonstrated
 - b As the site is isolated from other parcels under California Land Conservation Contracts, development of the site is not expected to encourage other landowners to terminate such contracts for their parcels Additionally, the isolation of the subject property from other contracted lands seems to indicate that no nearby parcels are being considered for long-term agricultural use
 - c The project site is located within the Stockton Sphere of Influence and is consistent with both the City of Stockton and San Joaquin County General Plans, the Airport Land Use Plan, and the Stockton Metropolitan Airport Overlay Zone

STOCKTON AIRPORT INDUSTRIAL LTD REORGANIZATION (A-82-2)
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- d The project could be developed in the County since it is presently in conformance with their General Plan designation for the site This would result in overlapping and duplicative jurisdictional situations and potentially different development standards Since the development is in need of urban services and municipal facilities, it is logical that the area be developed in the City and served by municipal government and services
- e All urban services and facilities are available and can readily be extended to the project site
- f Upon full development, the subdivision could provide many jobs which will significantly benefit the economy of the City of Stockton and San Joaquin County



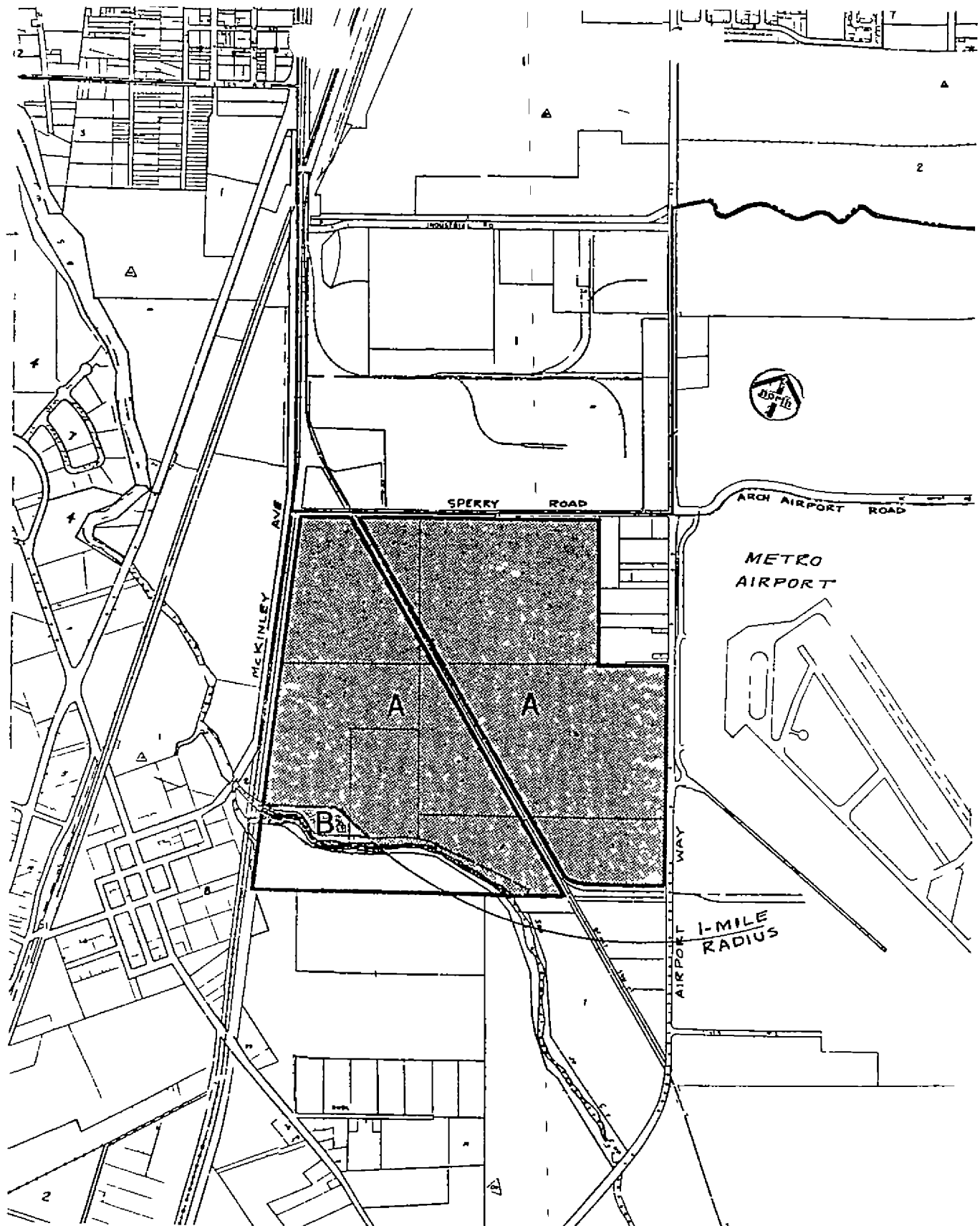
- | | |
|--------------------------|---------------------------|
| ① S 72° 29' 30" W 117.86 | ⑨ S 17° 01' 00" E 1636.11 |
| ② N 17° 30' 30" W 30.00 | ⑩ S 16° 49' 30" E 40.00 |
| ③ S 17° 30' 30" E 67.00 | ⑪ S 73° 05' 47" W 1363.61 |
| ④ R=38 Δ=87 37 32" | ⑫ S 47° 15' 17" E 182.22 |
| Ch=S 13 41 49 E 52.61 | ⑬ S 73° 05' 47" W 706.75 |
| L=58.11 | ⑭ N 45° 45' 25" W 228.11 |
| ⑤ N 72° 49' 10" E 134.9' | ⑮ NORTHWESTERLY 3300 ± |
| ⑥ S 12° 43' 59" E 200.00 | ⑯ N 9° 53' 25" W 3500 ± |
| ⑦ S 16° 10' 00" 204.25 | |
| ⑧ R=33 65 Δ=C 51 00 | |
| Ch=S 16 35 30 W 109.13' | |
| L=49° 1' | |



Rev. 6-22-84

No	Revised	B/	STOCKTON AIRPORT INDUSTRIAL, LTD REORGANIZATION/ANNEXATION NO A-82-2 (CONTAINS 480 ACRES NEAR CELL 5)	Map owned by
Drawn by C. B. J. P.			CITY OF STOCKTON DEPARTMENT OF PUBLIC WORKS	Date
Checked by V. J.				Drawing No.
Scale NO 1"				

EXHIBIT A



- -- AREA UNDER CALIFORNIA LAND CONSERVATION CONTRACT
- A -- AREA WITHIN ONE MILE OF CITY LIMITS
- B -- AREA PROPOSED FOR ANNEXATION OUTSIDE ONE MILE RADIUS

EXHIBIT B

LOCAL AGENCY FORMATION COMMISSION

LAFCO

COUNTY COURTHOUSE ROOM 508
22 EAST WEBER AVENUE
STOCKTON CALIFORNIA 95202
PHONE 209 / 944 2196

OF SAN JOAQUIN COUNTY

EXECUTIVE OFFICER
GERALD F. SCOTT

LEGAL COUNSEL
MICHAEL MCCREW
DEPU COUN COUNSEL

COMMISSION MEMBER

GEORGE L BARBER	CH	M
DS	SU	SO
HAROLD R NELSON	V	CEC
	S	CM
		VID
		C
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STANLEY MORTENS	N	E
	B	M
		MR
DON S MP	N	
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HARD S YOSHIK	VA	
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RECEIVED

SEP 19 1984

CITY OF STOCKTON
COMMUNITY DEVELOPMENT DEPT

September 18, 1984

City of Stockton
c/o City Clerk
City Hall
Stockton, CA 95202

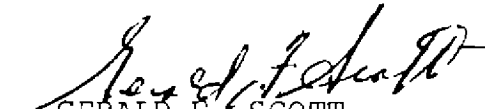
RE STOCKTON AIRPORT INDUSTRIAL REORGANIZATION (LAFCO 13-83), INCLUDING ANNEXATION TO THE CITY OF STOCKTON AND DETACHMENT FROM THE FRENCH CAMP-MC KINLEY RURAL COUNTY FIRE PROTECTION DISTRICT AND THE MONTEZUMA FIRE PROTECTION DISTRICT

Transmitted herewith for your records, is a certified copy of the Certificate of Completion for the above entitled jurisdictional change. As noted, the Certificate was recorded on August 31, 1984. The jurisdictional change is complete and effective as of the date of recording.

All filings required by Section 54900 of the Government Code have been completed. It is anticipated that this change will be included on the 1985-86 roll of the State Board of Equalization.

This letter completes our filing process on this matter. Please take appropriate action to notify all who need to know that the jurisdictional change is complete and now in effect.

Very truly yours,


GERALD F. SCOTT
Executive Officer

GFS kp

- cc County Surveyor w/certified copy of Certificate
- French Camp-McKinley Rural County Fire Protection District
- Montezuma Fire Protection District
- ✓Gunter Konold

84060757

CERTIFICATE OF COMPLETION ON
(G C 35350 / 54797 2 / 56460)

SAN JOAQUIN COUNTY
CORDER'S OFFICE
JES M JOHNSTONE

1984 AUG 31 PM 3 13

RECORDER Please return recorded documents
to LAFCo Room 508 Courthouse

RECORDED AT REQUEST OF

LaFCo

FEE **EXEMPT FROM FEE**

Short Form Designation

STOCKTON AIRPORT INDUSTRIAL

REORGANIZATION (LAFC 13-83)

1 Filed pursuant to action by City of Stockton
adopting Resolution No 40989 dated August 27, 1984
certified copy attached

2 The names of each district or city involved in this change of
organization or reorganization and the kind of type of change
of organization ordered for each such City or District are as
follows

CITY OR DISTRICT	TYPE OF CHANGE OR ORGANIZATION
<u>City of Stockton</u>	<u>Annexation</u>
<u>French Camp-McKinley Rural County Fire Protection District</u>	<u>Detachment</u>
<u>Montezuma Fire Protection District</u>	<u>Detachment</u>

3 The City or Districts are located in the following County(ies)
San Joaquin County

4 Boundary descriptions for said formation or change has been
attached

5 Terms and conditions if any are provided in said Resolution
attached

6 I hereby certify that the action taken by adoption of the above
cited Resolution complies with the boundaries and conditions
specified by the Local Agency Formation Commission of San Joaquin
County in Resolution No 619

THE FOREGOING IS A CORRECT COPY OF
THE ORIGINAL ON FILE IN THIS OFFICE

GERALD F SCOTT
EXECUTIVE OFFICER

Gerald F Scott
GERALD F SCOTT Executive Officer
Local Agency Formation Commission
of San Joaquin County

F-1 By Lois Martini
COMMISSION CLERK

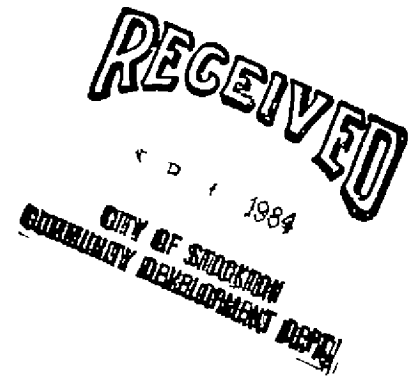
Dated AUGUST 31, 1984

Date 9-18-84



CITY OF STOCKTON

OFFICE
OF THE CITY CLERK
STOCKTON, CA, 95202
944 8459



September 5, 1984

TO INTERESTED PARTIES

REVISED LETTER

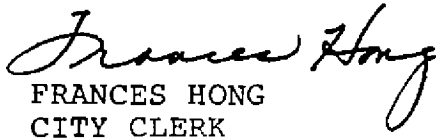
STOCKTON AIRPORT INDUSTRIAL LTD REORGANIZATION (A-82-2)
INCLUDING ANNEXATION TO THE CITY OF STOCKTON AND DETACHMENT FROM
THE FRENCH CAMP-McKINLEY RURAL COUNTY FIRE PROTECTION DISTRICT
AND THE MONTEZUMA FIRE PROTECTION DISTRICT AND NOTICE OF INTENT
NOT TO SUCCEED TO CALIFORNIA LAND CONSERVATION CONTRACT NO
72-C1-523

The letter and attachments sent you on September 4, 1984
regarding the subject annexation should be discarded and replaced
by this revised letter dated September 5, 1984 For your
information, the map showing the area involved was incorrect

The reorganization and annexation of the area designated above
was approved by the City Council on August 27, 1984

The Certificate of Completion by the Executive Officer of the
Local Agency Formation Commission for the above mentioned
annexation was recorded in the office of the San Joaquin County
Recorder on August 31, 1984 as Instrument No 84060757
Effective date of annexation is August 31, 1984

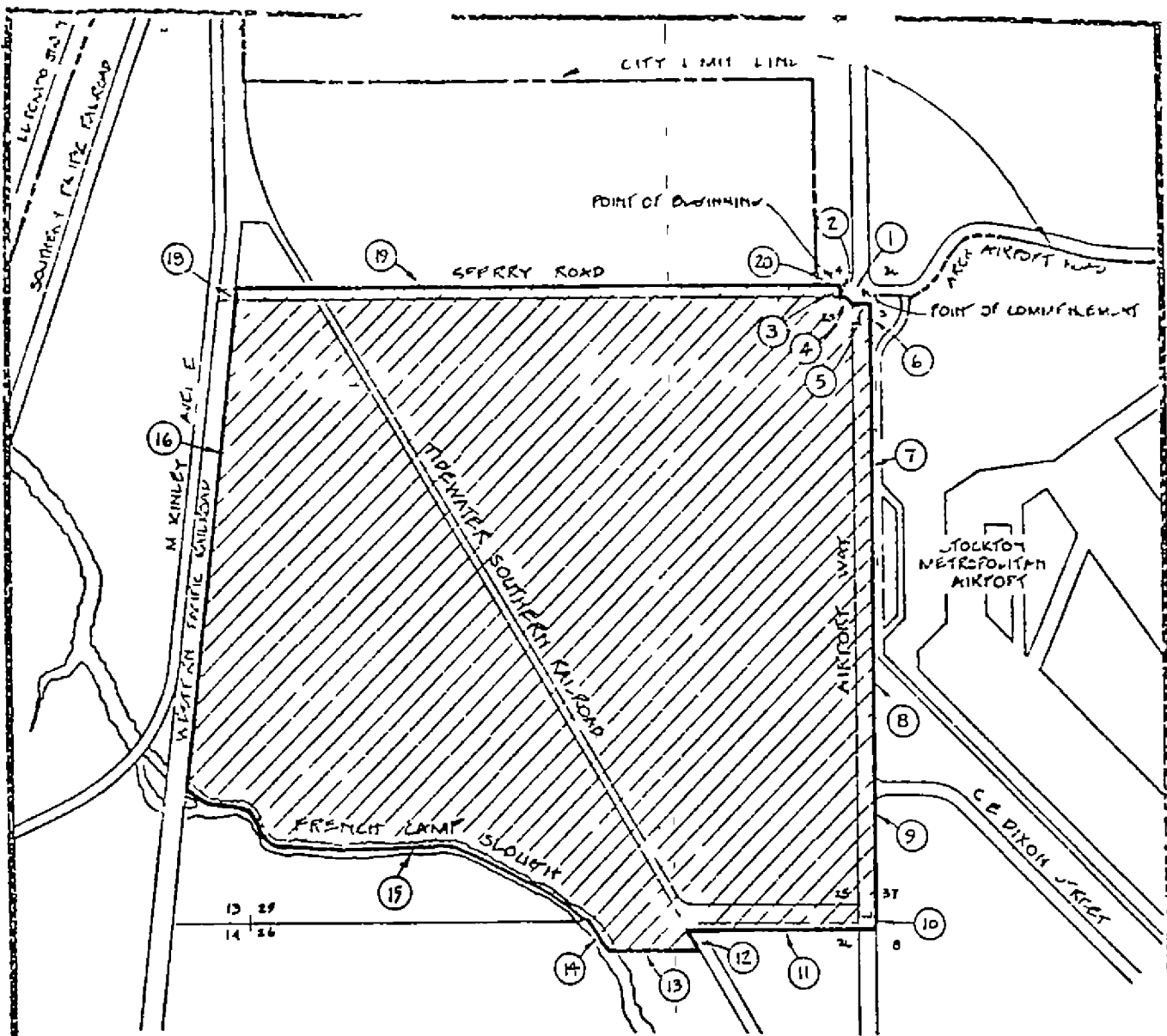
Attached is a copy of Resolution No 40,989 and the correct map
of the annexed area


FRANCES HONG
CITY CLERK

FH nck

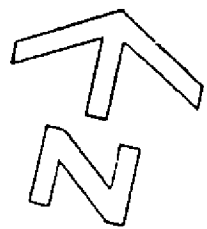
att

'Stockton Someplace Special!'



- | | |
|----------------------------|---------------------------|
| ① S 72° 29' 30" W 117 86 | ⑨ S 17° 01' 00" E 1636 11 |
| ② N 17° 30' 30" W 30 00 | ⑩ S 16° 49' 30" E 40 00 |
| ③ S 17° 30' 30" E 67 00 | ⑪ S 73° 05' 47" W 1363 61 |
| ④ R=38 Δ=87 37 32" | ⑫ S 47° 15' 17" E 182 22 |
| Ch=S 63° 41' 49" E 52 61 | ⑬ S 73° 05' 47" W 706 75' |
| L=58 11' | ⑭ N 45° 45' 25" N 228 11 |
| ⑤ N 72° 29' 30" E 124 49' | ⑮ NORTHWESTERLY 3300 ± |
| ⑥ S 12° 43' 59" E 200 00 | ⑯ N 9° 53' 25" W 3500 ± |
| ⑦ S 16° 10' 00" E 204 25 | |
| ⑧ R=3365 Δ=651 00 | |
| Ch=S 16° 35' 30" E 109 13' | |
| L=199 13 | |

REV 6-22 84



No	Revised	By	STOCKTON AIRPORT INDUSTRIAL, LTD REORGANIZATION/ANNEXATION NO A-82-2 (CITY RES 480 ACH. 1982 RES 11235)	Approved by	_____
Drawn by C. B. ...				Date	10/10
Checked by [Signature]			CITY OF STOCKTON DEPARTMENT OF PUBLIC WORKS	Drawing No	10
Scale 1" = 40'					

EXHIBIT A

Revised November 30, 1983
Revised June 22, 1984

STOCKTON AIRPORT INDUSTRIAL
REORGANIZATION/ANNEXATION NO A-82-2

EXHIBIT A

All that real property situated within Sections 13, 24, 25, 26, 37 and 38 of C M Weber Grant, "El Rancho del Campo de Los Franceses", more specifically described as follows

Commencing at the section corner common to Sections 24, 25, 36 and 37 of said Weber Grant as shown upon that Map of Survey filed for record in Book of Surveys in Volume 10, page 5, San Joaquin County Records, thence along the north line of said Section 25, S 72° 29' 30" W 87 74 feet, thence N 17° 30' 30" W 30 00 feet to the true POINT OF BEGINNING, said point being on the northerly line of Sperry Road (67 feet wide), said point also being on the existing City Limits line of the City of Stockton, thence along said City Limits line the following three (3) courses (1) crossing Sperry Road S 17° 30' 30" E 67 00 feet to the westerly termination of the round corner curve at the southwest corner of Sperry Road and Airport Way, (2) along a curve to the right, having a radius of 38 00 feet, a long chord bearing S 63° 41' 14" E 52 61 feet, a central angle of 87° 37' 32", an arc distance of 58 11 feet to the southerly termination of said round corner curve, and (3) easterly, crossing Airport Way, 134 99 feet, thence leaving said City Limits line, S 12° 43' 59" E 200 00 feet to a point that is 110 feet easterly of (measured at right angles) the westerly right of way line of Airport Way, thence parallel with the westerly right of way of Airport Way the following four (4) courses (1) S 16° 10' E 2169 25 feet, (2) along a curve to the left, having a radius of 33,645 feet, a long chord bearing S 16° 35' 30" E 499 13 feet, a central angle of 0° 51' 00", an arc distance of 499 13 feet, (3) S 17° 01' E 1636 11 feet and (4) S 16° 49' 30" E 40 feet to the easterly production of the north line of that certain 16 582 acre parcel shown on map of survey, filed in Book of Surveys, Volume 20, page 13, San Joaquin County Records, thence S 73° 05' 47" W along said easterly production and north line, and said north line produced westerly 1364 5 feet to the westerly line of the Tidewater Southern Railroad right of way, being on the easterly line of that certain tract of land described as PARCEL NO 2 in deed to the City of Stockton recorded in Volume 715, page 191, Book of Official Records of San Joaquin County, thence along said City of Stockton land the following three (3) courses (1) S 47° 15' 17" E 185 22 feet along said westerly line, (2) leaving said westerly line S 73° 05' 47" W 706 75 feet to the center of French Camp Slough (also known as Little Johns Creek), and (3) along the center of said slough N 45° 45' 25" W 228 11 feet to the southerly line of Section 25 of said Weber Grant, thence continue along the center of said slough in a general northwesterly direction 3300 feet, more or less, to the easterly line of the Western Pacific Railroad right of way, thence along said easterly line N 9° 53' 25" W 3500 feet, more or less, to the southerly line of Sperry Road (67 feet wide), thence N 17° 30' 30" W 67 00 feet to the northerly line of said Sperry Road, thence N 72° 29' 30" E along said northerly line 4285 7 feet to the southwest corner of that certain parcel of land annexed to the City of Stockton by Resolution No 26,330 on May 15, 1967, thence N 72° 29' 30" E 207 14 feet along said northerly line of Sperry Road and existing City Limits line to the hereinbefore mentioned true POINT OF BEGINNING

Contains 480 acres, more or less

FILED AND APPROVED AS TO DESCRIPTION BY

Robert T. Debraun

Henry M. Terata

EXHIBIT A

LOCAL AGENCY FORMATION COMMISSION

LAFCO

OF SAN JOAQUIN COUNTY

COUNTY COURTHOUSE ROOM 508
222 EAST WEBER AVENUE
STOCKTON CALIFORNIA 95202
PHONE 209 / 940-1100

EXECUTIVE OFFICER
GERALD F. SCOTT

LEGAL COUNSEL
MICHAEL MCGREW
COUNTY COUNSEL

RECEIVED

AUG 31 1984

COMMISSION MEMBERS

- GEORGE L. BARBER CHAIRMAN
4TH DISTRICT SUPERVISOR
- HAROLD R. NELSON VICE CHAIRMAN
PUBLIC MEMBER
- DAVID C. ENNIE
ESCALON CITY COUNCILMAN
- ARNOLD I. RUE
OCKTO CITY COUNCILMAN
- DOUGLASS W. WILHOI
2ND DISTRICT SUPERVISOR
- STANLEY MORTENSEN
PUBLIC MEMBER
- DON SIMPSON ALTERNATE
TRAC CITY COUNCILMAN
- RICHARD S. YOSHIKAWA
1ST DISTRICT SUPERVISOR

August 31, 1984

CITY OF STOCKTON
COMMUNITY DEVELOPMENT DEPT.

City of Stockton
City Hall
Stockton, CA 95202

French Camp-McKinley
Fire District
P O Box 9
French Camp, CA 95231

Montezuma Fire
District
2405 S "B" St
Stockton, CA 95206

RE STOCKTON AIRPORT INDUSTRIAL REORGANIZATION (LAFCO 13-83),
INCLUDING ANNEXATION TO THE CITY OF STOCKTON AND
DETACHMENT FROM THE FRENCH CAMP-MCKINLEY RURAL COUNTY
FIRE PROTECTION DISTRICT AND THE MONTEZEMA FIRE
PROTECTION DISTRICT


You are hereby notified that LAFCO's Certificate of Completion
for the above entitled jurisdictional change was recorded with
the San Joaquin County Recorder on August 31, 1984 as
Instrument No 84060757 The jurisdictional change
is complete and effective as of the date of recording

A Statement of Boundary Change has been transmitted to the State
Board of Equalization, the County Assessor, and the County
Auditor-Controller

Upon our receipt of the recorded documents, within about a week,
a certified copy of the recorded Certificate of Completion will
be sent to you for your records

Please notify any persons which you feel should know that the
jurisdictional change is complete and now in effect

Very truly yours,


GERALD F. SCOTT
Executive Officer

GFS kp

- cc County Elections Department w/map and boundary description
- ✓Gunter Konold
- Sheriff's Office
- Robert Fitzpatrick
- Thomas Shephard

Resolution No. 40,989

STOCKTON CITY COUNCIL

RESOLUTION ORDERING THE STOCKTON AIRPORT INDUSTRIAL LTD REORGANIZATION (A-82-2) INCLUDING ANNEXATION TO THE CITY OF STOCKTON AND DETACHMENT FROM THE FRENCH CAMP-MCKINLEY RURAL COUNTY FIRE PROTECTION DISTRICT AND THE MONTEZUMA FIRE PROTECTION DISTRICT AND NOTICE OF INTENT NOT TO SUCCEED TO CALIFORNIA LAND CONSERVATION CONTRACT NO 72-C1-523

WHEREAS preliminary proceedings for the above-entitled proposal were initiated by Resolution No 39 375 of the City of Stockton adopted on September 20 1982 and

WHEREAS the Local Agency Formation Commission of San Joaquin County on June 15 1984 adopted Resolution No 619 authorizing the City of Stockton to initiate proceedings on the above-entitled proposal in accordance with the applicable provisions of the District Reorganization Act and

WHEREAS in compliance with the requirements of the District Reorganization Act and the determination of the Local Agency Formation Commission the Council of the City of Stockton adopted Resolution No 40 846 on July 9 1984 initiating proceedings for the above-entitled proposal and

WHEREAS the resolution initiating proceedings for the above-entitled reorganization called a hearing upon said reorganization to be held July 30 1984 and continued to August 27 1984 at the hour of 7 00 p m or as soon thereafter as the matter could be heard in the Council Chamber City Hall Stockton California and directing the City Clerk to give notice of the hearing in the manner provided by the District Reorganization Act and

WHEREAS required notice was duly and properly given the affidavit of publication and certificate of posting and mailing are on file and at the time and place set for the hearing the Local Agency Formation Commission Resolution No 619 making determinations has been read aloud or summarized all

persons desiring to be heard have been heard and testimony and evidence for and against the proposed annexation have been duly considered the factors set forth in Government Code Section 56435 have been considered and

WHEREAS written protests filed and not withdrawn prior to the conclusion of the hearing represented less than one-half of the assessed valuation of the land within all territory proposed to be annexed or detached now therefore

BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF STOCKTON, AS FOLLOWS

1) That in exercise of the discretion and authority of Section 56438 of the Government Code the Council of the City of Stockton hereby orders the STOCKTON-AIRPORT INDUSTRIAL LTD REORGANIZATION (A-82-2) including annexation to the City of Stockton and detachment of affected territory from the French Camp-McKinley Rural County Fire Protection District and the Montezuma Fire Protection District, the boundaries for said subject territory being as set forth in Exhibit A attached hereto and incorporated herein by reference subject to the conditions as set forth below

The exemption of all of the subject territory proposed to be annexed to the City of Stockton from taxation for the payment of principal or interest or any other amounts which shall become due on account of any outstanding bonded indebtedness incurred by the City prior to the effective date of such annexation

2) That in exercise of the discretion and authority of Section 51243 of the Government Code the Council of the City of Stockton hereby states it intent not to succeed to the rights duties and powers of the County of San Joaquin under California

Land Conservation Contract No 72-C1-523 for the land within one mile of the existing boundaries of the City of Stockton

3) The Council of the City of Stockton hereby finds and determines that the following circumstances override the negative environmental consequences identified in the Environmental Impact Report

a) An existing need for the proposed type of land use in the Stockton area has been demonstrated

b) As the site is isolated from other parcels under California Land Conservation Contracts development of the site is not expected to encourage other landowners to terminate such contracts for their parcels Additionally the isolation of the subject property from other contracted lands seems to indicate that no nearby parcels are being considered for long-term agricultural use

c) The project site is located within the Stockton Sphere of Influence and is consistent with both the City of Stockton and San Joaquin County General Plans the Airport Land Use Plan, and the Stockton Metropolitan Airport Overlay Zone

d) The project could be developed in the County since it is presently in conformance with their General Plan designation for the site This would result in overlapping and duplicative jurisdictional situations and potentially different development standards Since the development is in need of urban services and municipal facilities it is logical that the area be developed in the City and served by municipal government and services

e) All urban services and facilities are available and can readily be extended to the project site

f) Upon full development the subdivision could provide many jobs which will significantly benefit the economy of the City of Stockton and San Joaquin County

PASSED APPROVED and ADOPTED this 27th day of
August 1984

/S/ RANDY RONK

RANDALL "RANDY" RONK, Mayor
of the City of Stockton

ATTEST

/S/ FRANCES HONG

FRANCES HONG City Clerk
of the City of Stockton

I Frances Hong certify that this document is a true and correct copy of Resolution No. 40,989 adopted by the City Council of the City of Stockton on August 27, 1984 and on file in the office of the City Clerk of the City of Stockton.

Do a August 31 1984 City Clerk of the City of Stockton
By [Signature]
Deputy

LOCAL AGENCY FORMATION COMMISSION

LAFCO

OF SAN JOAQUIN COUNTY

COUNTY COURTHOUSE ROOM 508
222 EAST WEBER AVENUE
STOCKTON CALIFORNIA 95202
PHONE 209 / 944 2196

EXECUTIVE OFFICER
GERALD F SCOTT

LEGAL COUNSEL
MICHAEL ROBERTS
DEPUTY COUNTY CLERK

Dunlop Honola

COMMISSION MEMBERS

- GEORGE L BARBER CHAIRMAN
H O STR CT SU ERV SR
- HAROLD R NELSON VICE C ARMAN
PUB C MEMBER
- DAVID C ENNIS
SCALON CT COUNC MAN
- ARNOLD I RUE
S OCKTON C TY COUNC LMAN
- DOUGLASS W WILHOIT
2ND D STR CT SU ERV SR
- WILHELM MORTENSEN ALTERNATE
UB C MEMBE
- DON SIMPSON A ERNATE
RAC C TY COUNC M N
- RICHARD B YOSHIKAWA AL ERN TE
S O STR CT SU E V SR

RECEIVED
DEC 12 1983

AGENDA

December 16, 1983 -- 2 00 P M

BOARD OF SUPERVISORS CHAMBERS

Courthouse, Stockton, California

* * *

Seasons Greetings



CITY OF STOCKTON
COMMUNITY DEVELOPMENT DEPT



A Roll Call

Resolution of Commendation for Arnold I Rue upon leaving the Commission

Approval of Minutes of October 21, 1983 meeting

B Matters Set For Public Hearing

- 1 PROPOSED STOCKTON AIRPORT INDUSTRIAL REORGANIZATION (Lafc 13-83), INCLUDING ANNEXATION TO THE CITY OF STOCKTON AND DETACHMENTS FROM THE FRENCH CAMP-McKINLEY RURAL COUNTY FIRE PROTECTION AND MONTEZUMA FIRE PROTECTION DISTRICTS
- 2 PROPOSED FORMATION OF NEW ISLAND RECLAMATION DISTRICT, DETACHMENT FROM RECLAMATION DISTRICT 2027, (MANDEVILLE ISLAND), AND ADOPTION OF SPHERE OF INFLUENCE (Lafc 16-83)
- 3 PROPOSED deBORD DETACHMENT FROM THE BANTA-CARBONA IRRIGATION DISTRICT (Lafc 17-83)
- 4 PROPOSED OXY LINE ANNEXATION TO THE LATHROP COUNTY WATER DISTRICT (Lafc 18-83)

C SPECIAL MATTERS

- 1 Presentation by the City of Tracy Concerning Sewer Expansion and Pending Annexations
- 2 1984 LAFCo Meeting Schedule
- 3 Resolution Re Employee Salary and Benefit Adjustments, 1984, and 1985
- 4 Election of Officers for 1984

Adjourn to the next regular or special meeting

LOCAL AGENCY FORMATION COMMISSION

LAFCO

OF SAN JOAQUIN COUNTY

COUNTY COURTHOUSE ROOM 508
222 EAST WEBER AVENUE
STOCKTON CALIFORNIA 95202
PHONE 209 / 944 2198

EXECUTIVE OFFICER
GERALD F SCOTT

LEGAL COUNSEL
MICHAEL MCGREW
DEPUTY COUNTY COUNSEL

COMMISSION MEMBERS

GEORGE L BARBER C AIRMAN
T D S R C SU ERV SOR
HAROLD R NELSON CE CHAR M N
BL C MEMBE
DAVID C ENNIS
D C COUNC MAN
ARNOLD I RUE
S OC ON CIT COU CLM N
DOUGLASS W WILHOIT
D O STR C B E V SOR
STANLEY MORTENSEN AL ERN TE
U C MEMBE
DON SIMPSON A TE NATE
RA C CO NCLM N
RICHARD B YOSHIKAWA A TE N TE
S O IC SUPERV SO

EXECUTIVE OFFICER'S REPORT

COMMISSION MEETING DECEMBER 16, 1983

TO LAFCO Commissioners
FROM Gerald F Scott, Executive Officer
SUBJECT PROPOSED STOCKTON AIRPORT INDUSTRIAL REORGANIZATION (LAFCO 13-83), INCLUDING ANNEXATION TO THE CITY OF STOCKTON AND DETACHMENTS FROM THE FRENCH CAMP-MCKINLEY RURAL COUNTY FIRE PROTECTION AND MONTEZUMA FIRE PROTECTION DISTRICTS

* * *

PROPOSAL The Stockton Airport Industrial Reorganization was initiated by resolution of the City of Stockton proposing to annex 516 acres to the City and simultaneously detach the property from the French Camp-McKinley Rural County Fire Protection District and the Montezuma Fire Protection District. The affected territory is located directly west of the Stockton Metropolitan Airport and is bounded on the north by Sperry Road, on the west by McKinley Avenue and the Western Pacific Railroad track, on the east by Airport Way, and on the south by a line one mile southerly of Sperry Road. Processing is proposed as an uninhabited (less than 12 registered voters) reorganization under the District Reorganization Act.

SIGNIFICANT ISSUES The Commission's deliberation of this proposal should include several significant issues

- The establishment of a "corridor of unincorporated territory" along the north side of Sperry Road (J M Manufacturing Co and Valimet Inc)
- The annexation of agricultural land located southerly of French Camp Slough,
- The justification for conversion of prime agricultural land to urban use and
- The consideration of the City's Environmental Impact Report and adoption of a statement of overriding considerations for any significant effects

DISCUSSION The purpose of this proposal is to secure City services and City zoning to accomodate development of the Airport Gateway Center, a proposed industrial/office subdivision of 83 lots on 401 acres of property north of the French Camp Slough. The 30 acre remainder parcel, located south of the French Camp Slough, is planned to remain in agricultural use. In order to establish contiguity with the City, the proposed annexation also includes 37 acres located at the southwest corner of Sperry Road and Airport Way which have been randomly divided into 13 parcels. The proposal also includes the right-of-way of the Tidewater Southern Railroad which traverses the property and a 5 acre parcel located along the south boundary, owned by the City of Stockton, and containing an abandoned sewage treatment facility.

A more detailed description of the proposal and the existing uses is contained in the City's Environmental Impact Report (EIR) which has been sent to all Commissioners. Copies of the EIR are available for review at the LAFCo Office and the City Planning Department.

The Airport Gateway site is currently used for irrigated row crops during the summer season. The existing development at the southeast corner of Sperry Road and Airport Way includes 12 single family residences and an industrial/agricultural storage building.

The territory proposed for annexation is shown for industrial use on both the City and County General Plans, except for the 30 acre parcel located south of the French Camp Slough which is designated for agriculture on both plans. The Airport Gateway site is zoned for General Agriculture and is currently under a Williamson Act contract. The properties at the southwest corner of Airport Way and Sperry Road are zoned M-2, Heavy Industrial. The City proposes to prezone the territory with a combination of E-P (Enterprise Performance) for the Gateway project site, M-2 (Heavy Industrial) for properties currently zoned M-2 by the County, and A-1 (Agricultural) for that portion south-erly of the French Camp Slough. The territory is within the City of Stockton's adopted Sphere of Influence.

The City has submitted the attached "City Services Plan" to demonstrate how services would be provided and funded. It concludes that all City services can be provided, however, as indicated, several services will require the developer's participation in funding the installation of facilities and operating costs.

As shown on the map, the proposal would create a "corridor of unincorporated territory," surrounded by the City on three sides, consisting of 147 acres. Adopted LAFCo policy provides that "Boundaries which create islands, strips or corridors within an agency providing urban services shall be avoided." The owners, J M Manufacturing Co and Valimet Inc, have been advised of staff's concern and have been sent a copy of this report. Commission policy in this regard could be achieved by expanding the annexation boundary to include the "corridor" area. However, it should be noted that all efforts by the City and the developer to obtain the consent of J M

Manufacturing to annex have failed. The attached letter from Steven Herum, Attorney for the proponents, discusses their efforts and lack of success in this regard.

The Airport Gateway property is currently under Williamson Act contract and restricted to agricultural use. However, since LAFCo upheld the City's protest of the contract, the City may terminate the contract on the majority of the property. A relatively small portion of the property near French Camp Slough would remain under contract. Please see map Fig 8 in the EIR. In a separate action, the owners have filed with the County to cancel the Williamson Act contract using the one-time "window" proceedings.

The U S Soil Conservation Service and the City's EIR both report that the project site is prime agricultural land. Section 54790.2 of the Government Code (attached) provides State Policies and Priorities in regard to the annexation of prime agricultural land. In general, both Commission policy and State policy discourages unnecessary or premature conversion of prime agriculture land. The City's EIR, in appendix pages H-12 through H-29, entitled "High Technology Business Park Site Evaluation," investigates the need for additional industrial land in the City at this time. The study narrows the comparison to competing sites, projects buildout, and concludes that annexation at this time would not be premature. Please refer to the EIR appendix for details. The development plan projects a 10-year buildout for the Airport Gateway Project.

No special districts which now serve the territory would be significantly affected by the proposal. However, the French Camp-McKinley Fire District stated that, should J M Manufacturing and Valimet be added to the annexation, they would expect to lose about \$18,000 per year in taxes.

The County Planning Department has recommended in favor of the annexation provided that the corridor on the north is included and that the territory southerly of French Camp Slough is deleted.

Consistent with City policy, the City's resolution requests that the annexed territory be exempt from paying any portion of the existing outstanding bond debt of the City. Property tax would be shared in accordance with City-County agreement.

As stated, the City as lead agency has prepared an Environmental Impact Report for this proposal. The EIR also covers the potential to include the J M and Valimet properties within the annexation. State Law requires that the Commission review and consider the City's EIR prior to approving an annexation. Since the EIR concludes that the conversion of agricultural land is an unavoidable significant environmental effect, the Commission must adopt a "Statement of Overriding Considerations" or deny the proposal. A draft statement of overriding consideration has been attached for consideration of the Commission.

CONCLUSION Although the proposal would convert about 400 acres of agricultural land, the facts seem to support the finding that the annexation would not be contrary to the State's Policies and Procedures in this regard. Concerning the EIR, the attached statement of overriding considerations is deemed to be appropriate for this situation.

Two changes in boundary are indicated by Commission Policy. First, the agricultural land southerly of French Camp Slough should be deleted. Secondly, the corridor of unincorporated territory should be added to the annexation boundary.


With the noted changes, the proposal appears to be consistent with the City and County General Plans, the City Sphere of Influence, State Policies, and the Commission's Guidelines for orderly expansion of City Services.

RECOMMENDATION The following determinations are recommended:

1. Certify that the Commission has reviewed and considered the City's Environmental Impact Report and adopt a "Statement of Overriding Consideration" concerning conversion of prime agricultural land. (Draft Statement attached for consideration)
2. Approve the proposal subject to the following boundary changes and conditions:
 - (a) Delete from the boundary all territory located southerly of the French Camp Slough.
 - (b) Add all territory located in the "corridor of unincorporated territory" to the north, relocating the boundary to the east line of the Western Pacific Railroad right-of-way.
 - (c) The exemption of all of the subject territory proposed to be annexed to the City of Stockton from taxation for the payment of principal or interest, or any other amounts which shall become due on account of any outstanding bonded indebtedness incurred by the City prior to the effective date of such annexation.
 - (d) The City shall prezone the affected territory prior to completion of the reorganization.

- 3 Designate the City of Stockton as conducting authority and authorize the City Council to initiate proceedings for uninhabited reorganization in accordance with Section 56430 of the Government Code

Very truly yours,


GERALD F. SCOTT
Executive Officer

GFS kp

ATTA City Services Plan
Proposal Maps
Letter From Steven Herum, Attorney
Letter From U S Soil Conservation Service
Section 54790 2
Justification of Proposal
Draft Statement of Overriding Considerations

Stockton Airport Industrial Ltd
Reorganization/Annexation No A-82-2

City Services Plan

1 Schedule of Activities

May 25, 1982 - petition to annex submitted by Teachers Management and Investment Corporation, General Partner of Stockton Airport Industrial Ltd

May 20, 1982 -- request to City to protest the California Land Conservation Contract No 72-CL-523 submitted by Teachers Management and Investment Corporation

September 20, 1982 -- City Council approved resolution authorizing the filing of the annexation proposal with LAFCO, consisting of 401 acres proposed for an industrial subdivision north of French Camp Slough, 45 99 acres planned for agricultural use south of the slough, 34 66 acres in the northeast corner containing 13 parcels, 30 03 acres of road and railroad rights-of-way, and 4 31 acres containing an abandoned sewage treatment plant and access road, for a total of 515 99 acres

September 20, 1982 -- City Council approved resolution protesting the execution of the Land Conservation Agreement

October 15, 1982 -- LAFCO upheld the City's protest to Land Conservation Contract

November 18, 1982 -- Notice of Preparation/Expanded Initial Study prepared and circulated for the Airport Gateway Center project

Mid-September 1983 -- anticipated Final Environmental Impact Report

2 Development Planning

County General Plan designation of site -- industrial north of French Camp Slough and Open Space/Agriculture south of slough

City General Plan designation of site -- industrial north of French Camp Slough and Open Space/Agriculture south of slough

City Zoning - proposed E P (Enterprise Performance) and M-2 (Heavy Industrial) north of French Camp Slough, A-1 (Agricultural) south of slough, Planning Commission hearing tentatively

planned for September or October 1983 City Council hearing approximately six weeks thereafter

Land Use -- proposed 83-lot industrial/office park north of French Camp Slough, agricultural use south of slough The tentative map planned for consideration by the Planning Commission in September or October 1983 Existing 13 parcels in northeast portion containing an industrial/agricultural storage building and 12 single-family residences

3 Municipal Services

- a) Police protection - the City Police Department does not anticipate significant problems in serving the proposed development Immediately upon annexation, the area will receive the same level of police protection currently provided the City in general Cost of police services will be funded for the most part from the City's general fund Major revenue sources for the general fund are sales taxes, utility taxes, property taxes, service charges, fines, forfeitures, indirect cost reimbursements, and state subventions
- b) Fire protection -- the nearest City fire station is located at Airport Way and First Street The area is currently served by the French Camp-McKinley fire district Normal City fire protection will be provided immediately upon annexation The cost of providing fire services is covered by the general fund As the proposed subdivision and nearby sites are developed in the future, a new fire station will be needed The developer will be required to participate in an Area of Benefit created for site acquisition and development of a new fire station The cost of a fire station is estimated to be \$650,000
- c) Paramedic service -- the Stockton Fire Department has a paramedic unit operating out of the fire station at Airport Way and First Street Annexation will make this service available Funding for paramedic service is derived from the general fund
- d) Streets -- annexation will not require changes in existing frontage improvements, such as streets, curbs, gutters, sidewalks Site development will require street improvements to accommodate projected traffic volume according to existing City standards Streets are in existence now at the periphery of the proposal Airport Way, Sperry Road, Arch-Airport Road, and McKinley Avenue The owner/developer will be required to provide and finance street modifications

and improvements as the proposed subdivision is developed and as will be conditioned on the tentative map, summarized as follows

- 1) all necessary onsite improvements
- 2) a minimum of three southbound lanes on Airport Way along tract frontage
- 3) full improvements on Sperry Road, including six lanes, raised median, and railroad overcrossing
- 4) owner/developer shall participate in an Area of Benefit to construct or widen Sperry Road and/or Industrial Drive westerly to Interstate 5
- 5) owner/developer shall participate in an Area of Benefit to realign and/or widen Arch-Airport Road between Airport Way and State Highway 99

Streets, constructed to City standards and dedicated, will be maintained by the City of Stockton. The general fund and gas tax funds are the sources of financing this maintenance

- e) Street lighting -- the developer will be required to install and finance street lighting to existing City standards as development occurs. Upon dedication the City will assume the maintenance and operation of street lights with general fund monies
- f) Traffic signals -- as the proposed industrial subdivision is developed and traffic volume reaches a certain level, the developer at the discretion of the Public Works Director will be required to install traffic signals as will be conditioned on the tentative map. The developer shall finance 75% or 100% of the cost, respectively. The City, with general fund sources, will assume the maintenance and operation of traffic signals
- g) Storm drainage -- there are no public storm drainage facilities in the project area, and the owner/developer must provide a complete stormwater drainage system, including detention basin and pump station, upon project development as per tentative map conditions. The City will assume the maintenance and operation of the pump station. A maintenance district, financed and operated by the property owners, will assume the maintenance and operation of the detention basin
- h) Sanitary sewers -- there is an 18" line in Airport Way, a 24" line in the easterly portion of Sperry Road and a 12" line in the westerly portion of Sperry Road. Connections to these existing lines must be made at the developer's

expense as development occurs, per tentative map conditions. User fees collected by the City from the development will be applied to the operation, maintenance and treatment of the sanitary sewage system. The type of development proposed is not anticipated to produce substantial quantities or strengths that would require extraordinary treatment by the City.

- 1) Water service -- California Water Service expressed its ability and willingness to serve the proposed development by providing water for industrial, office and fire protection needs. The cost distribution has not yet been established. A 16" water line exists at the intersections of McKinley Road/Industrial Drive and Airport Way/Industrial Drive. The Airport Way line has a 12" line extended to or near the intersection of Sperry Road. The most likely option for water service will be to connect the two closest points of connection on McKinley Road and Airport Way to achieve a looped water supply system according to City standards.
- j) Solid waste -- Stockton Scavenger Association will provide solid waste disposal services at the same level as the remainder of its City service area.

4 Costs

- a) Property taxes -- there is no difference in property taxes between the City and the County.
- b) Utility tax -- there is a six percent tax on utility charges in the City.
- c) Construction permit fees -- building standards and construction permit fees are similar between the City and the County.
- d) Business license fees -- business license fees are structured differently between the two jurisdictions, specific information may be obtained by contacting the City's Licensing Division, telephone 944-8313.
- e) Exemption from bonded indebtedness -- an annexing area will not have to pay for any outstanding indebtedness incurred by the City prior to the effective date of such annexation.
- f) Annexation processing fees - the City of Stockton will pay all annexation processing fees.
- g) Owner/developer will be required to finance or participate in Area of Benefit arrangements in providing public

facilities outlined in Item 3 above

- h) User fees are required upon connection to sanitary sewer system for maintenance and operation of that system

5 Economic Impacts on Community

The proposed industrial subdivision is projected to generate 12,000 jobs at full build-out, which will facilitate the growth of housing and service-related businesses. It could be the catalyst to an airport environs development of industrial/commercial/office uses.

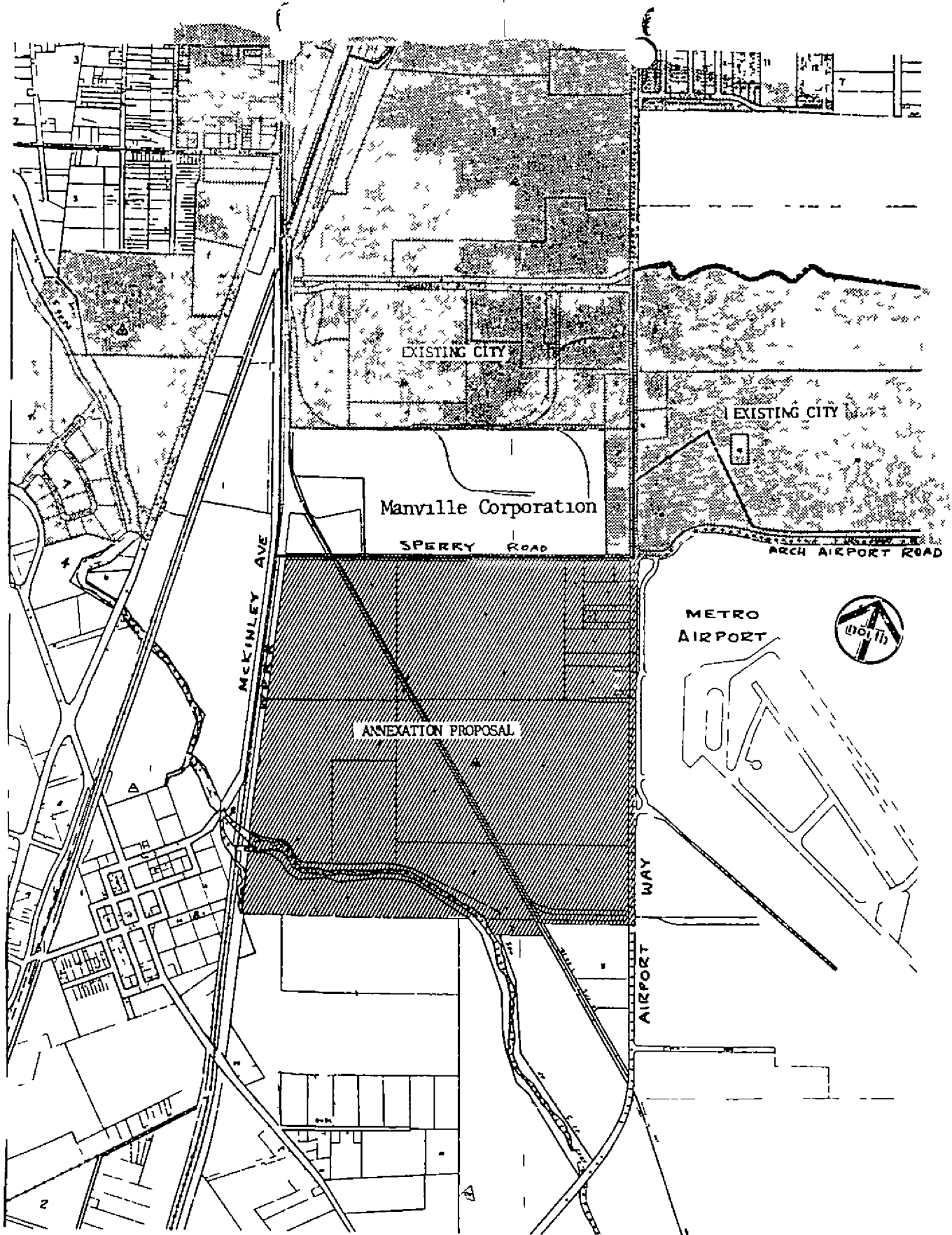
6 Governmental Aspects

The proposed annexation and development is located within the Stockton Sphere of Influence and the urban growth area. The industrial/office park proposal is consistent with the City's General Plan, the Airport Land Use Plan, and the Stockton Metropolitan Airport Overlay Zone, and is compatible with surrounding existing and proposed land uses.

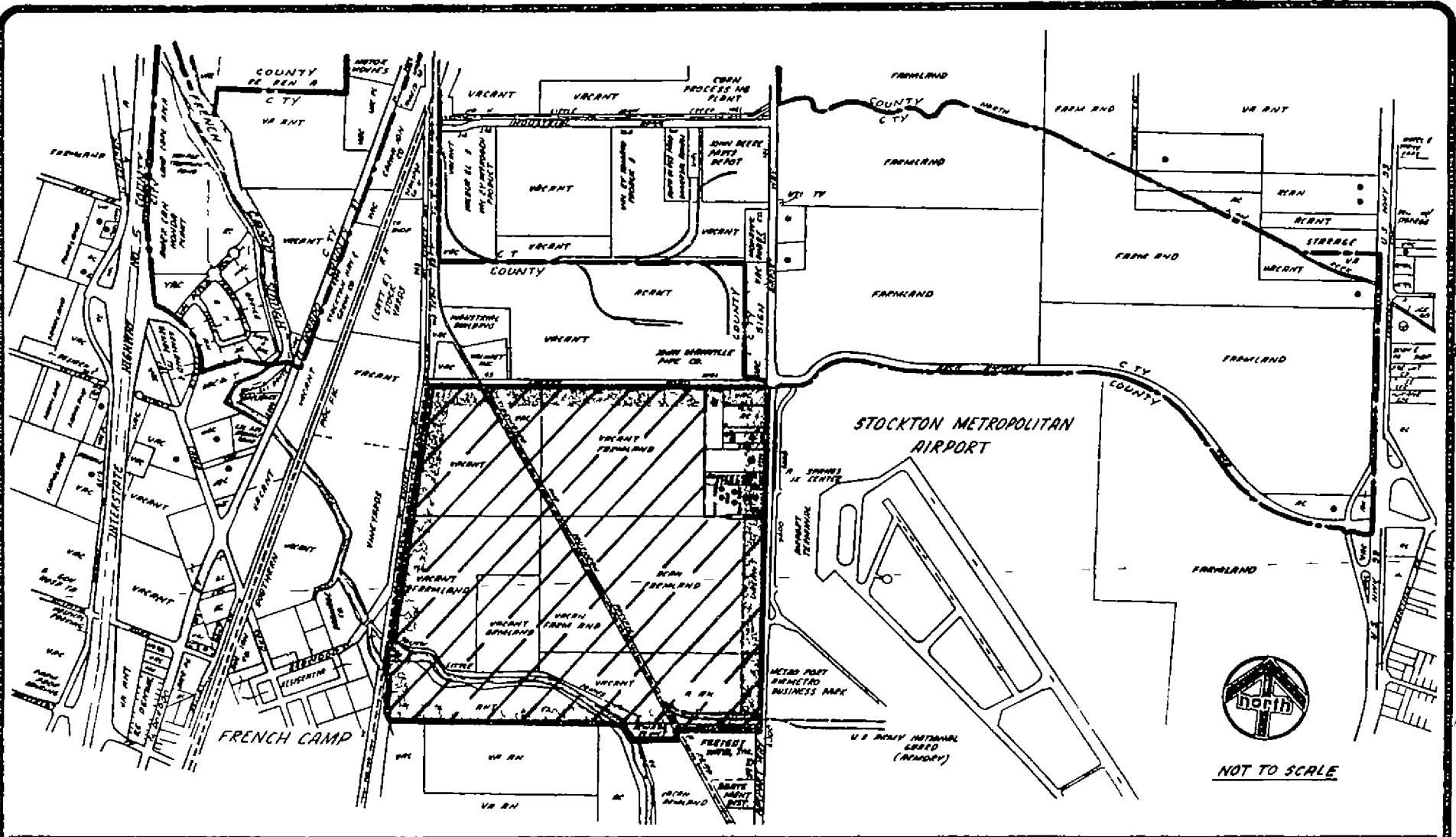
The project could be developed in the County since it is in conformance with that general plan. This would result in overlapping and duplicative jurisdictional situations. Since the development is in need of urban services and municipal facilities, it is proposed that the area be developed in the City and served by municipal government.

City services will be provided upon the effective date of the annexation. All facilities noted above are available and can be expanded and connected to at the applicant's expense or on the basis of Area of Benefit arrangements.

8/17/83



STOCKTON AIRPORT INDUSTRIAL LTD
REORGANIZATION/ANNEXATION NO A-82-2



LAND USE MAP

LEGEND

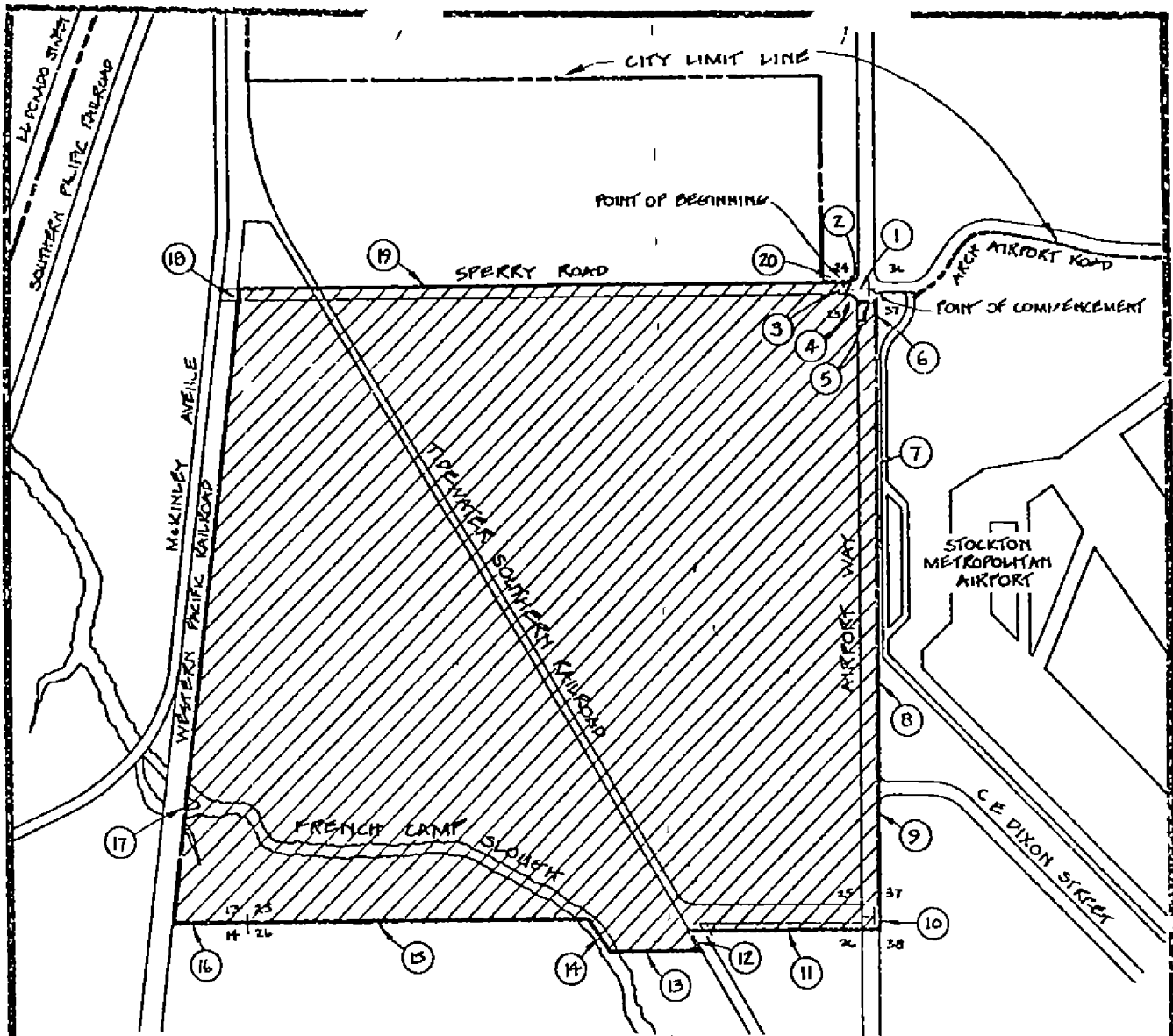
STOCKTON AIRPORT INDUSTRIAL, LTD.
and CITY of STOCKTON - EIR 3-82



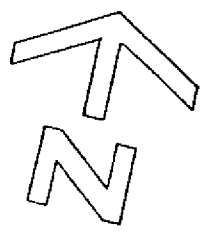
PROPOSED ANNEXATION AREA (516 Ac)
 PROPOSED INDUSTRIAL SUBDIVISION (447 Ac)
 SINGLE FAMILY UNITS

STOCKTON CITY PLANNING COMMISSION

FIG 7



- | | |
|---------------------------|---------------------------|
| ① S 72° 29' 30" W 117 86' | ⑨ S 17 01' 00" E 1636 11' |
| ② N 17 30' 30" W 30 00 | ⑩ S 16 49 30" E 40 00 |
| ③ S 17 30 30" E 67 00 | ⑪ S. 73° 05 47" W 1363 61 |
| ④ R=38 Δ=87° 31' 32" | ⑫ S 47 15 17" E 185 22 |
| Ch=S 13° 41' 49" E 52 61 | ⑬ S 73° 05 47" W 706 75' |
| L=58 11' | ⑭ N 45 45' 25" W 228 11' |
| ⑤ N 72 29 30 E 134 99' | ⑮ S 73 05' 47 W 2531 41' |
| ⑥ S 12 43 59" E 200 00 | ⑯ S 73° 38 12" W 522 86' |
| ⑦ S 16° 10 00" E 2169 25 | ⑰ N 9 53 25 W 4521 13' |
| ⑧ R=33 645 Δ=0° 51 00" | ⑱ N 12° 12' 49" W 61 27' |
| Ch=S 16 35 30 E 499 13' | ⑳ N 72 29' 30" E 4269 02' |
| L=499 13' | ㉑ N 72 29' 30" E 177 14 |



No	Revised	By	STOCKTON AIRPORT INDUSTRIAL, LTD REORGANIZATION/ANNEXATION NO A-82-2 (CONTAINS 516 ACRES MORE OR LESS)	App oved by
				<i>C. B. Johnson</i> City Engineer
Drawn by <i>C. PROFFER</i>			CITY OF STOCKTON DEPARTMENT OF PUBLIC WORKS	Date <i>1-1-82</i>
Checked by <i>RLW</i>				Drawing <i>110</i>
Scale NONE				

EXHIBIT A

Neumiller & Beardslee

A PROFESSIONAL CORPORATION
ATTORNEYS AND COUNSELORS

FIFTH FLOOR WATERFRONT OFFICE TOWER II
509 WEST WEBER AVENUE
STOCKTON CALIFORNIA 95202

FOUNDED AS
ASHLE & NEUMILLER
NUAF

STEVEN A HERUM

MAILING ADDRESS
PO DRAWER 6
STOCKTON CA 95202
U.S. MAIL

December 5, 1983

RECEIVED

DEC 07 1983

LAFCO

Gerald Scott, Executive Officer
LAFCO
County Courthouse
222 East Weber Avenue, Room 508
Stockton, California 95202

Dear Jerry

In connection with the Teachers Management, Inc (TMI) application schedule for public hearing before LAFCO on December 16, I want to inform you of efforts undertaken by TMI and others to encourage J-M Manufacturing to join the pending application request

As you know, almost one year ago we contacted both you and Mr Gunter Konold of the City Community Development Department regarding attempts at joining J-M Manufacturing in the annexation. Our interest in this matter stems from our fear that J-M Manufacturing might be involuntarily included within the annexation application

We feared and continue to fear that should J-M Manufacturing be involuntarily included in the annexation application, that they would then exercise their right of majority protest to prevent the annexation process. Annexation of the TMI property is critical to implementing the project. Unless the annexation is completed, the anticipated significant economic benefits will not be realized

Accordingly, we encouraged relevant local government representatives to contact J-M Manufacturing and to advocate that they voluntarily join the annexation. Apparently, these efforts did not prove successful

To be certain that every avenue was exhausted, we researched and collected information concerning the financial impact of annexation to J-M Manufacturing. As you will recall, I contacted you asking for assistance in gathering the necessary information. After preparing this information, we had numerous telephone conversations with the J-M Manufacturing's Corporate Legal Counsel and other managers

Gerald Scott, Executive Officer
December 5, 1983
Page Two

After these telephone conversations, we met with the Corporate Legal Counsel to discuss, in detail, the financial impact of annexation. Over two hours were spent at this meeting.

Based on this face to face meeting, the Corporate Legal Counsel asked that we put in written form the reasons in favor of J-M Manufacturing joining the annexation. He indicated that this letter would be presented to appropriate J-M Manufacturing decision-makers. With the assistance of Mr. Konold, yourself and the affected local districts we prepared such a letter. A copy of that letter is enclosed.

J-M Manufacturing received and reviewed the letter. J-M Manufacturing has communicated to us that they do not wish to join the annexation. Further, I understand that they would exercise their majority protest rights in the event the J-M property was involuntarily included in the annexation. They wanted us to know, however, that they did not oppose the pending annexation in its present form. While I cannot represent the position of J-M Manufacturing, I believe that they generally concluded that the disadvantages of annexation outweighed the potential advantages.

In short, we have actively worked with the City and LAFCO staff in formulating and implementing a strategy that would convince J-M Manufacturing to annex to the City of Stockton. Notwithstanding our best efforts J-M Manufacturing simply will not permit annexation of its property.

If I can provide you or members of the Commission with further information regarding this matter, please do not hesitate to contact me.

Very truly yours,


STEVEN A. HERUM
Attorney at Law

SAH/km



United States
Department of
Agriculture

Soil
Conservation
Service

1222 Monaco Court #23
Stockton, California 95207

November 23 1983

Mr Gerald F Scott
Executive Officer
Local Agency Formation Commission
222 East Weber Avenue
Stockton, California 95202

Dear Mr Scott

We have reviewed your referral LAFC 13-83 - proposed Stockton Airport Industrial Reorganization and wish to make the following comments

The area according to our old published Soil Survey (Stockton Area 1951) shows two soils on the site (see attached map) NS Nord Stockton Complex IIIe5 this is a mixture of Stockton clay and old Indian mounds which has been leveled until it is no longer identifiable ST Stockton Clay IIIe5 makes up the majority of the site We would note that the area will be remapped in the near future and that the above classifications are subject to change

We would classify these soils as prime agricultural soils and as such should be preserved for the production of food and fiber

One other note, these soils are subject to high shrink-swell, thus proper precautions should be taken during construction to see that proper bedding is provided for building foundations concrete slabs and sidewalks Any topsoil removed during the construction phase, should be stockpiled and then used for lawns, etc On site investigations should be made to determine depth to hardpan, etc

Thank you for the opportunity to comment on the proposal Should you need additional information, please contact this office

Sincerely

CLIFFORD C SORENSEN
District Conservationist

CCS/slw

RECEIVED

NOV 28 1983

LAFCO



The Soil Conservation Service
is an agency of the
Department of Agriculture

SCS AS 1
10-79

7

54790 2 Conversion of open-space lands to other use, policies and priorities

In reviewing and approving or disapproving proposals which could reasonably be expected to induce, facilitate or lead to the conversion of existing open-space lands to uses other than open-space uses, the commission shall consider the following policies and priorities

(a) Development or use of land for other than open-space uses shall be guided away from existing prime agricultural lands in open-space use toward areas containing nonprime agricultural lands, unless such an action would not promote the planned, orderly, efficient development of an area

(b) Development of existing vacant or nonprime agricultural lands for urban uses within an agency's existing jurisdiction or within an agency's sphere of influence should be encouraged before any proposal is approved which would allow for or lead to the development of existing open-space lands for non-open-space uses which are outside of the agency's existing jurisdiction or outside of an agency's existing sphere of influence

(Added by Stats 1974, c 531)

JUSTIFICATION OF PROPOSAL
for Reorganization or Change of Organization Affecting Local Agencies

Filed with Local Agency Formation Commission of San Joaquin County
c/o Gerald F. Scott, Executive Officer
County Courthouse, Room 508
222 East Weber Avenue Phone (209) 944-2196
Stockton, California 95202

Date August 17, 1983

The undersigned, on behalf of the proponents of the subject proposal, hereby give notice of intention to

- | | |
|--|---|
| <input type="checkbox"/> incorporate a city | <input type="checkbox"/> disincorporate a city |
| <input type="checkbox"/> form a district | <input type="checkbox"/> dissolve a district |
| <input checked="" type="checkbox"/> annex territory to an agency | <input checked="" type="checkbox"/> detach territory from an agency |
| <input type="checkbox"/> consolidate existing agencies | |

To further deliberations by the Commission, we submit the following

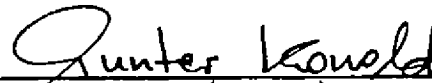
- 1 Three (3) copies of this completed "Justification of Proposal"
- 2 Seven (7) copies of legal metes and bounds description of affected territory - consistent with standards acceptable to the County Surveyor's Office
- 3 Fifteen (15) copies of maps showing affected territory and affected agencies (maps to be no smaller than 8½" x 11" which is the most preferable size and shall be no larger than 18" x 26")
- 4 Filing & processing fees in accordance with LAFCO fee schedule
- 5 City Council resolution (certified)

The following persons (not to exceed three) are to be mailed copies of the Executive Officer's Report and notice of commission hearings regarding the subject proposal

Mr Robert W Fitzpatrick, The TMI Building, 14 Corporate Plaza Drive, Newport Beach,
(name) (address) CA 92660

Mr Thomas Shephard, Neumiller & Beardslee, P O Drawer 20, Stockton, CA 95201
(name) (address)

Mr Gunter Konold, Community Development Department, City Hall, Stockton, CA 95202
(name) (address)


(signed)

COMPLETE ALL QUESTIONNAIRE ITEMS

If a question is not applicable to your proposal, you may so state

- 1 Local agency organization changes hereby proposed--designate affected agencies and annexations by name
Stockton Airport Industrial Ltd
Reorganization, including Annexation No A 82-2 to the City of Stockton and detachment from the French Camp-McKinley Rural County Fire Protection District

- 2 Statutory provisions governing proceedings
 District Reorganization Act of 1965
 Municipal Organization Act of 1977

- 3 Do proposed boundaries create an island or corridor of unincorporated territory?
A 147+ acre corridor will be created of property owned by Johns Manville Corporation and Valimet, Incorporated
- 4 Do proposed boundaries split lines of assessment or ownership?
No
- 5 Land area affected a) square miles 806 b) acres 516+
- 6 Population in subject area 31 (estimate) 7 Number of registered voters 9
- 8 Registered voter density (per acre) 0174 9 Number of dwelling units 12
- 10 Estimate of population increase in next 10 years
0
- 11 Present land use of subject area
An industrial/agricultural storage building, 12 single family residences, several outbuildings, vacant parcels, crops, an abandoned sewage plant
- 12 What is the intended development of this area
Industrial subdivision on vacant 401 acres north of French Camp Slough, no change proposed on other parcels
- 13 Present zoning
GA-20, I-PA, M-2
- 14 Describe proposed zoning changes
A-1, E-P, M-2
- 15 Assessed value of land \$536,941 (82/83) Assessed value of personal property
- 16 Assessed value of improvements \$358,215 (82/83) \$ 0
- 17 Value of publicly owned land in area \$ Unknown
- 18 Governmental services required by this proposal which are not presently available
General City services upon annexation
- 19 What alternative measures would provide services listed in Item 18 above?
Retain current County services if area is not annexed
- 20 What modifications must be made to existing utility and governmental facilities to provide services initiated by this proposal?
See attached City Services Plan and Environmental Impact Report
- 21 What approximate costs will be incurred in accomplishing requirements of Item 20 above?
Not determined at this time
- 22 How will such costs be met?
Installation costs are proposed to be assumed by owner/developer, City will assume cost of maintenance and operation after dedication
- 23 Will provisions of this proposal impose greater than normal burden on servicing agency or affected property?
Burden on City will be mainly in determining needed facilities and in coordinating their installation
- 24 Check here if you are submitting further comments and evaluations on additional pages

DRAFT "

STOCKTON AIRPORT INDUSTRIAL LTD
REORGANIZATION/ANNEXATION NO A-82-2
CITY OF STOCKTON EIR 3-82
STATEMENT OF OVERRIDING CONSIDERATIONS

The California Environmental Quality Act (CEQA) Guidelines provides, under Section 15091, that

No public agency shall approve or carry out a project for which an EIR has been completed which identified one or more significant environmental effects of the project unless the public agency makes one or more written findings for each of those significant effects, accompanied by a brief explanation of the rationale for each finding

Furthermore, Section 15093 provides that

- (a) CEQA requires the decision maker to balance the benefits of a proposed project against its unavoidable environmental risks in determining whether to approve the project. If the benefits of a proposed project outweigh the unavoidable adverse environmental effects, the adverse environmental effects may be considered "acceptable"
- (b) Where the decision of the public agency allows the occurrence of significant effects which are identified in the final EIR but are not at least substantially mitigated, the agency shall state in writing the specific reasons to support its action based on the final EIR and/or other information in the record. This statement may be necessary if the agency also makes a finding under Section 15091(a)(2) or (a)(3)
- (c) If an agency makes a statement of overriding considerations, the statement should be included in the record of the project approval and should be mentioned in the notice of determination

As discussed in FIR3-82, the project site is located on prime (Class II) agricultural land. Conversion of the site to urban use will constitute a significant environmental impact which cannot be mitigated. Therefore, a Statement of Overriding Considerations must be adopted if the proposed project is to be approved

The following findings and statements of overriding considerations are offered, with substantial supporting evidence provided in EIR3-82, in the event that the decision-making body wishes to approve the project

- 1 The conversion of the subject site to urban use is appropriate at this time as a need for the proposed land use has been shown, and conversion of alternate suitable parcels in the surrounding area would not significantly reduce the potential effects on agricultural lands. This finding is substantiated by a study entitled, "High Technology Business Park-Site Evaluation", which is included in the City of Stockton's Responses to Comments on Draft EIR3-82 (Appendix H)
- 2 Because the subdivision site is isolated from other parcels under land conservation contracts, development of the site should not encourage other landowners to terminate such contracts for their parcels. According to the San Joaquin County Assessor's Office, there are currently over 550,000 acres of agricultural land in the County under land conservation contracts. Development of the proposed subdivision will require termination of the contract on 401 acres, which is about 0.07 percent of the total land in San Joaquin County under land conservation contracts. As shown on Figure 9 in EIR3-82, the subdivision site is the only area in the vicinity under a land conservation contract. This seems to indicate that no nearby parcels are being considered for long-term agricultural use.
- 3 The proposed annexation and development is located within the Stockton Sphere of Influence and the City's urban growth area. The industrial/office park proposal is consistent with the City's General Plan, the Airport Land Use Plan, and the Stockton Metropolitan Airport Overlay Zone, and is compatible with surrounding existing and proposed land uses.
- 4 The project could be developed in the County since it is presently in conformance with their General Plan and zoning designations for the site. This would result in overlapping and duplicative jurisdictional situations. Since the development is in need of urban services and municipal facilities, it is logical that the area be developed in the City and served by municipal government and services.

City services will be provided upon the effective date of the annexation. All urban services and facilities are available and can be expanded and connected to at the applicant's expense or on the basis of Area of Benefit arrangements.

- 5 The proposed industrial subdivision is projected to generate 12,000 jobs at full build-out, which will facilitate the growth of housing and service-related businesses. It could be the catalyst to an airport environs development of industrial/commercial/office uses, which will significantly benefit the economy of the City of Stockton as well as San Joaquin County.

December 8, 1983